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September 24, 1999

OFFICE OF ENVIRONMENTAL
QUALITY CONTROL

TO: GENEVIEVE SALMONSON, DIRECTOR
OFFICE OF ENVIRONMENTAL QUALITY CONTROL
DEPARTMENT OF HEALTH

FROM: KAZU HAYASHIDA
DIRECTOR OF TRANSPORTATION

SUBJECT: FINDING OF NO SIGNIFICANT IMPACT FOR PAVE ADDITIONAL
BARGE TERMINAL AREA, KAWAIHAE HARBOR, HAWAII
JOB H. C. 5313

In accordance with Act 241, SLH 1992, we have completed the formal Draft Environmental Assessment 30-day review period. We have not received any adverse comments and have determined that this project will not have a significant environmental effect. Therefore, we are filing a Finding of No Significant Impact (FONSI). Please publish the notice of availability for this project in the October 8, 1999 OEQC Bulletin. ✓

Enclosed are the original and four (4) copies of the FONSI. Please note Mr. Brian T. Nishimura, Planning Consultant, will E-mail the required summary of the proposed action directly to your office.

Should you have any questions, please contact Napoleon Agraan of our Harbors Division at 587-1956.

Enc.

c: Wesley Segawa, Wesley Segawa & Associates, Inc.
Brian T. Nishimura, Planning Consultant

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OCT 8 1999

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FINAL ENVIRONMENTAL ASSESSMENT
AND FINDING OF NO SIGNIFICANT IMPACT

② PAVE ADDITIONAL BARGE TERMINAL AREA *

* ① KAWAIHAE HARBOR *

Job H. C. 5313

KAWAIHAE 1ST, SOUTH KOHALA,
ISLAND OF HAWAII
TAX MAP KEY NO. (3) 6-1-03: POR. OF 24

APPROVING AGENCY: STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION

869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

PREPARED BY:
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SEPTEMBER, 1999

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1. INTRODUCTION

1.1 Purpose

The State of Hawaii, Department of Transportation, Harbors Division, is proposing landside improvements to their Kawaihae Harbor facilities at Kawaihae 1st, South Kohala, Hawaii. The purpose of this Environmental Assessment is to comply with the requirements of Chapter 343, Hawaii Revised Statutes (HRS) which are triggered by the use of State land and funding for the proposed project.

1.2 Identification of Proposing Agency

Mr. Kazu Hayashida is the Director of the Department of Transportation for the State of Hawaii. The mailing address for the Department of Transportation is 869 Punchbowl Street, Honolulu, Hawaii 96813-5097.

1.3 Identification of Approving Agency

In accordance with Chapter 343, HRS, the Governor, or an authorized representative, is the appropriate accepting authority of the Environmental Assessment. The State Department of Transportation will be the approving agency for this Environmental Assessment

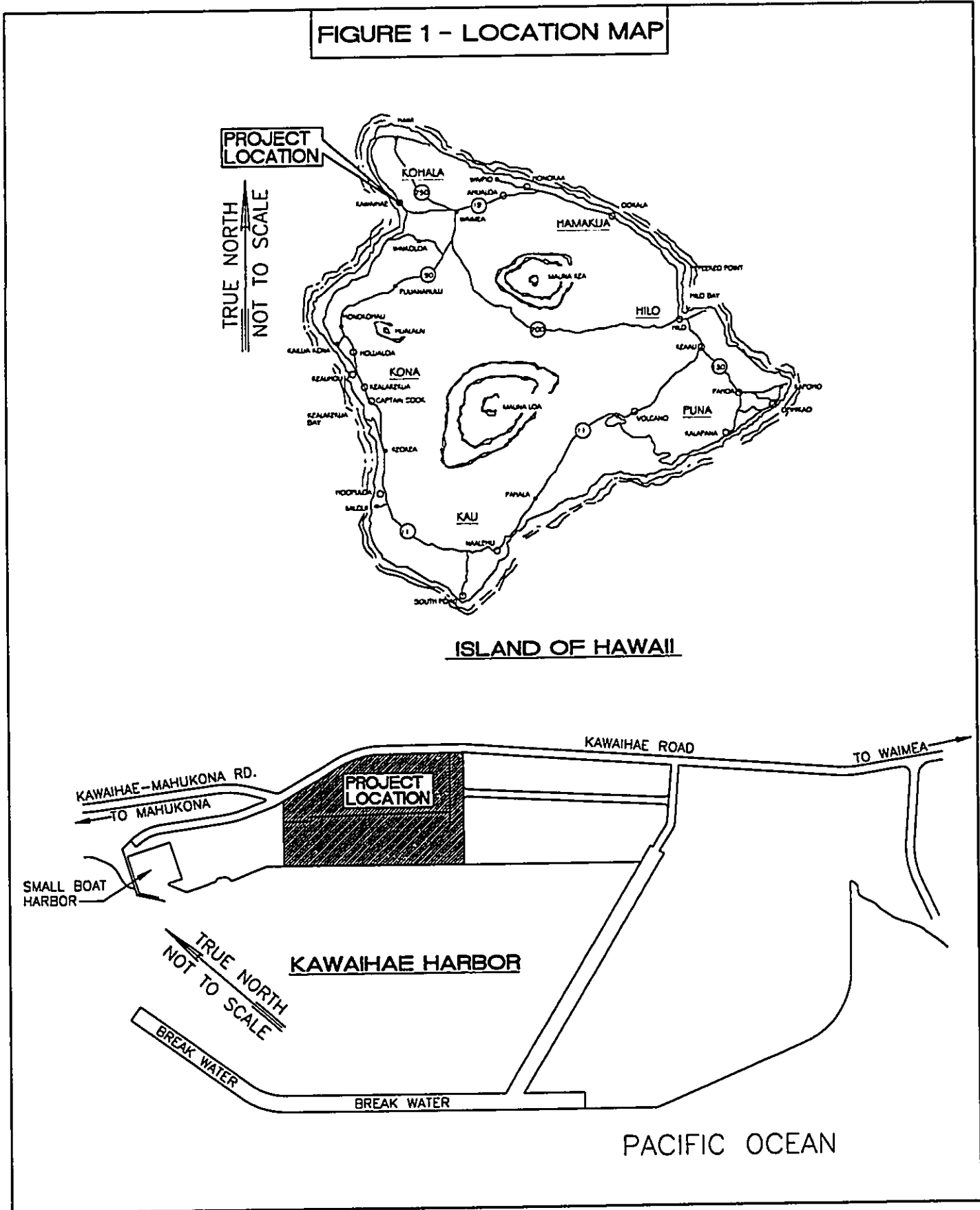
1.4 Technical Description

The proposed project involves the paving of an area formerly occupied by Kawaihae Terminals at Kawaihae Harbor, Kawaihae 1st, South Kohala, Hawaii, TMK: (3) 6-1-03: Portion of 24. (See Figure 1 - Location Map and Figure 2 - Site Plan) The work includes demolition and removal of existing asphalt concrete pavements, grading and compaction, installation of base course, portland-cement concrete (PCC) paving and striping. Additional improvements include a chain link fence, waterline for fire protection, area lighting, communications and drainage system. The total improved area for the proposed project is approximately 7.1 acres of land. Upon completion of the improvements, the project area will be utilized by Young Brothers for inter-island barge operations.

As part of the project's scope of work, future infrastructure demands were estimated based on potential uses of the unimproved 3.8-acre site. (see attached Figure 3) This information was used to size the pipes and conduits under the new pavement to minimize replacement if and when improvements are planned for the site.

