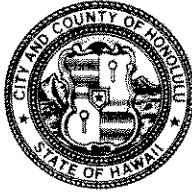


DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU

650 SOUTH KING STREET, 3RD FLOOR • HONOLULU, HAWAII 96813
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MUFI HANNEMANN
MAYOR

EDWARD Y. HIRATA
DIRECTOR

TPD05-00282

June 13, 2005

Ms. Genevieve Salmonson, Director
Office of Environmental Quality Control
235 South Beretania Street, Suite 702
Honolulu, Hawaii 96813

Subject: Finding of No Significant Impact (FONSI) for Waipio Point Access Road
Improvements, Waipahu, Oahu, Hawaii

Dear Ms. Salmonson:

The City and County of Honolulu, Department of Transportation Services has reviewed the comments received during the 30-day public comment period which ended on January 22, 2005. We have determined that this project will not have significant environmental effects and have issued a FONSI. Please publish this notice in the June 23, 2005 OEQC Environmental Notice.

We have enclosed a completed OEQC Publication Form and four copies of the final Environmental Assessment. If you have any questions, please contact Mr. Brian Suzuki, Project Manager, at 527-6880.

Sincerely,

A handwritten signature in cursive script, appearing to read "Edward Y. Hirata".

EDWARD Y. HIRATA
Director

Enclosures

RECEIVED
JUN 13 4:23
OFFICE OF ENVIRONMENTAL
QUALITY CONTROL

2005-06-23 DA FEA WAIPIO POINT ACCESS ROAD
IMPROVEMENTS

JUN 23 2005

WAIPIO POINT ACCESS ROAD Improvements

FINAL ENVIRONMENTAL ASSESSMENT

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OFF. OF ENVIRONMENT/
QUALITY CONTROL

- City and County of Honolulu
Department of Transportation Services
- JUNE 2005

WAIPIO POINT ACCESS ROAD IMPROVEMENTS
Waipahu, Oahu, Hawaii


**Final Environmental Assessment/
Finding of No Significant Impact**

Submitted Pursuant to:

Chapter 343, Hawaii Revised Statutes

City and County of Honolulu
Department of Transportation Services

6/13/05
Date of Approval


Edward Y. Hirata, Director
City and County of Honolulu
Department of Transportation Services

The following person may be contacted for additional information concerning this document:

Mr. Brian Suzuki
City and County of Honolulu
Department of Transportation Services
650 South King Street, 3rd Floor
Honolulu, Hawaii 96813
(808) 527-6880

This report documents environmental impact studies of proposed improvements to Waipio Point Access Road (WPAR) from the Oahu Railway and Land Company (OR&L) Bicycle/Pedestrian Path to the entrance of Waipio Peninsula Soccer Park in Waipahu, Oahu, Hawaii. The proposed project includes providing a paved parking area for NAVSEA Inactive Ships On-Site Maintenance Office (NISMO) staff and contractors; addressing drainage and flooding problems on WPAR and the adjacent NISMO facility; and providing bicycle and pedestrian facilities between the OR&L Bicycle/Pedestrian Path and the entrance to the soccer park. The proposed project is not anticipated to have significant impacts based on criteria specified in Section 11-200-12b of the Hawaii Administrative Rules.

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**WAIPIO POINT ACCESS ROAD
Improvements**

FINAL ENVIRONMENTAL ASSESSMENT

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- Appendix E: Draft Environmental Assessment Comment Letters and Responses
- Appendix F: Waipio Point Access Road Traffic Evaluation

ACRONYM LIST

- AAQS - Ambient Air Quality Standards
- ACHP - Advisory Council on Historic Preservation
- APE - Area of Potential Effect
- AYSO - American Youth Soccer Organization
- BMP - Best Management Practices
- Bridge No. 2 - Waipio Point Access Road Bridge No. 2 over Wailani Drainage Channel
- CatEX - Categorical Exclusion
- CFR - Code of Federal Regulations
- CZM - Coastal Zone Management
- City - City and County of Honolulu
- DA - Department of the Army
- DBEDT - Department of Business and Economic Development
- DES - Department of Enterprise Services
- DLNR - Department of Land and Natural Resources
- DOH - Department of Health
- DPP - Department of Planning and Permitting
- DTS - Department of Transportation Services
- EA - Environmental Assessment
- ESA - Endangered Species Act
- EPA - Environmental Protection Agency
- FCC - Federal Communications Commission
- FHWA - Federal Highways Administration
- FISC - Navy's Fleet and Industrial Supply Center

FIRM – Flood Insurance Rate Map
GOE – Grant of Easement
HAR – Hawaii Administrative Rules
HRS – Hawaii Revised Statutes
LOS – Level of Service
LUO – Land Use Ordinance
NAVMAG – Navy Magazine Facility
NEPA – National Environmental Protection Act
NHPA – National Historic Preservation Act
NISMO – NAVSEA Inactive Ships On-Site Maintenance Office
NMFS – National Marine Fisheries Service
NOx – Nitrogen Oxide
NPDES – National Pollutant Discharge Elimination System
OHA – Office of Hawaiian Affairs
OR&L – Oahu Railway and Land Company
PB – Parsons Brinckerhoff Quade & Douglas
PCB – Polychlorinated Biphenyls
PTSF – Percent Time Spent Following
SDOH – State Department of Health
SDOT – State Department of Transportation
SDWA – Safe Drinking Water Act
SHPD – State Historic Preservation Division
SHPO – State Historic Preservation Officer
SLUC – State Land Use Commission
SMA – Special Management Area
SOBA – Southern Basalt Aquifer

USACE – United States Army Corp of Engineers

USFWS – United States Fish and Wildlife Service

WPAR – Waipio Point Access Road

SUMMARY ■

WAIPIO POINT ACCESS ROAD Improvements

FINAL ENVIRONMENTAL ASSESSMENT

SUMMARY

S.1 INTRODUCTION

The City and County of Honolulu (City), Department of Transportation Services (DTS) is issuing this Final Environmental Assessment (EA) for this project, officially named "Waipio Point Access Road Improvements Study." The project is located on the island of Oahu, Hawaii, and will improve Waipio Point Access Road (WPAR), a two-lane collector road, between the Oahu Railway and Land Company (OR&L) Bicycle/Pedestrian Path and the Waipio Peninsula Soccer Park (see Figure S-1).

This section of WPAR is currently owned by the U.S. Department of the Navy, which operates facilities on the peninsula, including a NAVSEA Inactive Ships On-Site Maintenance Office (NISMO) adjacent to the roadway. In addition to the soccer park and NISMO, the other major land use within the project limits is the city-owned Ted Makalena Golf Course, which is located across WPAR from the NISMO facility. The purpose of the project is to address the conditions of a Grant of Easement (GOE) dated December 2, 1999, between the Navy and the City for use of WPAR as an access to the soccer park.