The proposed project is being developed under the guidance of the State Department of

FIGURE 1 - LOCATION MAP



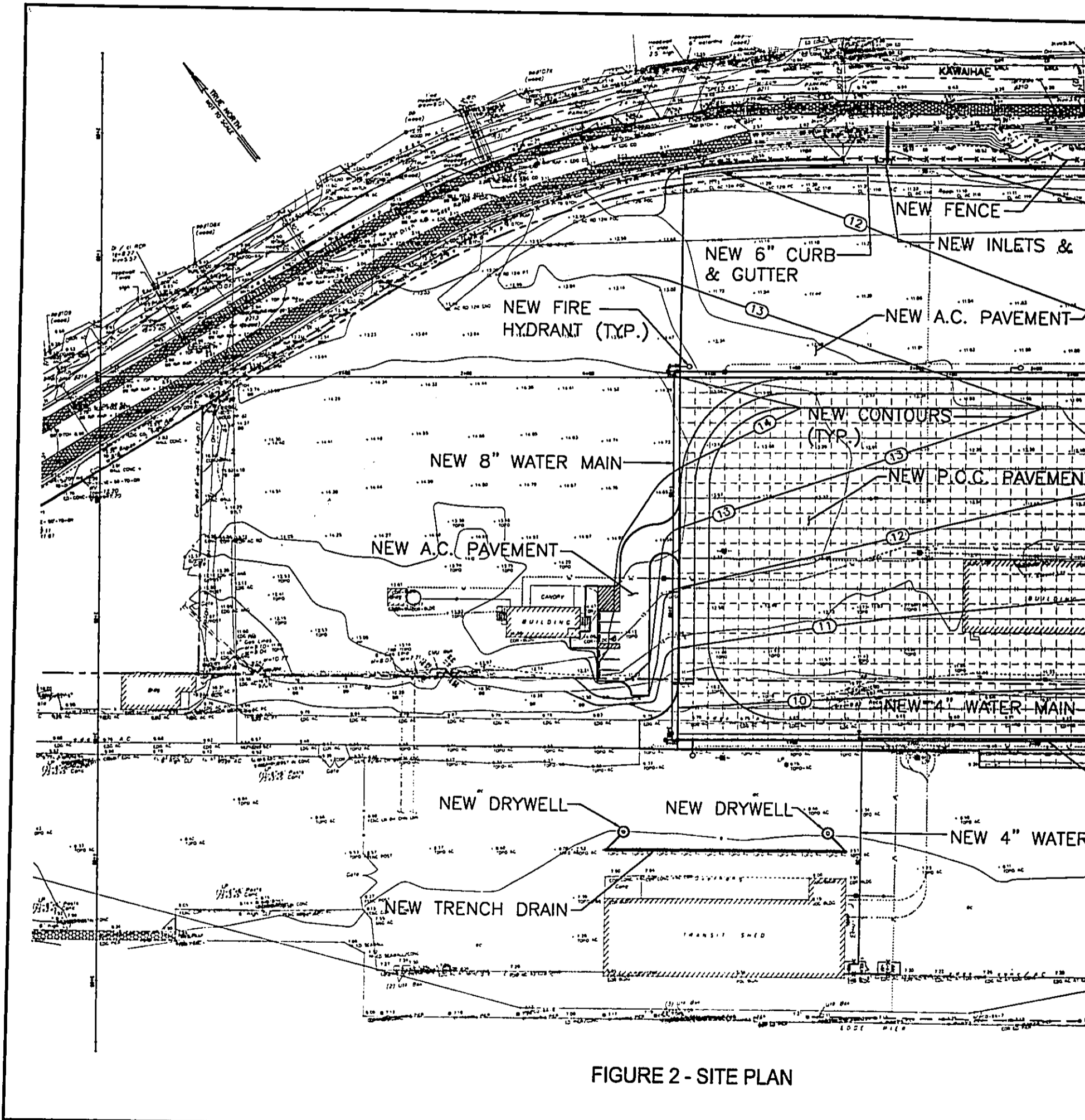


FIGURE 2 - SITE PLAN

Transportation, Harbors Division. The contact person for the subject project is Mr. Napoleon Agraan, phone number (808) 587-1956.

1.5 Project Background

1.5.1 Need for the Project

The proposed project is part of the planned improvements to Kawaihae Harbor identified in the "Hawaii Commercial Harbors 2020 Master Plan" prepared for the State Department of Transportation, Harbors Division in August, 1998. The objectives of the Master Plan are:

- To plan the proper development of Hilo and Kawaihae harbors, thereby facilitating maritime shipments of the essential commodities required by Hawaii's citizenry;
- To optimize the utilization of land and water resources committed to maritime cargo, passenger and fishing operations in an economically responsible manner;
- To provide terminals, other harbor resources and access to these facilities in locations within Hilo Bay, Kawaihae Bay and other sites in a manner that best relates to and serves Hawaii's port system in an efficient, safe and secure manner;
- To minimize the impact on environmental quality and recreational opportunities contiguous with Hawaii's port facilities.

The State of Hawaii is dependent on the ocean shipping industry to transport essential commodities including food, clothing, fuel, building materials and automobiles as well as local products such as pineapple, sugar, molasses, livestock, and diversified agriculture to and from the neighbor islands, the mainland and foreign countries. Hawaii imports 80% of its food and merchandise and of this total, 98.6% is shipped by sea. Kawaihae Harbor provides maritime access for the rapidly growing communities on the west side of the Big Island. The proposed improvements will provide additional open storage facilities to support the growing volume of inter-island cargo at Kawaihae Harbor.

1.5.2 Land Use Designations

The subject property is situated within the State Land Use Urban District. The County General Plan Land Use Pattern Allocation Guide Map (LUPAG) designation for Kawaihae Harbor, including the project area, is Industrial. The County zoning designation for the project area is General Industrial (MG-1 acre). The project area is also situated within the County's Special Management Area (SMA).

The proposed project is consistent with all of the land use designations and regulations of

the State and County of Hawaii. In accordance with the provisions of Hawaii Revised Statutes (HRS) Section 266-2(b), the Department of Transportation, Harbors Division, has determined that the project is exempt from county permit approval.

1.5.3 Listing of Permits and Approvals

The following list of permits and approvals may be required for the proposed project:

State of Hawaii	
Department of Transportation	Approval-Project Construction Plans
Department of Health	NPDES Stormwater Discharge Permit NPDES Hydrotesting Permit
County of Hawaii	
Department of Water Supply	Approval-Project Construction Plans

1.6 Agency and Public Consultation

The following public and private organizations were consulted during the preparation of this environmental assessment:

United States Fish and Wildlife Services, Division of Ecological Services
United States Department of the Army, U.S. Army Engineer District, Honolulu
State of Hawaii, Department of Land and Natural Resources, Historic Preservation
Division and Division of Forestry and Wildlife
State of Hawaii, Department of Health
State of Hawaii, Department of Transportation
State of Hawaii, Department of Hawaiian Home Lands
State of Hawaii, Office of Hawaiian Affairs
County of Hawaii, Planning Department
County of Hawaii, Department of Public Works
County of Hawaii, Department of Water Supply
County of Hawaii, Police Department
County of Hawaii, Fire Department

2. ENVIRONMENTAL SETTING

2.1 Physical Environment

2.1.1 Geology and Hazards

Environmental Setting

The project area is located on the lower northwestern slopes of Mauna Kea and consists of the Hamakua volcanic series (Sterns and Macdonald, 1946). The Hamakua volcanic series covered the southern portion of the Pololu and Hawi volcanic series during the early volcanic activity of Mauna Kea. These lava flows consisted mainly of basaltic lava flows.

The volcanic hazard as assessed by the United States Geological Survey for the project area is "8" on a scale of ascending risk 9 to 1 (Heliker 1990). Zone "8" includes the lower slopes of Mauna Kea, most of which has not been affected by lava flows for the past 10,000 years.

The entire island of Hawaii is in earthquake zone 3 of the Uniform Building Code which establishes structural design standards for earthquake resistance for certain types of buildings. This zone is prone to major damages from potential earthquake activity.

The project area is situated within the mandatory tsunami evacuation area. The project site, however, is not within the Coastal High Hazard Area (VE) designation of the National Flood Insurance Rate Map (FIRM).

Impacts and Mitigation Measures

The proposed project will not expose harbor activities and structures to any additional hazard risk that does not already exist. The volcanic hazard risk is relatively low and the site is not within the Coastal High Hazard Area (VE) designation of the National Flood Insurance Rate Map (FIRM). All construction activity will be in compliance with current code requirements.

2.1.2 Soils

Environmental Setting

The soils of the general area consist of the Kawaihae series, as classified by the U.S. Department of Agriculture Soil Conservation Service (SCS) Soil Survey (U. S. Soil Conservation Service 1973). The Kawaihae series consists of somewhat excessively

drained, extremely stony soils that formed in volcanic ash. In a representative profile the surface layer is dark reddish-brown extremely stony very fine sandy loam over pahoehoe lava bedrock. Topsoil depth is 2 inches, subsoil depth is 33 inches and the bedrock depth is 20-40 inches. Permeability is moderate, runoff is medium, and the erosion hazard is moderate. It should be noted, however, that the project area consists of coral fill material which was dredged during construction of the harbor basin in 1959.

Impacts and Mitigation Measures

The project area is situated on coral fill material which has been graded and is relatively flat. As such, the potential for soil erosion is negligible. In addition, all construction activities will comply with the applicable requirements of the State Department of Health and the Department of Public Works.