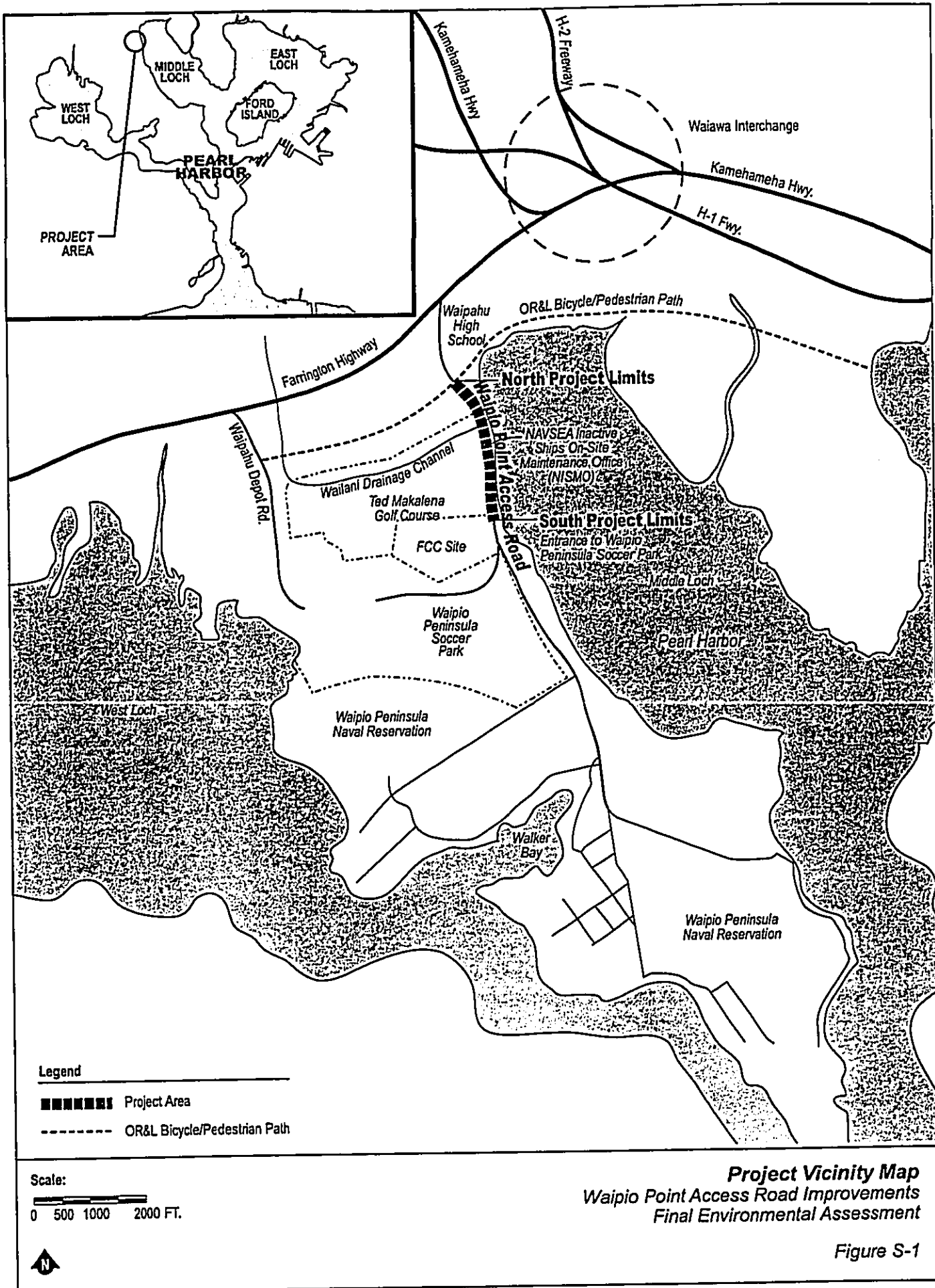
The proposed project will require use of City and Federal Highway Administration (FHWA) funds to design and construct the roadway improvements, triggering the environmental review requirements of Chapter 343 of the Hawaii Revised Statutes (HRS) and the National Environmental Policy Act (NEPA).

As an agency action under HRS Chapter 343, DTS has determined that an EA would be the appropriate environmental review document because the project is unlikely to cause a "significant" impact. As the lead federal agency under NEPA, the FHWA has determined that the project would qualify for a categorical exclusion (CatEx), which means that an environmental review document, such as an EA or EIS, would not be required for NEPA compliance. The Navy indicated that they could recognize another agency's CatEx designation of this project (April 6, 2004 meeting with the Navy).

This Final EA, which was prepared to comply with HRS Chapter 343, and Title 11, Chapter 200 of the Hawaii Administrative Rules, discloses the environmental and social impacts that could result from the project's implementation, and commit to the implementation of specific mitigation measures. It also contains a record of all consultation activities that have been conducted as part of project planning. In addition, it documents compliance with federal laws and regulations because of the proposed use of federal funds administered by the FHWA.

S.2 PURPOSE AND NEED FOR THE PROJECT

In December 1999, the Navy and the City executed the GOE, which granted the City the right to use WPAR for public vehicular access to the recently completed Waipio Peninsula Soccer Park (see Appendix A). However, the easement included the following conditions for the City that is applicable to roadway improvements:



- Parking for naval activities shall be maintained adjacent to the premises and not be reduced;
- The City shall provide reasonable drainage improvements to prevent flooding of the roadway and adjacent naval facilities; and
- The City shall make any necessary improvements to ensure that traffic conditions at intersections along WPAR do not fall below Level of Service (LOS) D (Scale from A to F, representing best to worst, respectively, with no E in the scale).

Because the NISMO, located on the east side of WPAR along Pearl Harbor's Middle Loch, has insufficient space to accommodate its parking requirements, the west side of WPAR, across from the facility's entrance, is used as parking for Navy contractors and visitors. The area provides enough space for approximately 75 vehicles, but is not paved and no stalls are marked.

WPAR's drainage facilities consist of an unlined drainage ditch along the west side of the roadway, adjacent to the fence for the Ted Makalena Golf Course. During heavy rains, the golf course becomes saturated and runoff sheet flows into the drainage ditch. During periods of heavy rainfall, the ditch often is unable to contain the storm waters, causing flooding of the roadway and the NISMO facility.

Based on observations of traffic conditions conducted as part of the traffic evaluation for this project, the normal weekday traffic condition on WPAR is free-flowing with very low volumes. Because normal weekday traffic generation along WPAR is not expected to change dramatically, this traffic condition would continue in the future. Therefore, the last condition above does not require project action.

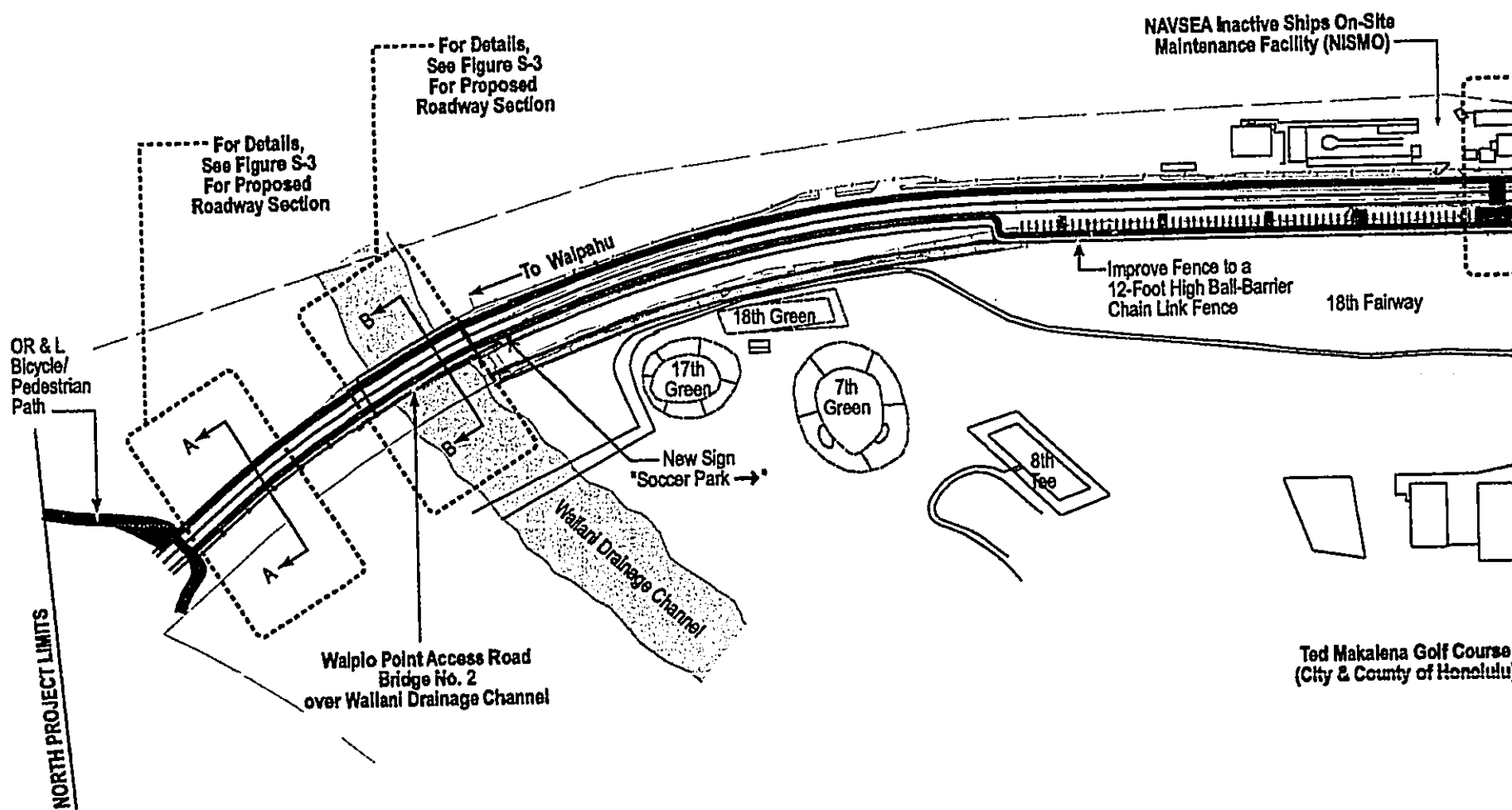
In addition to addressing the GOE conditions, the City wants to provide bicycle and pedestrian access to Waipio Peninsula Soccer Park. Currently, the WPAR has no bicycle facilities and has limited pedestrian facilities.