2.1.3 Climate

Environmental Setting

The project area is situated along the northwest coast of the Island of Hawaii which is characterized as having a dry, arid climate. The annual rainfall along this leeward coast is approximately 10 inches. More than 75% of the annual rainfall occurs during the six month period between October and March.

The mean annual temperature is 77 degrees Fahrenheit. Northeasterly trade winds predominate most of the year although strong southwesterly winds during the winter generate much of the rainfall. The wind patterns are also marked by a strong diurnal shift, from daytime on-shore winds to nighttime off-shore winds.

Impacts and Mitigation Measures

The strong wind patterns of the area are a concern for dust control particularly during the period of construction. The contractor will be instructed to utilize best management practices to minimize dust impact. The paving of the project area will have a long term positive impact by reducing the area of exposed surfaces which are potential dust problems.

2.1.4 Hydrology and Drainage

Environmental Setting

The project area is within the Waimea aquifer system which has a sustainable yield of approximately 24 million gallons per day. In fiscal year 1993-94 daily withdrawals from

the Waimea aquifer system averaged approximately 2.8 million gallons per day.

According to the Flood Insurance Rate Map (FIRM) prepared by the Federal Emergency Management Agency dated September 18, 1988, most of the project area is situated within Flood Zone "X" (areas determined to be outside the 500 year flood plain). A portion of the property, closest to the Kawaihae Road, however, is within the shaded Zone "X" designation (Other Flood Areas), defined as areas of 500 year flood, areas of 100-year flood with average depths of less than 1 foot or with drainage areas less than 1 square mile.

Impacts and Mitigation Measures

A portion of the property, closest to the Kawaihae Road, is within the shaded Zone "X" designation (Other Flood Areas), defined as areas of 500 year flood, areas of 100-year flood with average depths of less than 1 foot or with drainage areas less than 1 square mile. The additional runoff generated by the proposed project will be disposed of by a combination of sheet flow into the ocean and drywells complying with all necessary governmental requirements. The makai portion of this project will direct storm water drainage by means of sheet flow directly to the ocean. This is the most cost-effective method and does not require the construction of inlets, piping, drywells or discharge culverts in this area. A portion of this sheet flow drainage will need to be intercepted by a trench drain and drywells in front of the Overseas Terminal Building. The mauka portion of this project will direct storm water along new curbs to new drain inlets. The water will then be discharged through new reinforced concrete pipes into the existing drainage channel.

2.1.5 Water Quality

Environmental Setting

The offshore waters of the Kawaihae area are classified by State Department of Health administrative rules as Class A waters. The objective of Class A waters is to have "their use for recreational purposes and aesthetic enjoyment be protected." The waters within Kawaihae Harbor and the small boat harbor are classified "B" to be protected for small boat harbors, commercial and industrial shipping, bait fishing, compatible recreation, and the support and propagation of aquatic life, and aesthetic enjoyment.

Four gulches including Pohaukole, Makehua, Makahuna and Kawaihae cross under the Kawaihae Road in the vicinity of the project area. These gulches carry storm runoff but remain dry for most of the year.

Impacts and Mitigation Measures

The State Department of Health has indicated that the proposed project may require state permits involving water quality/protection and discharges to waters of the State. The design engineer and/or the contractor will obtain all necessary permits to comply with all requirements regarding water quality. The contractor will be required to utilize best management practices to minimize any potential impacts.

2.1.6 Flora and Fauna

Environmental Setting

The project area is part of the state's Kawaihae Harbor complex which consists of fill material dredged during the construction of the harbor. The entire area has been previously disturbed during construction of the harbor as well as the ongoing harbor activities. There is no vegetation on the project site aside from a few landscaping plants. As such, it is highly unlikely that any listed, candidate or proposed endangered plant species are present in the project area. Vegetation of the surrounding area consists principally of introduced exotics including kiawe (*Prosopis pallida*) and buffel grass (*Cenchrus ciliaris*). (Hawaii County Department of Water Supply, 1995)

In terms of fauna, there are two federally endangered, threatened or candidate species that are known to be present in the area and they are the Hawaiian hawk (*Buteo solitarius*) and the Hawaiian hoary bat (*Lasiurus cinereus semotus*). Native birds known to the area include the golden plover (*Pluvialis dominica*), ruddy turnstone (*Arenaria interpres*) and the puco (*Asio flammeus*). (Hawaii County Department of Water Supply, 1995) The proposed project will not have any significant impact on these protected or native species since the project is restricted to the previously disturbed harbor area.

Introduced mammals expected to be found in the project area include mongoose, feral cats and dogs and several varieties of mice and rats. Introduced birds common to the area include the yellow billed cardinal, rock dove, zebra dove, sparrow, black francolin, gray francolin, myna, and sandgrouse.

Impacts

The proposed project will not have any significant impact on any protected or native species since the project is restricted to a previously disturbed area with ongoing harbor activities.

2.1.7 Air Quality

Environmental Setting

The air quality of the subject area is affected by pollutants derived from the volcanic emissions from the ongoing Kilauea eruption. Other sources of air pollutants to a limited degree include vehicle emissions and dust from agricultural, industrial and construction activities from adjacent property, harbor activities and vehicles utilizing the Kawaihae Road. Vehicle exhaust emissions along the Kawaihae Road will normally dissipate quickly due to the brisk winds that prevail most of the year.

Impacts and Mitigation Measures

Short term impacts will result from the proposed construction activity including dust and exhaust from machinery involved in the installation of the project improvements. Given the relative short construction time period and the sparse development of surrounding properties, the potential impacts of these construction activities should be minimal. In addition, the contractor will be instructed to utilize best management practices to minimize dust impact.

The paving of the project area will have a long term positive impact by reducing the area of exposed surfaces which are potential dust problems.

2.1.8 Noise

Environmental Setting

Existing noise levels are typical of a commercial harbor complex which is affected by operating equipment, machinery and truck traffic.

Impacts and Mitigation Measures

Temporary noise impacts will occur from construction activities of the project improvements and are unavoidable. Mitigation measures can be taken, however, to minimize noise impacts including the use of mufflers and implementing construction curfew periods. State Department of Health regulations must be adhered to during construction.

2.1.9 Scenic Resources

Environmental Setting

The South Kohala district of the Big Island is blessed with wide open vistas of the mountains and the sea. Much of the district is undeveloped except for small pockets of urban concentration at Kawaihae, Waimea, Waikoloa and the resort destination areas along the coast.

Impacts and Mitigation Measures

The open space and scenic vistas of the South Kohala district and the Kawaihae area in particular will not be adversely affected by the proposed improvements to the Kawaihae Harbor facility.

2.2 Social, Cultural and Economic Setting

2.2.1 Socio-Economic Characteristics

Setting

The South Kohala district has the fastest population growth rate of all the districts on the island. Between 1970 and 1980 the district population increased by 99% and between 1980 and 1990 it increased another 98%. Despite this significant population growth, the South Kohala district is still largely undeveloped and has a low density, rural character with a 1990 population of approximately 9,140 residing within a total of 175,100 acres. Most of the residents in the South Kohala district reside in the town of Waimea, Waikoloa Village or within the resort areas along the coast.

The major economic activity for the South Kohala district is the tourism industry led by the three major destination resort complexes of the Mauna Kea Resort, Mauna Lani Resort and Waikoloa Beach Resort. By 1994, these major resorts generated approximately 5,000 jobs for the region. Other economic activities include diversified agriculture which consists of truck farming, flowers and nursery products; ranching; retail and service businesses; and industrial based businesses centered around Kawaihae Harbor.

Impacts

The proposed paving of additional barge terminal area at Kawaihae Harbor will facilitate maritime access for the commerce required by the Big Island's growing western communities. These improvements will help to relieve congestion and improve the

movement of essential commodities (food, clothing, building materials, automobiles, fuel) for the growing businesses and industries. These harbor improvements, in and of itself, will not generate growth, but provides the necessary infrastructure to support and sustain the primary economic activity of the region.

2.2.2 Adjacent Land Uses

Existing Setting

The project area is situated in a sparsely populated section in the northwestern section of the South Kohala district. The Kawaihae Harbor and its industrial, recreational and commercial sport fishing activities is the predominant land use in the project area. There is a small commercial complex with restaurants, convenience store, gas station, and marine related businesses within the Kawaihae urban corridor. The Department of Hawaiian Home Lands has a partially developed 90 acre industrial park, and a developing residential lot subdivision north of the project area. Other land uses in the vicinity include the Puukohola National Historic Site, several single family dwellings and vacant land.