S.3 PROJECT DESCRIPTION



The proposed project will improve WPAR from the OR&L Bicycle/Pedestrian Path to the entrance to the Waipio Peninsula Soccer Park in order to address the GOE conditions, and the lack of a pedestrian and cycling link to the soccer park. The project will not increase the vehicular capacity of the WPAR.

To address the GOE condition regarding parking for NISMO personnel, a paved parking area at the same location and configuration as the existing parking area will be provided (see Figures S-2, S-3, and S-4). The number of parking spaces will be increased from the existing 75 spaces to between 90 to 100 spaces. The roadway will be modified to include operational improvements to enhance the safety of the current parking maneuvers.

MIDDLE LOCH



Legend

-  Asphalt Shoulder
-  Roadway Lane
-  Paved Parking Area
-  Landscaped Area
-  Shared-Use Path
-  Concrete Lined Drainage Channel

Scale:

0 40 80 160 FT.



LE LOCH

For Details,
See Figure S-3
For Proposed
Roadway Section

Inactive Ships On-Site
Maintenance Facility (NISMO)

Waipio Peninsula
Soccer Park Sign

To Waipio Peninsula
Soccer Park

SOUTH PROJECT LIMITS

to a
Ball-Barrier
Access

18th Fairway

Golf Cart Path

18th
Green

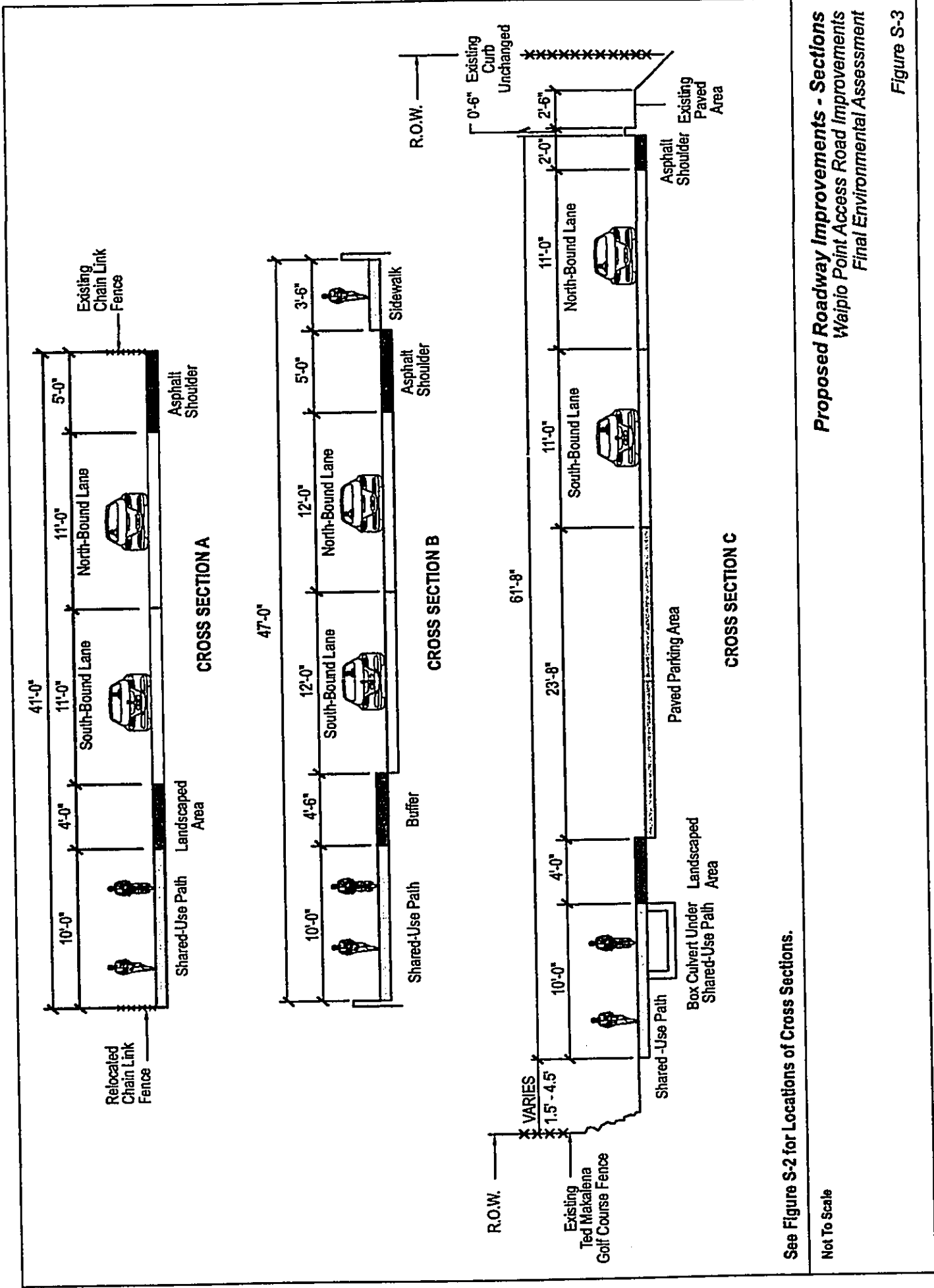
Golf Course
Paved Parking Lot

9th
Green

Ted Makalena Golf Course
(City & County of Honolulu)

Proposed Roadway Improvements
Waipio Point Access Road Improvements
Final Environmental Assessment

Figure S-2

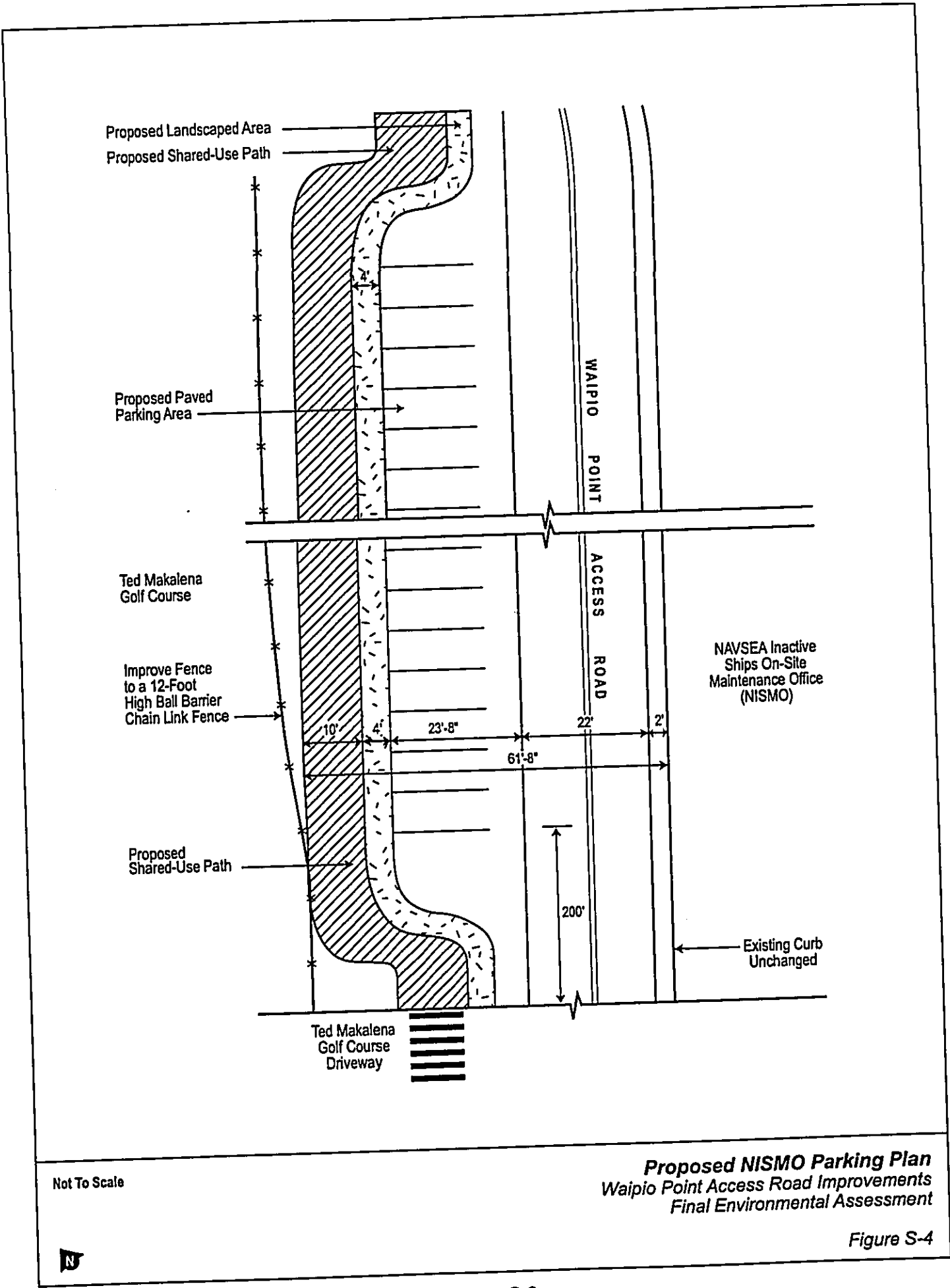


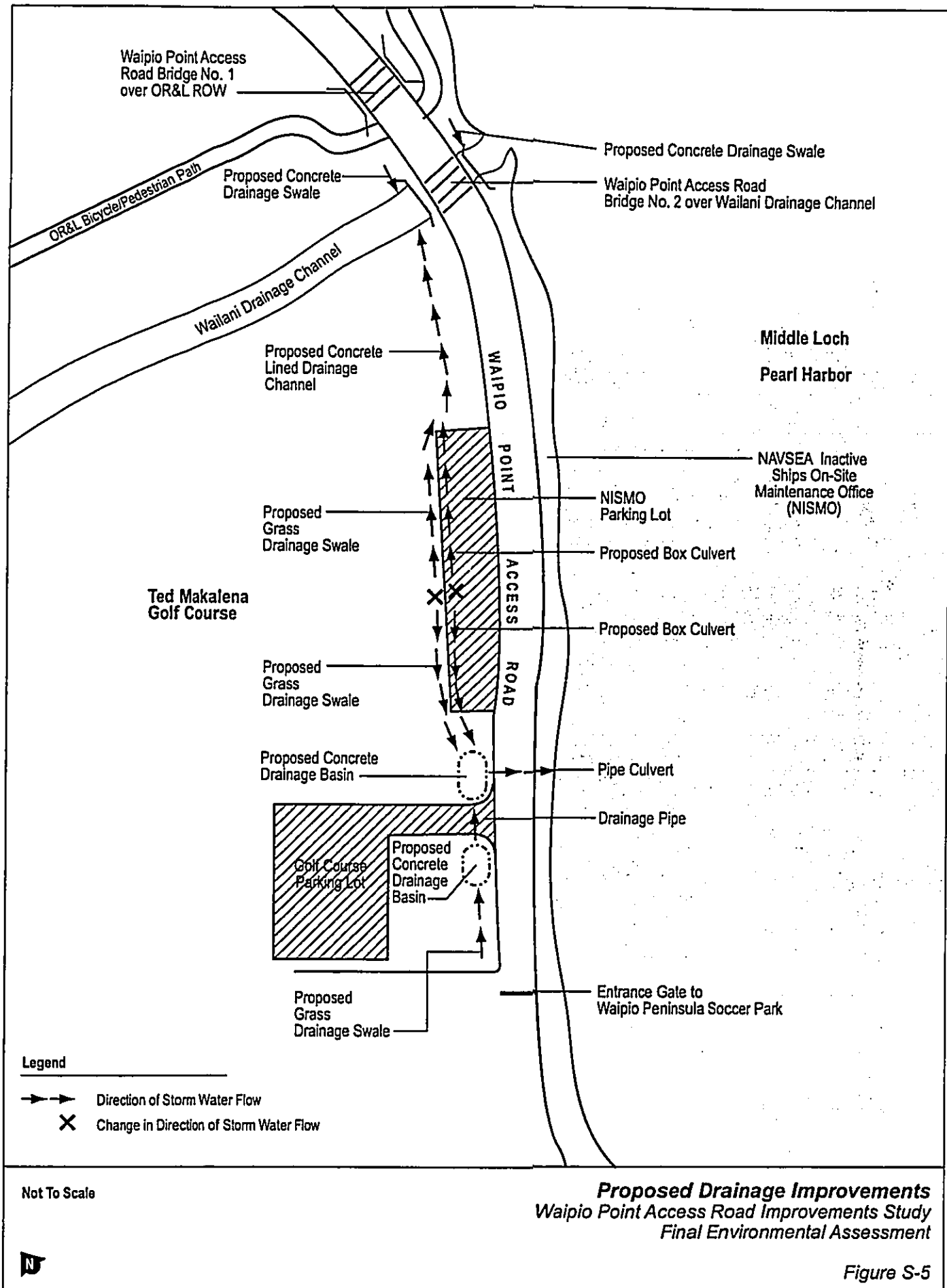
See Figure S-2 for Locations of Cross Sections.

Not To Scale

Proposed Roadway Improvements - Sections
 Waipio Point Access Road Improvements
 Final Environmental Assessment

Figure S-3





To address the GOE condition on drainage, the following drainage facilities will be constructed, which will be designed to accommodate up to a 50-year storm event (see Figure S-5):

- Roadway grade adjustment so that storm water flows away from the NISMO facility;
- Shallow box culvert to accommodate roadway storm water;
- Grass drainage swale within Ted Makalena Golf Course;
- Grass drainage swale south of the golf course driveway;
- Small concrete drainage basins on both sides of the golf course driveway (the basin north of the driveway is within the golf course), connected by a drainage pipe under the driveway;
- Culvert under WPAR from the drainage basin area located in the golf course (north of the golf course driveway) to Middle Loch;
- Concrete lined open drainage channel between the NISMO parking area and Wailani Drainage Channel, replacing the existing drainage ditch at this location; and
- Concrete lined drainage swales north of WPAR Bridge No. 2 over Wailani Drainage Channel to accommodate drainage flowing from the north end of WPAR.

In addition to the above parking and drainage improvements, a ten-foot wide shared-use path on the west side of the roadway will be provided between the OR&L Bicycle/Pedestrian Path and the entrance to the soccer park (see Figures S-2, S-3 and S-4). Also, the OR&L Bicycle/Pedestrian Path crossing at WPAR will be made more visible to approaching motorists. In addition, landscaping will be provided next to the shared-use path and NISMO parking area; the approaches and railings of Bridge No. 2 will be upgraded; consistent lighting will be provided along WPAR based on City standards; and the golf course fence will be improved to a 12-foot high ball-barrier chain link fence in the vicinity of the NISMO parking.

Although the proposed project requires re-designation of about 0.08 acres of property from the golf course to the roadway right-of-way at two locations, these areas are not within "playable" areas of the golf course (i.e., fairways, greens, or rough). In addition, the project requires 0.05 acres of Navy land northeast of Bridge No. 2 to accommodate the concrete drainage swale. The City's roadway easement with the Navy would need to be amended to reflect this change.

The cost of the project is estimated to be about \$4.5 million. Construction of the project could begin as soon as 2006, but this schedule is dependent upon funding availability for design and construction.

S.4 ALTERNATIVES

Two other improvement alternatives were considered for the proposed project, but were deemed not feasible for various reasons. Both alternatives would provide bicycle lanes and sidewalks between the OR&L Bicycle/Pedestrian Path and the soccer park. Similar to the proposed project described in Section S.3, both alternatives would provide parking for NISMO personnel; and improved drainage. The difference between the

alternatives and the proposed project is how NISMO parking and drainage are addressed. Figures of these alternatives are provided in Appendix B.

The first alternative would maintain the NISMO parking at the same location, but the configuration of the area would include diagonal parking, with a one-way south bound circulation aisle on the roadway side of the area to accommodate parking maneuvers, and circulation. This alternative was deemed not feasible because it would require a half-acre more property from Ted Makalena Golf Course than the proposed project. The half-acre required would be located adjacent to the golf course's 18th hole fairway. See Figures B-1, B-2 and B-3 in Appendix B.

The second alternative would relocate the NISMO parking area to a vacant grassy area next to the golf course parking lot. The grassy area would be converted to a paved parking area with access from the golf course driveway. This alternative was deemed not feasible because it would not fulfill the GOE requirement of maintaining parking adjacent to the NISMO facility. See Figures B-4, B-5 and B-6 in Appendix B.

S.5 IMPACTS AND MITIGATION

The proposed project is not expected to cause substantial environmental impacts because most improvements would occur within the existing WPAR right-of-way, and the project does not require an increase in roadway capacity. The following provides a brief summary of the environmental impacts of the project. A summary of mitigation measures are also provided for those impacts considered adverse.

Topography, Geologic Conditions, and Hazardous Materials

The proposed project will not require substantial excavation other than what is needed to provide drainage culverts, swales and channels, nor will it require substantial earthmoving activities, or structures beyond the shared-use path. However, excavation activities may uncover soil and/or groundwater contaminated with petroleum previously released from the Navy's abandoned JP-5 pipeline.