Impacts

The proposed harbor improvements will not have any significant impact on adjacent land uses. The proposed project will provide new open spaces for the existing Young Brothers' (YB) inter-island barge operations, presently operating at Pier 1, Kawaihae Harbor. The inter-island cargo operations have been ongoing for over thirty years with all of the cargo either originating or destined for harbors within Hawaii. YB's entire operations will be moved upon completion of this project, thus, the proposed improvements will not create additional harbor activities which have the potential to increase any adverse impacts, including the introduction of alien pest species, to the island of Hawaii.

2.3 Public Facilities and Services

2.3.1 Roads and Traffic Circulation

Existing Setting

The Kawaihae Road has a pavement width of 22 feet within a 60 feet wide road right-of-way. It is the primary roadway link between Waimea and Kawaihae and runs in an east-west direction. According to the Traffic Assessment report for the Kohala Ranch Project IV by the Traffic Management Consultant (October 1990), the capacity of

Kawaihae Road was estimated to be 2,140 vehicles per hour for both directions.

The inter-island barge terminal area handles approximately 9,135 containers and approximately 9,135 heavy trucks (HS20-44) per year.

Impacts

The proposed improvements will not have any significant adverse impact on the roads and traffic circulation in the area. A new left turn lane into the Matson yard will improve traffic flow by allowing thru-vehicles to pass by the "backed-up" Matson trucks. The new fencing, gates and lane striping will also improve overall traffic flow patterns and improve the security of the site.

2.3.2 Water System

Existing Setting

The project area is served by the County's Lalamilo Water System (I.D. No. 160) which extends from the Mauna Lani Resort at the southern end to the Department of Hawaiian Home Land's industrial subdivision on the northern end. This system is supplied with ground water from four wells on state land at the 1,200 foot elevation in Lalamilo. The Lalamilo wells have an effective capacity of 3.9 million gallons/day (mgd), of which, the Mauna Kea Beach and Mauna Lani Resorts have a commitment for 1.4 mgd. In 1994, the average daily pumpage was approximately 2.8 mgd. (Hawaii County Department of Water Supply, 1995)

There is one additional well, currently under construction with private funds from the Mauna Lani Resort, on property south of the existing Lalamilo well field. When completed, this well will have a maximum capacity of 1.8 mgd, however, only 10% of the capacity (0.180 mgd) will be made available to the DWS.

There is an existing 6 inch waterline within the Kawaihae Road right-of-way serving the project area. The Department of Water Supply will be replacing the existing line with a new 12 inch waterline in the near future. The new line is intended to improve the water service of the existing customers of the Lalamilo Water System but will have no effect on the existing DWS water availability policy for the Lalamilo Water System which is to allow one meter for each existing lot. The Lalamilo Water System will not have any additional water to accommodate requests for change of zones, subdivisions or ohana dwellings.

There are no immediate plans for any other system improvements including source development, transmission or storage. The long range strategy for dealing with the future

water needs of the region have been detailed in the Final Environmental Impact Statement for the Kohala Water Transmission System prepared for the Department of Water Supply, County of Hawaii by Megumi Kon, Inc. in November, 1995.

Impacts

The proposed project will not have any significant impact on the water system serving the area since no new activities are being introduced. Proposed improvements to the harbor water system will make more efficient use of the water provided by the Department of Water Supply.

2.3.3 Protective Services

Existing Setting

The closest County fire station is situated along the Queen Kaahumanu Highway approximately six miles south of the project area. Police protection services are available in the town of Waimea situated approximately 10 miles west of the project area.

Impacts

The proposed project is not likely to create an additional burden on the existing service providers. The proposed improvements to the Young Brother's site would consist of a 12" feed and an 8" loop with hydrants space less than 300' apart. These improvements will substantially improve fire flow protection to the area.

2.3.4 Power and Communication Systems

Setting

The South Kohala coast receives its power through overhead 69 KV and 12 KV lines from the Hawaii Electric Light Company's (HELCO) power plant in Waimea. Telephone service in the region is provided by GTE Hawaiian Tel through a central switching station in Kailua-Kona.

The electrical work for the proposed project will replace the existing overhead electrical distribution system with a new underground system and provide area lighting for the new paved barge terminal area. The new underground electrical system will be provided for HELCO and GTE Hawaiian Tel cabling and will consist of handholes and ductlines, sized to meet both utility company's requirements. Pad mounted transformers and separate handholes and ductlines for lighting circuits will also be provided. Area lighting for the new paved barge terminal area will utilize (16) 45' high steel poles, reinforced

concrete pedestals, stanchions, and energy efficient low pressure sodium floodlights.

Impacts

The proposed project will not have any significant adverse impact on the power and communication systems serving the region.

2.3.5 Wastewater

Setting

The project area is not within the service limits of the County wastewater disposal system. The existing Overseas Terminal Building and the District Office Building at Kawaihae Harbor are connected to their own independent cesspools. The proposed project will not include any modifications to either building or to either wastewater system.

Impacts

The proposed project will not have any adverse impact on the wastewater system at Kawaihae Harbor.

2.4 Archaeology, Historic and Cultural Resources

Setting

The South Kohala district and Kawaihae in particular, had a significant role in Hawaiian history. Writing about Kawaihae Kirch (1985) states:

"The port of Kawaihae, with its sheltered natural harbor, was an important calling point for many of the early European voyagers and traders. For about five years 1790-1794, it was the primary residence of Kamehameha I as he regrouped and prepared his forces for the ultimate conquest of Maui, Moloka'i, and O'ahu. Here at Kawaihae, on a prominent natural rise above the harbor, Kamehameha I constructed (or, most probably, rebuilt and expanded) a massive war temple (Luakini) named Pu'ukohola. It was on the alter of Pu'ukohola in 1791 that Kamehameha offered up the body of his arch-rival and cousin Keoua, as an offering to Kuku'ilimoku, his personal war god."

Also at Kawaihae Kirch (1985) writes:

"...not far from Pu'ukohola, is the house of John Young, the English seaman who served for many years as an advisor to Kamehameha. The site, with its stone and lime-plaster

structures, reflects a merger of sorts between the British background of Young and his adopted Hawaiian culture. The Young house site is surely one of the most important archaeological sites of the historic period in Hawai'i."

The Puukohola heiau, Mailekini heiau (a smaller associated heiau) and the John Young homestead have been declared a National Historic Site and have been included in a national park concept plan that was prepared by the U.S. Department of the Interior's National Park Service. In addition, archaeological surveys conducted for the Department of Hawaiian Home Lands have identified burial sites on property adjacent to the Kawaihae Road corridor. (Hawaii State Department of Hawaiian Home Lands, 1992)

Impacts

Preliminary comments submitted by the Hawaii State Department of Land and Natural Resources, State Historic Preservation Division stated in part, the following:

"We believe that the proposed project, which will consist of paving an area formerly occupied by Kawaihae Terminals, will have 'no effect' on significant historic sites because of the previous ground disturbance in the area. It is highly unlikely that any significant historic sites exist in this area because of the previous work related to the development of the harbor facilities."

3. SUMMARY OF POTENTIAL ADVERSE ENVIRONMENTAL IMPACTS AND PROPOSED MITIGATION MEASURES

3.1 Short Term Impacts

Construction Activity:

Impacts: Short term impacts will result from the proposed construction activity including increased noise levels, dust and exhaust from machinery involved in the installation of the project improvements. Other temporary impacts include traffic disruption, the storage/stockpiling of material and equipment during the time of construction. Given the relative short construction time period and the sparse development of surrounding properties, the potential impacts of these construction activities should be minimal.

Mitigation: The contractor will be instructed to utilize best management practices to minimize all impacts including the use of mufflers and implementing construction curfew periods. State Department of Health regulations must be adhered to during construction. Provisions will be made to handle traffic control as well as the storage of equipment and supplies in compliance with all regulatory requirements.

3.2 Long Term Impacts

Flood Hazard:

Impacts: A portion of the property, closest to the Kawaihae Road, is within the shaded Zone "X" designation (Other Flood Areas), defined as areas of 500 year flood, areas of 100-year flood with average depths of less than 1 foot or with drainage areas less than 1 square mile.

Mitigation: The additional runoff generated by the proposed project will be disposed of by a combination of sheet flow into the ocean and drywells in compliance with all necessary governmental requirements.

Water Quality:

Impacts: The State Department of Health has indicated that the proposed project may require state permits involving water quality/protection and discharges to waters of the State.

Mitigation: The design engineer and/or the contractor will obtain all necessary permits to comply with all requirements regarding water quality. The contractor will be required to utilize best management practices to minimize any potential impacts.

Socio-Economic:

Impacts: The proposed paving of additional barge terminal area at Kawaihae Harbor will facilitate maritime access for the commerce required by the Big Island's growing western communities. These improvements will help to relieve congestion and improve the movement of essential commodities (food, clothing, building materials, automobiles, fuel) for the growing businesses and industries. These harbor improvements will provide necessary infrastructure to support and sustain economic activity in the region. The paving of the project area will also have a long term positive impact on dust control by reducing the area of exposed surfaces which are potential dust problems.

Waterline and fire hydrant improvements will have a long-term positive benefit of substantially improving fire flow protection and making more efficient use of water. Lighting and fencing improvements will also improve security for the area.

Mitigation: The socio-economic impacts of the project are essentially beneficial and require no mitigation.

Area Lighting:

Impacts: The proposed project includes area lighting for the open storage area to be utilized by Young Brothers' (YB) inter-island cargo operations. The additional lighting system has the potential for spillover, glare, and possible attraction to wildlife.

Mitigation: The proposed project will utilize full cutoff low pressure sodium light fixtures that are shielded and will direct all of the lights downwards. In addition, the lighting control system has been designed to allow users to turn off most of the lights when the paved barge terminal area is not in use, although some of the lights will remain in operation to provide security lighting. These provisions are consistent with the guidelines included in The Newell's Shearwater Light Attraction Problem, A Guide for Architects, Planners and Resort Managers. These measures will mitigate impacts on wildlife, neighboring properties, and the observatories on the island.

4. ALTERNATIVES

4.1 No Action

If the proposed paving and infrastructure improvement project is not implemented, the project area will remain available for harbor support activities. However, the use of the area will be limited due to substandard facilities, lack of a stable surface for storage and machinery operations. In addition, the existing dust problem resulting from the unpaved surface will continue.

4.2 Alternative Sites

The Department of Transportation could utilize alternative sites for the additional barge terminal area at Kawaihae Harbor. However, the impacts generated by using another site within the existing harbor complex would be similar to the proposed site. There would be no apparent advantage for selecting an alternative site while creating additional logistical problems for the barge terminal operations.

5. DETERMINATION, FINDINGS AND REASONS FOR SUPPORTING DETERMINATION

5.1 Significance Criteria

According to the Department of Health Rules (11-200-12), an applicant or agency must determine whether an action may have a significant impact on the environment, including all phases of the project, its expected consequences both primary and secondary, its cumulative impact with other projects, and its short and long-term effects. In making the determination, the Rules establish "Significance Criteria" to be used as a basis for identifying whether significant environmental impact on the environment if it meets any one of the following thirteen criteria.

1. **Involves an irrevocable commitment to loss or destruction of any natural or cultural resources.**

The proposed project involves the installation of pavement and infrastructure improvements for a barge terminal area at the existing Kawaihae Harbor complex. As such, the proposed project will not have any impact on the visual character or scenic views of the area. In addition, prior land disturbances in constructing the harbor makes it highly unlikely that any natural or cultural resources remain intact within the right-of-way.

2. **Curtails the range of beneficial uses of the environment.**

The proposed barge terminal improvements are situated entirely within the existing Kawaihae Harbor complex. The project area has already been disturbed and will be utilized for the intended purpose of supporting harbor activities.

3. **Conflicts with the State's long-term environmental policies or goals and guidelines as expressed in Chapter 344, HRS; and any revisions thereof and amendments thereto, court decisions, or executive orders.**

The proposed development is consistent with the Environmental Policies established in Chapter 344, HRS, and the National Environmental Policy Act.

4. **Substantially affects the economic or social welfare of the community or state.**

The proposed project will have a substantial impact on the economic and social welfare of the community by increasing the barge handling capacity of the Kawaihae Harbor facility. Hawaii imports 80% of its food and merchandise and of this total, 98.6% is shipped by sea. Kawaihae Harbor provides maritime access for the rapidly growing communities on the west side of the Big Island. The proposed improvements will

provide additional open storage facilities to support the growing volume of inter-island cargo at Kawaihae Harbor.

5. Substantially affects public health

The proposed project will have a positive impact on public health by reducing the existing dust problems at the harbor by increasing the paved surface area of the facility.

6. Involves substantial secondary impacts, such as population changes or effects on public facilities.

The proposed project will not have any substantial secondary impacts because it is responding to an increase in cargo volume that already exists. Harbor facility improvements provide support to the State's economy by addressing the demand for shipping. The demand for shipping is determined by the local economy and not the other way around.

7. Involves a substantial degradation of environmental quality.

The proposed project is situated entirely within the previously disturbed Kawaihae Harbor complex. As such, the barge terminal improvements will not involve a substantial degradation of environmental quality.

8. Is individually limited but cumulatively has considerable effect on the environment, or involves a commitment for larger actions.

As stated previously, the proposed project will not have any substantial secondary impacts because it is not a generator of growth. The proposed project does not involve a commitment for larger actions and will not induce other actions having a cumulative effect on the environment.

9. Substantially affects a rare, threatened or endangered species or its habitat.

The proposed project will not have any substantial adverse effect on any rare, threatened or endangered species or its habitat.

10. Detrimentally affects air or water quality or ambient noise levels.

Short term impacts will result from the proposed construction activity including increased noise levels, dust and exhaust from machinery involved in the installation of the pavement and infrastructure. Given the relative short construction time period and the sparse development of surrounding properties, the potential impacts of these construction

activities should be minimal.

- 11. Affects or is likely to suffer damage by being located in an environmentally sensitive area, such as a flood plain, tsunami zone, beach, erosion-prone area, geologically hazardous land, estuary, freshwater, or coastal waters.**

A portion of the project area is situated within the shaded Zone "X" designation and considered to be a potential flood hazard area. The additional runoff generated by the proposed project will be disposed of by a combination of sheet flow into the ocean and drywells meeting with the approval of all necessary governmental requirements.

- 12. Substantially affects scenic vistas and view planes identified in county or state plans or studies.**

The proposed project involves paving and the installation of utilities in an area already utilized for harbor activities. The proposed project is not introducing a new use or activity into the area. As such, the proposed project will not have any impact on scenic vistas and view planes of the area.

- 13. Requires substantial energy consumption.**

The proposed project will not require substantial energy consumption.

5.2 Findings

Based on the foregoing information presented, it is determined that the proposed landside improvements to the Kawaihae Harbor facility will not have a significant effect. As such, a determination of a Finding of No Significant Impact for the proposed action is appropriate.

5.3 Reasons Supporting Determination

The nature and scale of the proposed action within the previously disturbed harbor facility is such that no significant environmental effects are anticipated. Potential impacts, if any, can be mitigated through careful construction management practices and compliance with all governmental requirements including those of the State Department of Health and State Department of Transportation.

REFERENCES

- Hawaii County Department of Water Supply by Megumi Kon Inc. *Kohala Water Transmission System, North and South Kohala Districts, Island of Hawaii Final Environmental Impact Statement* November 1995.
- Hawaii State Department of Hawaiian Home Lands by RM Towill Corp. *Kawaihae Ten-Year Master Plan, Final Environmental Impact Statement*. December 1992.
- Hawaii State Department of Transportation, Harbors Division. *Hawaii Commercial Harbors, 2020 Master Plan*. August, 1998
- Heliker, C. 1990. *Volcanic and Seismic Hazards on the Island of Hawaii*. Washington: GPO
- Kirch, P.V. 1985. *Feathered Gods and Fishhooks*. An Introduction to Hawaiian Archaeology and Prehistory. University of Hawaii Press, Honolulu.
- U.S. Soil Conservation Service. 1973. *Soil Survey of the Island of Hawaii, State of Hawaii*. Washington: U.S.D.A.
- Stearns, H.T. and Macdonald G.A. 1946. *Geology and Ground-Water Resources of the Island of Hawaii*. Bulletin 9 Hawaii Division of Hydrography. Advertiser Publishing Co., Ltd. Honolulu.
- Traffic Management Consultant. 1990. *Traffic Assessment Report for the Kohala Ranch Project IV*. Honolulu