Before construction, test borings along WPAR will be recovered to assess the likelihood of encountering contaminated media from the abandoned JP-5 pipeline. If it were determined that contaminated soil or groundwater exists within the area affected by excavation, a hazardous waste removal plan will be developed to address the removal and disposal of the contaminated media. Solid waste generated during construction will be properly handled and disposed of in accordance with State of Hawaii Department of Health (SDOH) requirements.

Water Resources

Excavation and grading activities have the potential to cause storm water erosion and sedimentation which could affect the quality of nearby surface waters, such as Pearl Harbor's Middle Loch. Best Management Practices (BMPs) will be implemented to prevent degradation of nearby surface waters. The Contractor will be required to obtain a National Pollutant Discharge Elimination System (NPDES) construction storm water permit from the SDOH and have site specific BMP plans approved by SDOH prior

to construction. In addition, utility relocations (see "Utilities" below) may require dewatering and the hydrotesting of relocated lines. If discharged into the storm drain system or Pearl Harbor, the contractor will need to obtain the appropriate NPDES permit.

New drainage structures and systems on and near WPAR will be able to handle up to a 50-year storm event. The pollutant loading of Middle Loch from roadway-related sources will be no different than what would occur under the No Build condition because traffic conditions will be the same.

Biological Resources

Four Monkeypod trees and one coconut tree immediately south and north, respectively, of the golf course driveway will be displaced for one of the drainage swales and the concrete drainage basins. The golf course drainage swale will require the relocation of 26 trees to other parts of the golf course.

The golf course drainage swale will be grass-lined and located outside the 18th hole fairway. The project is not anticipated to cause adverse effects to mostly introduced avian species previously observed in the vicinity of the project site.

The project will provide landscaping within a four foot wide strip along WPAR. All the trees displaced in the golf course will be relocated elsewhere in the property in coordination with the Department of Enterprise Services, the City agency responsible for operating the golf course.

Air Quality and Noise

Construction will generate fugitive dust emissions and relatively high noise levels. Since most construction activities will not be close to noise sensitive land uses, disruptions of normal activities due to construction-related noise are not anticipated. Standard measures to control dust will be employed during construction, such as watering during dry conditions, limiting areas of disturbance, and installation of windbreaks when appropriate. SDOH's community noise control standards will be followed, including the contractor obtaining a noise permit if required. The surrounding land uses are the golf course and NISMO facility. There are no nearby residences that would be affected by construction.

Ambient air quality and noise conditions along WPAR will continue to be affected by traffic conditions, which are currently free-flowing with very low volumes.

Roadway and Traffic System

Construction will require temporary lane closures that would delay traffic on WPAR. Access to NISMO, the golf course and the soccer park will always remain open. Police and flagmen will be stationed to direct traffic around lane closures or other safety hazards and work zones. Lane closures will not occur during major soccer tournaments.

During normal weekdays, traffic on WPAR will continue to be free-flowing with very low volumes because traffic generation from NISMO and the golf course will not change substantially. During major soccer tournaments, WPAR will operate at a LOS D condition, at peak periods. These events happen infrequently, and their peak traffic conditions do not coincide with NISMO peak traffic periods.

Bicycle and Pedestrian Facilities

Pedestrian movements will be maintained on WPAR, but may be temporarily relocated to provide safe passage through work areas.

The addition of a shared-use path provided by the project will support alternative modes of transportation to and from the soccer park and other land uses next to WPAR, and safety for those using the OR&L Bicycle/Pedestrian Path.

Parking Facilities

A minimum amount of NISMO parking will be maintained throughout construction, but some NISMO personnel might be directed to park in the golf course property.

The existing NISMO parking will be paved and modified to provide marked parking stalls. A crosswalk will traverse WPAR, connecting to the NISMO entrance. Handicapped and delivery/service vehicles marked stalls will be located adjacent to the crosswalk.

Utilities

The proposed project will relocate nine wooden utility poles to within the four-foot wide landscaping/utility strip. A utility pole north of Bridge No. 2 will also require relocation. An underground telephone cable north of the golf course entrance may require relocation due to the proposed concrete drainage basin and a four-inch pressure sewer line may require adjustment to avoid conflict with the proposed box culvert. A three-inch water line may require relocation depending on excavation plans. Utility relocations will be coordinated with the Navy and applicable utility companies. Properties whose services will be disrupted will receive prior notification.

Land Use

Property will be re-designated from the golf course to the roadway right-of-way, but no "playable" portion of the golf course will be affected. The affected property consists of unusable buffer areas between the golf course and the public roadway.

Cultural, Social, and Economic Conditions

Existing cultural, social and economic activities will continue, such as fishing from Bridge No. 2 and business operations at the NISMO facility and Ted Makalena Golf Course. There are no known cultural practices associated with the historic fishpond "Loko Eo" (see "Historic Properties" below).

Historic Properties

Although WPAR overlies a buried fishpond eligible for the National Register of Historic Places ("Loko Eo"), fishpond sediments were found to be about 21 feet below the surface. The deepest depth of required excavation ranges from seven to ten feet. Therefore, the project is not expected to affect the fishpond.

Parks and Recreational Resources

Despite the need for lane closures during construction, access to Ted Makalena Golf Course and Waipio Peninsula Soccer Park will remain open throughout construction.

The recreational amenities and services provided by the golf course and soccer park will remain the same as today. The golf course drainage swale will be adjacent to the golf course fence, and a small non-playable portion will be re-designated to be part of the roadway right-of-way.

Visual and Aesthetic Resources

The aesthetic quality of WPAR will be improved with the addition of a four-foot wide landscaping strip and the improved NISMO parking.

S.6 APPROVALS AND PERMITS

The following permits or approvals may be required following completion of the environmental review process under HRS Chapter 343 and NEPA.

Federal

- Department of Army permit pursuant to Section 404 of the Clean Water Act (CWA) and Section 10 of the Rivers and Harbors Act

State of Hawaii

- Coastal Zone Management federal consistency determination
- NPDES permit for storm water discharges relating to construction activities
- NPDES permit for discharges associated with construction activity dewatering
- NPDES permit for discharge of hydrotesting waters
- Water Quality Certification pursuant to CWA Section 401
- Stream Channel Alteration Permit

City and County of Honolulu

- Special Management Area Use Permit
- Shoreline Setback Variance
- Grading, Grubbing, Stockpiling and Excavation permit

S.7 COMMENTS AND COORDINATION

Prior to release of the Draft EA, over 30 federal, State, and City agencies, elected officials and other organizations were contacted by letter, and were asked if they were aware of any environmental or social issue associated with the proposed project.

Twenty-two of them responded by letter or e-mail, and their comments helped prepare the Draft EA. Copies of the request letters and responses are provided in Appendix C.

The project's Draft EA was announced in the December 23, 2004 edition of the Environmental Notice, which initiated the 30-day public comment period that ended on January 22, 2005. The Draft EA was mailed to fifty-three organizations including federal, State and County agencies; elected officials; utility companies; community organizations; and libraries. Seventeen letters providing comment on the Draft EA were received during the 30-day period. Some of the comments led to revisions of the EA, and are reflected in this document. In addition to the distribution of the Draft EA, DTS presented the project to the Waipahu Neighborhood Board on March 17, 2005.

Agency consultations for compliance with the following environmental regulations were also initiated during the EA process. Copies of the regulatory correspondence are provided in Appendix D:

- Section 7 of the Endangered Species Act
- Section 1424(e) of the Safe Drinking Water Act
- Section 106 of the National Historic Preservation Act
- HRS Section 6E-8 of the Hawaii Revised Statutes

S.8 FINDING OF NO SIGNIFICANT IMPACT

In accordance with Chapter 343 and Hawaii Administrative Rules (HAR), Sections 11-200-9 and 11-200-11.2, DTS has issued a FONSI for the proposed project. The Final EA/FONSI will be announced in the State's Environmental Notice. This determination is based on an evaluation of project impacts in relation to the "Significance Criteria" specified in HAR 11-200-12 (b). DTS found that the project impacts (see Section S.5) address all of the Significance Criteria.