**APPENDIX A - REPRODUCTION OF COMMENTS AND RESPONSES MADE
DURING THE PRE-ASSESSMENT CONSULTATION PERIOD**

1. Department of the Army, U.S. Army Engineer District, Honolulu, January 29, 1998.
2. State of Hawaii, Department of Land and Natural Resources, Division of Forestry and Wildlife, October 9, 1998.
3. State of Hawaii, Department of Land and Natural Resources, Historic Preservation Division, November 20, 1998.
4. State of Hawaii, Department of Health, November 24, 1998.
5. State of Hawaii, Hawaiian Homes Commission, October 12, 1998.
6. State of Hawaii, Office of Hawaiian Affairs, November 10, 1998.
7. County of Hawaii, Police Department, October 14, 1998.
8. County of Hawaii, Fire Department, October 13, 1998.
9. County of Hawaii, Planning Department, November 6, 1998.
Response: Kazu Hayashida, December 15, 1998

02/05/99 09:45 FAX 808 329 7457
02/03/99 13:42 FAX 8085244488

WRSA KONA
SEGAWA & ASSOC HNL

→ B. NISHIMURA
→ SEGAWA KONA

001
001

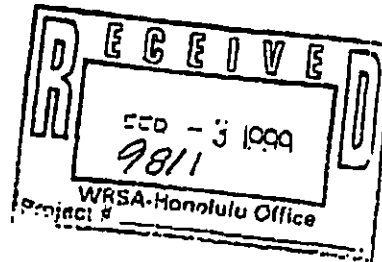


REPLY TO
ATTENTION OF

DEPARTMENT OF THE ARMY
U.S. ARMY ENGINEER DISTRICT, HONOLULU
FORT SHAFTER, HAWAII 96858-5440

COPY

January 29, 1999



Operations Branch

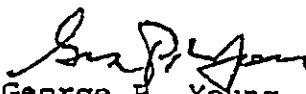
Mr: Neal Fukumoto, P.E.
Welsey R. Segawa & Associates, Inc.
736 South Street, Suite 203
Honolulu, Hawaii 96813-5127

Dear Mr. Fukumoto:

This response to your letter dated November 4, 1998, requesting comments on the plans to improve the Kawaihae Harbor Barge Terminal. Based on the information you provided it appears that a Department of the Army permit will not be required for the project.

If you have any questions concerning this determination, please contact Mr. William Lennan of my staff at 438-9258, extension 13, and mention File Number 990000037.

Sincerely


George F. Young, C.E.
Chief, Operations Branch

BENJAMIN J. CAYETANO
GOVERNOR OF HAWAII



MICHAEL D. WILSON
CHAIRPERSON
BOARD OF LAND AND NATURAL RESOURCES

DEPUTY
GILBERT S. COLOMA-AGARAN

STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES
DIVISION OF FORESTRY AND WILDLIFE
1151 PUNCHBOWL STREET
HONOLULU, HAWAII 96813
October 9, 1998

AQUACULTURE DEVELOPMENT
PROGRAM
AQUATIC RESOURCES
BOATING AND OCEAN RECREATION
CONSERVATION AND
ENVIRONMENTAL AFFAIRS
CONSERVATION AND
RESOURCES ENFORCEMENT
CONVEYANCES
FORESTRY AND WILDLIFE
HISTORIC PRESERVATION
LAND MANAGEMENT
STATE PARKS
WATER AND LAND DEVELOPMENT
WATER RESOURCE MANAGEMENT

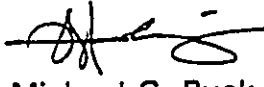
Mr. Brian T. Nishimura
Planning Consultant
101 Aupuni Street, Suite 217
Hilo, Hawaii 96720

Dear Mr. Nishimura:

Subject: Request for Comments: Pre-Environmental Assessment Consultation

We have reviewed the information provided for the State of Hawaii, Department of Transportation, Harbors Division proposed landside improvements to their Kawaihae harbor facilities at Kawaihae 1st, South Kohala, Hawaii. The property is 6.4 acres. The Department of Land and Natural Resources, Division of Forestry and Wildlife has no objections to the proposed improvements to Kawaihae harbor facilities at Kawaihae, South Kohala, Hawaii as indicated by your letter dated October 7, 1998. Furthermore, we anticipate no further notification or consultation on the proposed project.

Sincerely yours,


Michael G. Buck
Administrator

Copy: DOFAW, Hawaii District

BENJAMIN J. CAYETANO
GOVERNOR OF HAWAII



STATE OF HAWAII

DEPARTMENT OF LAND AND NATURAL RESOURCES

HISTORIC PRESERVATION DIVISION
Kakuhikawa Building, Room 555
601 Kamohala Boulevard
Kapolei, Hawaii 96707

MICHAEL D. WILSON, CHAIRPERSON
BOARD OF LAND AND NATURAL RESOURCES

DEPUTIES
GILBERT COLOMA-AGARAN
TIMOTHY E. JOHNS

AQUATIC RESOURCES
BOATING AND OCEAN RECREATION
CONSERVATION AND RESOURCES

ENFORCEMENT
CONVEYANCES
FORESTRY AND WILDLIFE
HISTORIC PRESERVATION
LAND
STATE PARKS
WATER RESOURCE MANAGEMENT

November 20, 1998

Mr. Brian T. Nishimura, Planning Consultant
101 Aupuni Street, Suite 217
Hilo, Hawaii 96720

LOG NO: 22393
DOC NO: 9810PM13

Dear Mr. Nishimura:

**SUBJECT: Pre-Environmental Assessment Consultation for
Proposed Improvements to the Kawaihae Harbor Facilities
Kawaihae 1, South Kohala, Hawaii Island
TMK:**

Thank you for your letter of October 7, 1998 and the opportunity to review and comment on the proposed project.

We believe that the proposed project, which will consist of paving an area formerly occupied by Kawaihae Terminals, will have "no effect" on significant historic sites because of the previous ground disturbance in the area. It is highly unlikely that any significant historic sites exist in this area because of the previous work related to the development of the harbor facilities.

If you have any questions please contact Patrick McCoy (587-0006).

Aloha,

A handwritten signature in black ink, appearing to read "Don Hibbard".

DON HIBBARD, Administrator
State Historic Preservation Division

PM:amk

BENJAMIN J. CAYETANO
GOVERNOR OF HAWAII



LAWRENCE MIKE
DIRECTOR OF HEALTH

STATE OF HAWAII
DEPARTMENT OF HEALTH
P.O. BOX 3378
HONOLULU, HAWAII 96801

In reply, please refer to:

November 24, 1998

98-221/epo

Mr. Brian T. Nishimura
Planning Consultant
101 Aupuni Street, Suite 217
Hilo, Hawaii 96720

Dear Mr. Nishimura:

Subject: Pre-Environmental Assessment Consultation
Landslide Improvements
Kawaihae Harbor, Hawaii

Thank you for allowing us to review and comment on the subject project. We have the following comments to offer:

Safe Drinking Water Branch (SDWB)

Any proposed drywells may be subject to Underground Injection Control (UIC) regulations and processing depending on the drywells construction and design.

Any questions regarding these comments should be directed to Mr. Chauncey Hew of the SDWB at 586-4258.

Office of Solid Waste Management (OSWM)

We would like to advise the applicant of the requirements of Hawaii Revised Statutes, Section 103D-407, as amended by Act 092 of the 1997 Legislature.

"All highway and road construction and improvement projects funded by the State or a county, or roadways that are to be accepted by the State or a county as public roads shall utilize a minimum of ten per cent crushed glass aggregate as specified by the State Department of Transportation in all basecourse (treated or untreated) and subbase when the glass is available to the quarry or contractor at a price no greater than that of the equivalent aggregate."

Mr. Brian T. Nishimura
November 24, 1998
Page 2

98-221/epo

Please contact Mr. Lane Otsu of the OSWM at 586-4240 with any questions concerning these comments.

Sincerely,



BRUCE S. ANDERSON, Ph.D.
Deputy Director for
Environmental Health

c: SDWB
OSWM

BENJAMIN J. CAYETANO
GOVERNOR
STATE OF HAWAII



STATE OF HAWAII
DEPARTMENT OF HAWAIIAN HOME LANDS
P.O. BOX 1879
HONOLULU, HAWAII 96805

KALI WATSON
CHAIRMAN
HAWAIIAN HOMES COMMISSION

JOBIE M. K. M. YAMAGUCHI
DEPUTY TO THE CHAIRMAN

October 12, 1998

Mr. Brian T. Nishimura
Planning Consultant
101 Aupuni Street
Suite 217
Hilo, Hawaii 96720

Dear Mr. Nishimura:

SUBJECT: Pre-Environmental Assessment Consultation

Thank you for notifying the department of the proposed project at the Kawaihae Harbor, Island of Hawaii.

Our initial concern is the potential impact of this project may have in the overall county water system in the area. Please keep us informed of the progress of the draft EA. The department reserves its right to submit comments later when more information is provided.

Should you have any questions, please call Linda Chinn, Land Management Division, at 1-587-6432.

Aloha,

A handwritten signature in cursive script that reads "Kali Watson".

KALI WATSON, Chairman
Hawaiian Homes Commission

PHONE (808) 594-1888

FAX (808) 594-1865



STATE OF HAWAII
OFFICE OF HAWAIIAN AFFAIRS
711 KAPI'OLANI BOULEVARD, SUITE 500
HONOLULU, HAWAII 96813

November 10, 1998

Brian T. Nishimura
Planning consultant
101 Aupuni Street, Suite 217
Hilo, Hawaii 96720

EIS 238

Re: Pre-Environmental Assessment Consultation, State of Hawaii, Department of
Transportation, Harbors Division, Landside Improvements at Kawaihae Harbor
Facilities, South Kohala, Hawaii.

Dear Mr. Nishimura:

Thank you for the opportunity to comment prior to the preparation of an environmental
assessment for the landside improvements proposed for the Kawaihae Harbor facilities in south
Kohala, Island of Hawaii.

At this time, the Office of Hawaiian Affairs has no comments on this project. Please provide us
with a copy of the Draft Environmental Assessment, however, when it is completed.

If you have any questions, please contact Sebastian Aloit, Land and Natural Resources Division
Officer or Lynn Lee, EIS Planner at 594-1936.

Sincerely,

Handwritten signature of Colin Kippen.

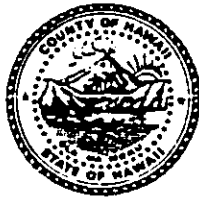
Colin Kippen,
Deputy Administrator

Handwritten signature of Sebastian Aloit.

Sebastian Aloit
Land and Natural Resources Division Officer

cc: Board of Trustees
Kona CAC

Stephen K. Yamashiro
Mayor



Wayne G. Carvalho
Police Chief

James S. Correa
Deputy Police Chief

County of Hawaii
POLICE DEPARTMENT

349 Kapiolani Street • Hilo, Hawaii 96720-3998
(808) 935-3311 • Fax (808) 961-2702

October 14, 1998

Mr. Brian T. Nishimura
Planning Consultant
101 Aupuni Street, Suite 217
Hilo, Hawaii 96720

Dear Mr. Nishimura:

SUBJECT: REQUEST FOR COMMENTS
PRE-ENVIRONMENTAL ASSESSMENT CONSULTATION

This responds to your letter of October 7, 1998, soliciting comments on proposed landside improvements to the Kawaihae Harbor facilities at Kawaihae 1st, South Kohala, Hawaii.

We do not believe the proposed project will negatively impact traffic or public safety in the area.

Thank you for the opportunity to comment.

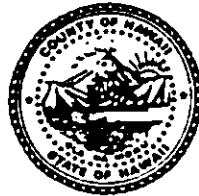
Sincerely,

Handwritten signature of Wayne G. Carvalho in black ink.

WAYNE G. CARVALHO
POLICE CHIEF

JV:lk

Stephen K. Yamashiro
Mayor



Nelson M. Tsuji
Fire Chief

Edward Bumatay
Deputy Fire Chief

County of Hawaii

FIRE DEPARTMENT

777 Kilauea Avenue • Mall Lane • Hilo, Hawaii 96720-4239
(808) 961-8297 • Fax (808) 961-8296

October 13, 1998

Mr. Brian T. Nishimura
Planning Consultant
101 Aupuni Street, Suite 217
Hilo, HI 96720

Dear Mr. Nishimura:

**Subject: Request for Comments
Pre-Environmental Assessment Consultation
State DOT Harbors Division
Kawaihae Harbor Facilities**

We have no comments regarding the above-referenced project.

Sincerely,


NELSON M. TSUJI
Fire Chief

NMT/mo



Stephen K. Yamashiro
Mayor



Virginia Goldstein
Director

Russell Kokubun
Deputy Director

County of Hawaii
PLANNING DEPARTMENT
25 Aupuni Street, Room 109 • Hilo, Hawaii 96720-4252
(808) 961-8288 • Fax (808) 961-8742

November 6, 1998

Mr. Brian T. Nishimura
Planning Consultant
101 Aupuni Street, Suite 217
Hilo, HI 96720

Dear Mr. Nishimura:

Request for Comments: Pre-Environmental Assessment (EA) Consultation
Proposed Pavement Addition for Inter-Island Barge Operations & Terminal Area
State of Hawaii Kawaihae Harbor Facility, South Kohala, Hawaii Island
TMK: 6-1-03: 24

In answer to your request for comments, our pre-EA comments are made pursuant to Hawaii Admin. R. Section 11-20-9(a) as the county agency responsible for implementing the Hawaii County General Plan. Most of our comments pertain to the land use laws under or related to the Planning Department's jurisdiction.

Questions or Additional Information Needed. A review of the preliminary site plan (September 28, 1998) and the cover letter (October 7, 1998) raised the following questions.

1. **TMK Number?** The submitted materials do not state the TMK number of the project site. A comparison of the site plan with the department's TMK maps seems to indicate that **TMK: 6-1-03: 24** (9.137 acres) may be the parcel location of this project. Please clarify and confirm the correct TMK number of this site.

For the purpose of these comments, however, parcel 24 will be the TMK referred to until information is received that identifies the correct number.

2. **Site Plan.** We found that the site plan was a reduced version and therefore not-to-scale as stated on the plan. Our review was able to determine, however, that the reduction a

Mr. Brian T. Nishimura
Planning Consultant
Page 2
November 6, 1998

was at a scale of 1" : 80'. For the public's review, the EA document should provide, if possible, a drawn-to-scale site plan.

3. **Hawaii Revised Statutes Section 266-2(b), Exemption from County Approvals?** Please inform us if this project will be exempt from county agency approval. If this project is exempted under the state law then the following comments do not apply. Until we are informed otherwise, however, these comments are offered with the assumption that the project is not exempt by H.R.S. Section 266-2(b), and therefore, the development would require compliance with all county regulatory requirements.

Discussed below are the County Land Use regulations that this project would be subject to.

4. **SLU (State Land Use): "Urban."** The SLU: "Urban" district signifies that county government is the primary jurisdiction to determine the permitted uses in this district.
5. **County Zoning: General Industrial (MG-1acre).** The proposed inter-island barge operations and terminal area is a permitted use consistent with the County's General Industrial Zoning, Section 25-5-152.

Pursuant to the County's MG zoning, the development of this project would require compliance with standards for yard setbacks, building height limits, front yard landscaping improvements, height limits of perimeter fencing, possible off-street parking, and Final Plan Approval of the overall project.

6. **Hawaii County GP (General Plan) Land Use Designation: Industrial.** The project site's GP designation is Industrial, according to the LUPAG (Land Use Pattern Allocation Guide) Map. A barge operation - terminal area qualifies as industrial development. Furthermore, the proposed improvements will also provide for large storage and transportation facilities. Both uses are consistent with the GP's industrial land use policy.
7. **SMA (Special Management Area) Requirements.** The proposed project site is within the County's SMA zone and will require the completion and review of an SMA assessment application, copy enclosed. Our initial review indicates that this project will require compliance with the Hawaii County Planning Commission SMA Rule 9 regulations. An SMA review requires the completion and submittal of an assessment

Mr. Brian T. Nishimura, Planning Consultant

Page 3

November 6, 1998

application. Furthermore, these materials indicate that the scope of this project may also require an SMA (major) Use Permit from the Planning Commission. If a review of the assessment determines that the development requires an SMA Use Permit then proceedings before the Planning Commission are subject to compliance with public notice and hearing procedures. Should the Planning Commission grant an SMA (major) permit for this project then such a permit is often issued subject to conditions. Accordingly, development of this project will require compliance with the SMA permit and its conditions.

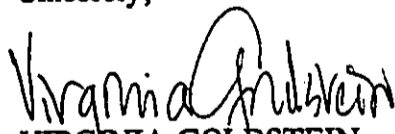
8. **Shoreline Setback (SSB) Requirements: None.** Parcel 24 is not a shoreline lot that abuts the ocean; and therefore, it is not subject to comply with any SSB requirements.