CHAPTER 1 ■

WAIPIO POINT ACCESS ROAD Improvements

FINAL ENVIRONMENTAL ASSESSMENT

CHAPTER 1 PROPOSED ACTION

1.1 INTRODUCTION

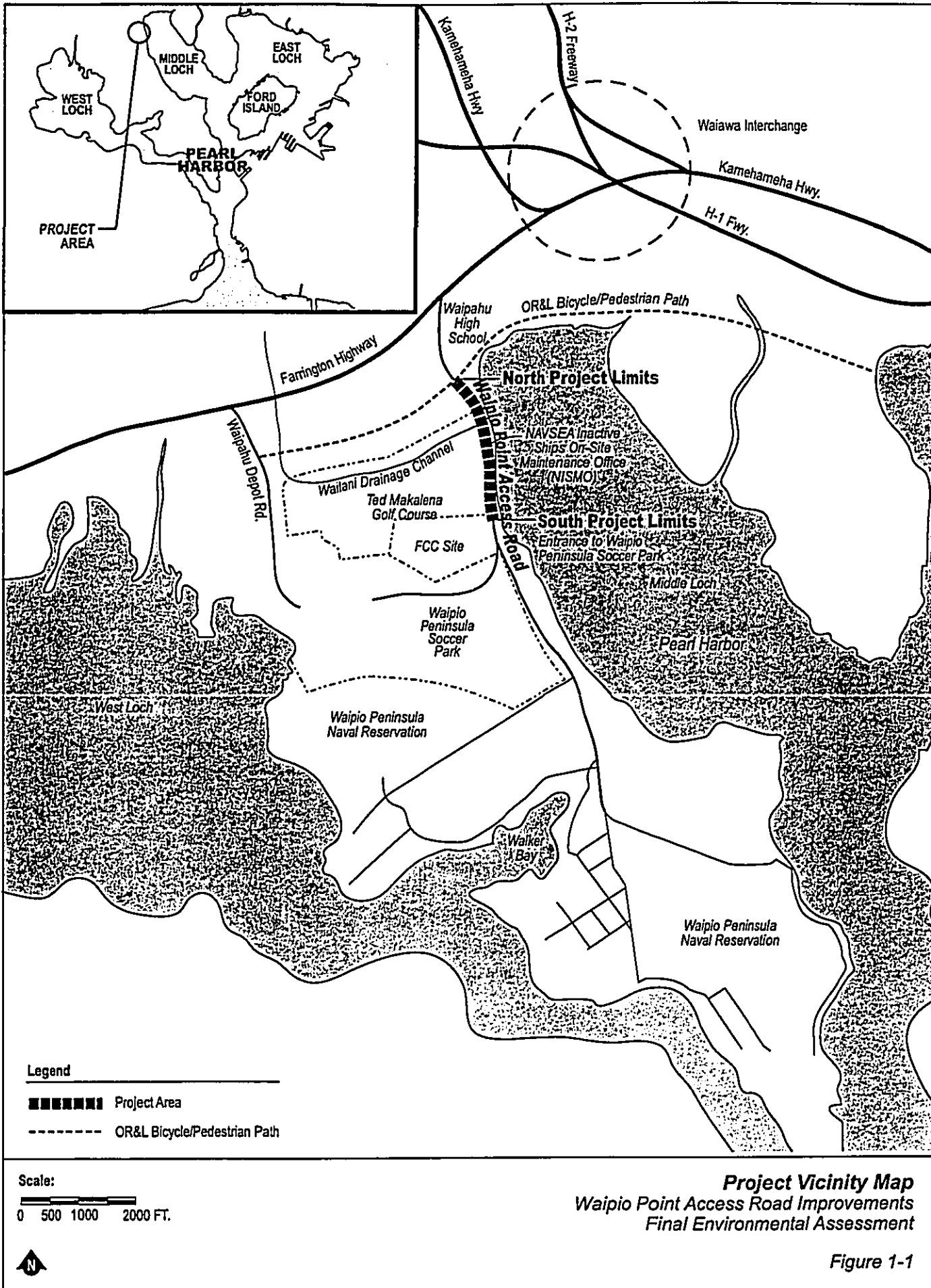
The City and County of Honolulu (City), Department of Transportation Services (DTS) proposes improvements to Waipio Point Access Road (WPAR) from the Oahu Railway and Land Company (OR&L) Bicycle/Pedestrian Path to the entrance to Waipio Peninsula Soccer Park in Waipahu, Oahu (see Figure 1-1). The purpose of the project is to address the conditions of a Grant of Easement (GOE) dated December 2, 1999 between the U.S. Department of the Navy, who currently owns this section of road, and the City for use of WPAR for access to the soccer park.

According to the GOE, the City must ensure that parking is available for NAVSEA Inactive Ships On-Site Maintenance Office (NISMO) personnel, roadway drainage is properly handled, and traffic conditions LOS D or better are maintained (see GOE in Appendix A). Because it was determined that the current two-lane configuration of the roadway will provide sufficient capacity for projected travel demand for at least 20 years, the proposed improvements focused on providing long term parking for NISMO personnel and addressing the road's drainage problems.

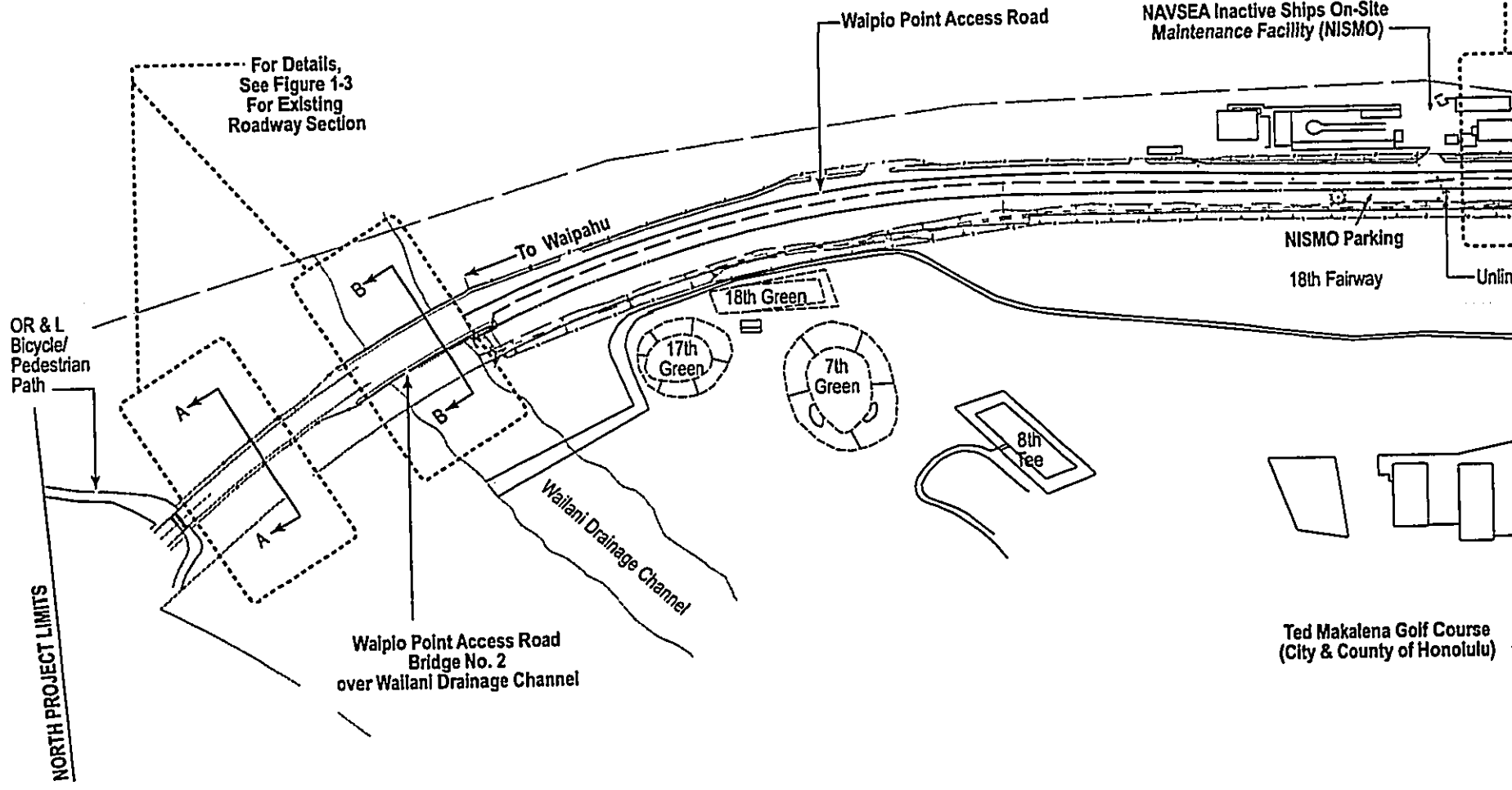
1.1.1 Background and Existing Waipio Point Access Road

In 2000, the City completed Phase 1 of the Waipio Peninsula Soccer Park on approximately 300 acres of land leased from the Navy in December 1998. The soccer park currently consists of 19 soccer fields, and the first phase of the soccer stadium is able to accommodate approximately 4,000 spectators and provide locker rooms for participants. The City's long term plans to expand the soccer park include an additional 13 soccer fields, and to increase the capacity of the soccer stadium to seat approximately 10,000 spectators. The City also plans to provide free standing concession stands and restrooms, media and medical facilities, and administrative offices. Also, parking will be increased to accommodate up to 5,000 vehicles. The GOE, which granted the City the right to use WPAR for public vehicular access to the soccer park, included conditions which are described in Section 1.2.