The Department's Rule 11 regulates county SSB requirements. The preliminary site plan confirmed that the inland location of this project site is approximately 153 feet +/- from the southwest edge of the harbor's pier to the new pavement boundary; consequently, the project site's inland distance from the pier edge exceeds the minimum 40 feet SSB requirement.

9. **Coastal Flood Zone?** Please consult with the County Department of Public Works - Engineering Division to determine if parcel 24 may be located in a coastal flood zone. Location in a flood zone may require certain procedures or construction requirements. DPW - Engineering Division, Telephone: 327-3530 (Kona); 961-8327 (Hilo).
10. **Further Notification or Consultation?** Yes, please include the Hawaii County Planning Department for future notifications or consultation requests. In addition, we look forward to receiving the information requested above.

Please contact Earl Lucero of my staff at 961-8288 for any questions or clarification that may be needed on the topics commented on.

Sincerely,


VIRGINIA GOLDSTEIN
Planning Director

EML:pak

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Mr. Brian T. Nishimura, Planning Consultant
Page 4
November 6, 1998

Enclosures: Plan Approval Application
SMA Use Permit Assessment Application

c: Mr. Jiro Sumada, Deputy Chief Engineer
West HI Planning Office
Mr. Thomas Pack, DPW - Engineering Division, Kona Office
Mr. Kazu Hayashida, Director of Transportation,
State of HI - Department of Transportation
869 Punchbowl Street, Honolulu 96813-5097
State DOT-Harbors Division
Mr. Ian Birnie, District Manager - HI District - Port of Hilo
State DOT-Harbors Division

BENJAMIN L. CAYETANO
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

KAZU HAYASHIDA
DIRECTOR

DEPUTY DIRECTORS
GLENN M. OKIMOTO
BRIAN K. MINAII

IN REPLY REFER TO

HAR-ED
4920.99

December 15, 1998

Ms. Virginia Goldstein, Director
Planning Department
County of Hawaii
25 Aupuni Street, Room 109
Hilo, Hawaii 96720-4252

Dear Ms. Goldstein:

Subject: Pave Additional Barge Terminal Area
Kawaihae Harbor, Hawaii - Job H. C. 5313
Pre-Environmental Assessment Consultation

Thank you for your response and comments to our consultant, Mr. Brian Nishimura, dated November 6, 1998, regarding our pre-environmental assessment consultation for the subject project. We will be including the Hawaii Planning Department in future correspondence and offer the following in response to your comments:

1. The Tax Map Key for the project is 6-1-03:24.
2. We will provide a plan drawn to scale with our Draft Environmental Assessment.
3. In accordance with Hawaii Revised Statutes, Section 266-2(b), we are exercising our exemption from the County approvals for this project. We are, however, interested in complying with the codes and requirements to the extent possible and, therefore, request information on the requirements for the County's MG zoning. Also, we will consult with the County Department of Public Works since a portion of the project area is identified as Zone X (areas of 500-year flood) in the Flood Insurance Rate Map.

Ms. Virginia Goldstein
Page 2
December 15, 1998

HAR-ED
4920.99

If you have any questions, please contact our Project Manager, Mr. Napoleon Agraan, on Oahu at (808) 587-1956.

Very truly yours,



KAZU HAYASHIDA
Director of Transportation

c: Mr. Jiro Sumada, Deputy Chief Engineer
Mr. Thomas Pack, DPW- Engineering Division, Kona Office
Mr. Wesley Segawa - Wesley Segawa & Associates, Inc.
Mr. Brian T. Nishimura - Planning Consultant ✓

**APPENDIX B - REPRODUCTION OF COMMENTS AND RESPONSES MADE
DURING THE DRAFT ENVIRONMENTAL ASSESSMENT REVIEW PERIOD**

1. State of Hawaii, Office of Environmental Quality Control, August 5, 1999.
Response: Department of Transportation, September 10, 1999.

BENJAMIN J. CAYETANO
GOVERNOR



GENEVIEVE SALMONSON
DIRECTOR

STATE OF HAWAII
OFFICE OF ENVIRONMENTAL QUALITY CONTROL

235 SOUTH BERETANIA STREET
SUITE 702
HONOLULU, HAWAII 96813
TELEPHONE (808) 588-4186
FACSIMILE (808) 588-4186

August 5, 1999

Mr. Kazu Hayashida, Director
State Department of Transportation
869 Punchbowl Street
Honolulu, Hawaii 96813

Dear Mr. Hayashida:

Subject: Draft Environmental Assessment for Kawaihae Harbor -
Pave Additional Barge Terminal Area, Hawaii

Thank you for the opportunity to review the above project. We have the following questions and comments.


1. According to the environmental assessment, "the project also includes a master plan of approximately 3.8 acres south of the project site." There is no other discussion or evaluation of impacts relating to the master plan south of the project site. Please fully disclose details of the master plan and its associated impacts.
2. The project includes "area lighting." Please describe what specific mitigation measures will be applied to minimize spillover, glare and other lighting impacts? Please consider designing the lighting using the Department of Land and Natural Resources' guidelines entitled The Newell's Shearwater Light Attraction Problem, A Guide for Architects, Planners, and Resort Managers to reduce lighting impacts.
3. These additional harbor activities have the potential to introduce alien pest species into the island Hawaii. The environmental assessment must fully study this issue and propose mitigation measures to minimize this impact. For an example of mitigation measures, please refer to the attached Alien Species Action Plan for Kahului Airport.
4. The environmental assessment does not discuss wastewater issues. Please describe how wastewater is currently disposed and how it will be handled in the future. Please consult the Department of Health to determine whether the

Mr. Hayashida
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existing or future sewage disposal system meets their standards. If not, please correct the problem.

Should you have any questions, please call Jeyan Thirugnanam at 586-4185. Mahalo.

Sincerely,


Genevieve Salmonson
Director

Enclosures

c: Brian Nishimura

BENJAMIN J. CAYETANO
GOVERNOR



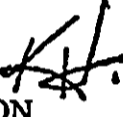
STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
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KAZU HAYASHIDA
DIRECTOR
DEPUTY DIRECTORS
BRIAN K. MINAJI
GLENN M. OKIMOTO

IN REPLY REFER TO:
HAR-ED
6964.00

September 10, 1999

TO: GENEVIEVE SALMONSON, DIRECTOR
OFFICE OF ENVIRONMENTAL QUALITY CONTROL
DEPARTMENT OF HEALTH

FROM: KAZU HAYASHIDA 
DIRECTOR OF TRANSPORTATION

SUBJECT: DRAFT ENVIRONMENTAL ASSESSMENT (EA) FOR PAVE
ADDITIONAL BARGE TERMINAL AREA, KAWAIHAE HARBOR,
HAWAII - JOB H. C. 5313

The following responses are provided to your comments of August 5, 1999 regarding our Draft EA for the subject project.

1. **Master Plan Area:** As part of the project's scope of work, future demands were estimated based on potential uses of the unimproved 3.8-acre site (see attached sheet.) This information was used to size the pipes and conduits under the new pavement to minimize replacement if and when improvements are planned for the site. If proposed developments of the adjacent area arise, environmental impacts will be assessed appropriately. The terminology "Master Plan" is a misnomer and will be revised.
2. **Area Lighting:** In order to mitigate impacts on wildlife, neighboring properties, and the various observatories, the proposed project will utilize full cutoff low pressure sodium light fixtures that are shielded and direct all of the lights downwards. In addition, the lighting control system has been designed to allow users to turn off most of the lights when the paved barge terminal area is not in use, although some of the lights will remain in operation to provide security lighting. These provisions are consistent with the guidelines included in The Newell's Shearwater Light Attraction Problem. A Guide for Architects, Planners, and Resort Managers.
3. **Alien Pest Species:** The proposed project will provide new open spaces for the existing Young Brothers' (YB) inter-island barge operations, presently operating at Pier 1, Kawaihae Harbor. The inter-island cargo operations have been ongoing for over

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thirty years with all of the cargo either originating or destined for harbors within Hawaii. YB's entire operations will be moved upon completion of this project, thus, the proposed improvements will not create additional harbor activities which have the potential to introduce alien species in the Island of Hawaii.

4. Wastewater: The existing Overseas Terminal Building and the District Office Building at Kawaihae Harbor are connected to their own independent cesspools. The proposed project will not include any modifications to either building or to either wastewater system. Note, the above information will be added to our Final EA.

Should you have any questions, please contact Napoleon Agraan of our Harbors Division at 587-1956.

Att.

c: Mr. Wesley Segawa, Wesley R. Segawa and Associates, Inc.
Mr. Brian T. Nishimura, Planning Consultant ✓