WPAR is a two-lane facility between Farrington Highway and the entrance to the soccer park. The road is only about one mile long with a posted speed limit of 20 mph, but is still classified as an urban major collector. The existing WPAR within the project limits is depicted in Figures 1-2 and 1-3. This segment of roadway is owned by the Navy, which operates facilities on the peninsula, including a NISMO adjacent to the roadway. As noted on Figures 1-2 and 1-3, pedestrian facilities are limited to sidewalks on Waipio Point Access Road Bridge No. 2 over Wailani Drainage Channel (Bridge No. 2) (see Cross Section B on Figure 1-3), and no bicycle facilities are provided, except for the OR&L Bicycle/Pedestrian Path crossing. An unpaved parking area next to Ted Makalena Golf Course directly across from the NISMO facility is provided for NISMO personnel (see Cross Section C on Figure 1-3). Besides the NISMO facility and Ted Makalena Golf Course, the other users of the roadway are a Federal Aviation Administration facility and Hokuahi Landscaping located near the entrance to the soccer park. WPAR terminates at the soccer park entrance.



MIDDLE LOCH



Scale:
0 40 80 160 FT.



E LOCH

For Details,
See Figure 1-3
For Existing
Roadway Section

Inactive Ships On-Site
Maintenance Facility (NISMO)

Waipio Peninsula
Soccer Park Sign

To Waipio Pt. →

SOUTH PROJECT LIMITS

NISMO Parking

18th Fairway

Unlined Drainage Ditch

Golf Cart Path

18th
Green

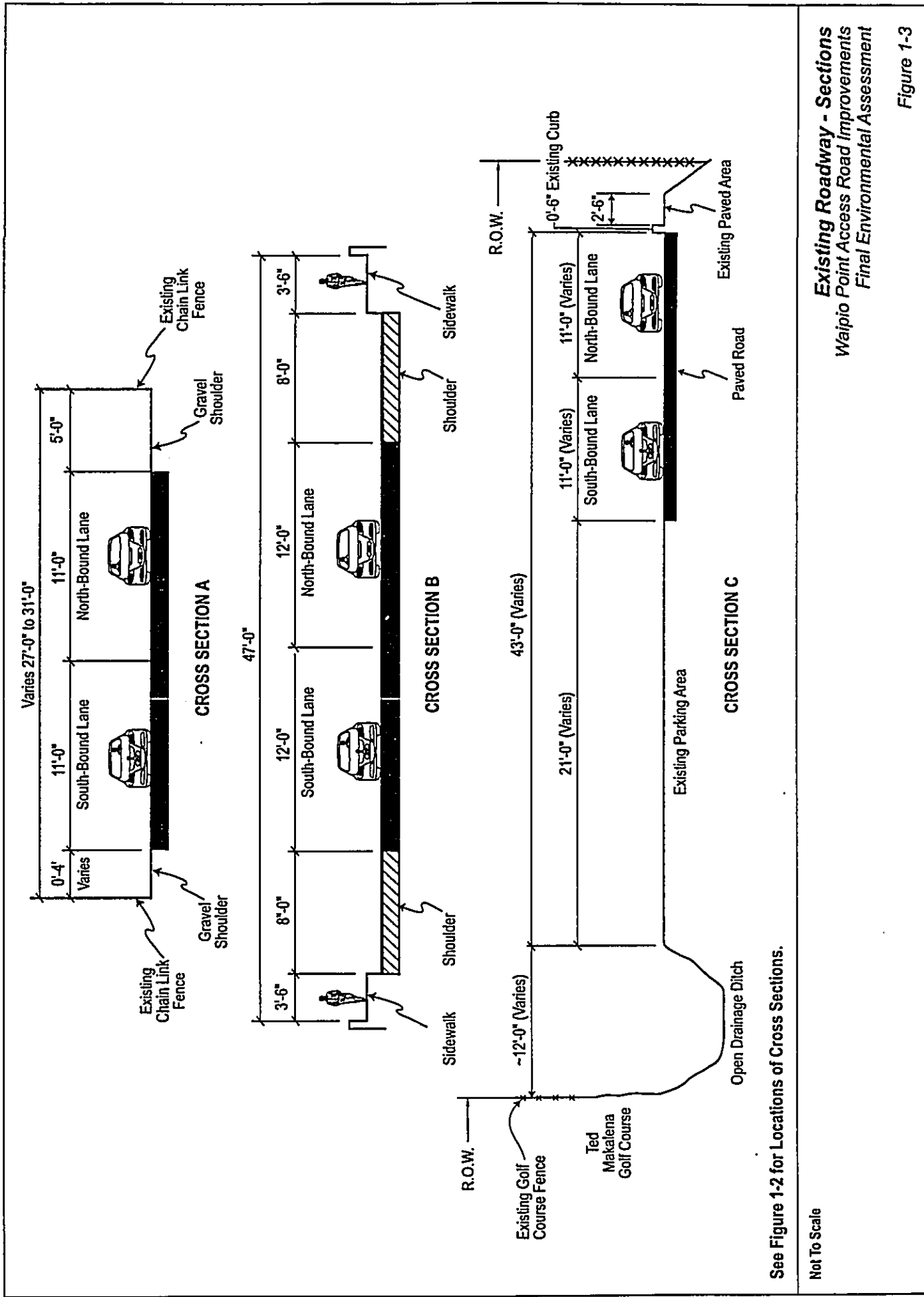
Golf Course
Paved Parking Lot

9th
Green

Ted Makalena Golf Course
(City & County of Honolulu)

Existing Waipio Access Point Road
Waipio Point Access Road Improvements
Final Environmental Assessment

Figure 1-2



See Figure 1-2 for Locations of Cross Sections.

Not To Scale

Existing Roadway - Sections
 Waipio Point Access Road Improvements
 Final Environmental Assessment

Figure 1-3

1.1.2 Planning Context

The proposed project will require use of City and Federal Highway Administration (FHWA) funds to design and construct the roadway improvements, which triggers the environmental review requirements of Chapter 343 of the Hawaii Revised Statutes (HRS) and the National Environmental Policy Act (NEPA), respectively. In addition, the GOE states that any roadway work must be approved in writing by the Navy, and be in compliance with all applicable federal environmental laws and regulations, including NEPA.

As an agency action under HRS Chapter 343, DTS has determined that an environmental assessment (EA) would be the appropriate environmental review document because the project is unlikely to cause a "significant" impact. The bases for concluding that the project's impacts would not be "significant" are provided in Chapter 4.

As the lead federal agency under NEPA, the FHWA has determined that the project would qualify for a categorical exclusion (CatEx), meaning that an environmental review document, such as an EA or EIS, would not be required for NEPA compliance. Projects that qualify for a CatEx are of a type that past experience indicates do not affect the environment significantly. The Navy did not object to the CatEx designation of this project.

This Final EA has been prepared to comply with HRS Chapter 343, and Title 11, Chapter 200 of the Hawaii Administrative Rules. This document is intended to disclose the environmental and social impacts that could result from the project's implementation, and commit to the implementation of specific mitigation measures. It also contains a record of all consultation activities that have been conducted as part of project planning. In addition, this Final EA documents compliance with federal laws and regulations because of the proposed use of federal funds administered by the FHWA.

1.2 PURPOSE AND NEED FOR THE PROJECT

In December 1999, the Navy and the City executed the GOE, which granted the City the right to use WPAR for public vehicular access to the recently completed Waipio Point Soccer Park (see Appendix A). However, the easement included the following conditions for the City that is applicable to roadway improvements:

- Parking for naval activities shall be maintained adjacent to the premises and not be reduced;
- The City shall provide reasonable drainage improvements to prevent flooding of the roadway and adjacent naval facilities; and
- The City shall make any necessary improvements to ensure that traffic conditions at intersections along WPAR do not fall below Level of Service (LOS) D (Scale from A to F, representing best to worst, respectively, with no E in the scale).

Although during major soccer tournaments the LOS on WPAR falls to a "D" during peak hours, and would continue to be LOS D during future soccer tournaments (see Section

2.2.1), these events happen relatively infrequently, and do not justify increasing the capacity of WPAR from its current two-lane configuration. The normal weekday traffic condition on WPAR is free-flowing with very low volumes. Normal weekday traffic generation along WPAR is not expected to change dramatically, which means that free-flowing traffic conditions are expected to continue in the future. Therefore, the last GOE condition above does not require project action.

In addition to addressing the GOE conditions, the City wants to provide a shared-use path for bicycle and pedestrian access to Waipio Peninsula Soccer Park. Currently, the WPAR has no bicycle facilities and limited pedestrian facilities.

1.2.1 Maintenance of Navy Parking

The NISMO facility, located along WPAR, currently has insufficient space to accommodate its parking requirements. Therefore, the west side of WPAR, across from the facility's entrance, is used as parking for Navy contractors and visitors. The area provides enough space for approximately 75 vehicles, parked perpendicular to the roadway. At any given time, the number of vehicles parked at this location is largely based on the NISMO workload. The parking area is not paved nor is its stalls marked, but is stabilized with gravel, and is defined by concrete tire stops imprinted with "Reserved for NISMO Parking".

Many of the parking users back their vehicles into the area. At most times, this does not present a major problem because the road supports very low volumes. Fortunately, the peak periods in which parking users arrive and depart (weekday early morning and late afternoon) do not coincide with normal soccer park activities. However, during major soccer tournaments the backing and pull-out maneuvers can be difficult.

1.2.2 Inadequate Drainage

The WPAR right-of-way includes an unlined drainage ditch along the Ted Makalena Golf Course fence. One end of the drainage ditch is about 80 feet north of the golf course driveway and the other end discharges into the Wailani Drainage Channel at Bridge No. 2. During heavy rains, the golf course becomes saturated and runoff sheet flows into the drainage ditch, which often is unable to contain the storm waters. If the ditch were overtopped, storm waters would flow over the roadway and into the NISMO facility on the east side of the road. Flooding often occurs at the NISMO entrance gate because it is located at the low point of the roadway. This flooding situation is worse during high tide, which is about two to three feet higher than low tide. During high tide, the height of the water in the Wailani Drainage Channel rises above the bottom of the drainage ditch, resulting in channel water backing up the ditch, and impeding the outfall of storm water into the channel. According to the Navy, the drainage ditch is not able to convey storm water flows during low tide as well if the intensity and duration of rainfall is high, and the golf course is saturated.

In addition, NISMO and golf course personnel have indicated that during storm conditions, runoff from the north side of the bridge flows over the bridge and discharges into the unlined drainage ditch. A drainage study conducted in 1994 stated that there is no formal drainage system north of the bridge, and that the drainage

pattern is not well defined. Runoff as far north as Waipahu High School appears to generally run along WPAR towards Wailani Drainage Channel.

Navy personnel noted that flooding problems have led to automobile accidents. They indicated that many drivers illegally travel at high speeds on the straight stretch of roadway, and when the road is flooded or even if it has only ponded water, vehicles have been observed to hydroplane, swerve and skid in front of the NISMO facility

The existing drainage ditch is difficult to maintain, and becomes unsightly as it often collects litter and trash from passerbys, fishermen, or deposited by storm water flows. In addition, Navy personnel believe that the ditch is a roadside hazard, and therefore, have placed concrete piles in front of the ditch in areas where there are no other barricades to prevent vehicles from accidentally entering the ditch.

1.2.3 System Linkage: Bicycle and Pedestrian Facilities

In addition to addressing the GOE conditions, the City wants to provide bicycle and pedestrian access to Waipio Peninsula Soccer Park to improve the access for bicyclists and safety for pedestrians. Currently, WPAR has no bicycle facilities and has limited pedestrian facilities. Both cyclists and pedestrians have been observed using WPAR to access fishing grounds and the park. Although the soccer park generates vehicular traffic, it also generates alternative modes of transportation that includes both cycling and pedestrian.

The OR&L Bicycle/Pedestrian Path crosses the roadway and is the northern limit of the project. Currently, there are a few signs on WPAR indicating a bicycle crossing, and, no pavement markings on the roadway delineate a crossing. This path is used by cyclists and pedestrians because it provides a relatively safe passage between Waipahu and Pearl City. There is no other dedicated bicycle or pedestrian facility that provides access between these two locales. The eastern terminus of the path is at Aiea Bay Recreational Area near McGrew Point. The path used to extend to the Arizona Memorial, but the Navy has closed that section of the path for security reasons. The western terminus of the path is at Waipahu Depot Road, but the State of Hawaii Department of Transportation (SDOT) is planning to extend the path across the rest of Waipahu, connect it with an existing path through West Loch Shoreline Park, and extend it further west using the OR&L Right-of-Way to Kahe Point on the Waianae Coast.

The existing pedestrian facilities along WPAR consist of three-and-a-half foot wide sidewalks on both sides of the Bridge No. 2.

1.3 PROJECT DESCRIPTION

The proposed project will improve WPAR from the OR&L Bicycle/Pedestrian Path to the entrance to the Waipio Peninsula Soccer Park in order to address the GOE conditions, and the lack of a pedestrian and cycling link to the soccer park (see Figures 1-4 and 1-5). As noted in Section 1.2, the proposed project will not increase the vehicular capacity of the WPAR. The No Build condition would consist of maintaining the current state of WPAR.

1.3.1 NISMO Parking

To address the GOE condition regarding maintaining parking for NISMO personnel, the project will provide a paved parking area at the same location and general configuration as the existing parking area (see Figures 1-4, 1-5 and 1-6). The number of parking spaces will be increased from about 75 existing spaces to between 90 to 100 spaces. NISMO personnel would still have to conduct the same backing and pull-out maneuvers on WPAR as they do today. The roadway will be modified to include operational improvements to enhance the safety of the current parking maneuvers.

1.3.2 Drainage Improvements

To address the GOE condition on drainage, the following drainage facilities will be constructed, which would be designed to accommodate up to a 50-year storm event (see Figure 1-7):

- Roadway grades will be adjusted so that storm water on the road flows toward the west side of the roadway, away from the NISMO facility.
- A shallow box culvert will be installed underneath the proposed shared-use path (see Section 1.3.3) at the NISMO parking area to accommodate roadway storm water.
- A grass drainage swale will be excavated within Ted Makalena Golf Course adjacent to the existing property line to accommodate golf course storm water.
- A grass drainage swale will be excavated within the grassy area south of the golf course driveway adjacent to WPAR to accommodate storm water from the golf course parking lot. A small concrete drainage basin¹ will be provided at the north end of the swale, next to the driveway.
- A drainage pipe constructed under the driveway will direct the storm water flow from the parking area drainage swale to another concrete drainage basin² located adjacent to WPAR within the golf course near the golf course driveway. The golf course drainage swale and box culvert will also connect to the drainage basin.
- A culvert³ will be installed under WPAR from the concrete drainage basin within the golf course (near the golf course driveway) to Middle Loch. Due to the low elevation in this area, the culvert outlet would be partially submerged during high tide.
- Concrete lined drainage channel between the NISMO parking area and the Wailani Drainage Channel, replacing the existing unlined drainage ditch at this location.

¹ The concrete drainage basin at this location will be maintained by the Department of Facility Maintenance.

² The Ted Makalena Golf Course (Department of Enterprise Services - DES) will be responsible for the maintenance for the concrete drainage basin at this location.

³ The culvert will be maintained by the Department of Facility Maintenance.

MIDDLE LOCH

NAVSEA Inactive Ships On-Site Maintenance Facility (NISMO)

For Details, See Figure 1-5 For Proposed Roadway Section

For Details, See Figure 1-5 For Proposed Roadway Section

Improve Fence to a 12-Foot High Ball-Barrier Chain Link Fence

18th Fairway

To Waipahu

18th Green

17th Green

7th Green

New Sign "Soccer Park →"

8th Tee

Waiala Drainage Channel



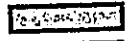

Waipio Point Access Road Bridge No. 2 over Waiala Drainage Channel

OR & L Bicycle/Pedestrian Path

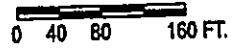
NORTH PROJECT LIMITS

Ted Makalena Golf Course (City & County of Honolulu)

Legend

-  Asphalt Shoulder
-  Roadway Lane
-  Paved Parking Area
-  Landscaped Area
-  Shared-Use Path
-  Concrete Lined Drainage Channel

Scale:



LE LOCH

For Details,
See Figure 1-5
For Proposed
Roadway Section

Inactive Ships On-Site
Maintenance Facility (NISMO)

Waipio Peninsula
Soccer Park Sign

To Waipio Peninsula
Soccer Park

SOUTH PROJECT LIMITS

to a
Ball-Barrier
Access

18th Fairway

Golf Cart Path

18th
Green

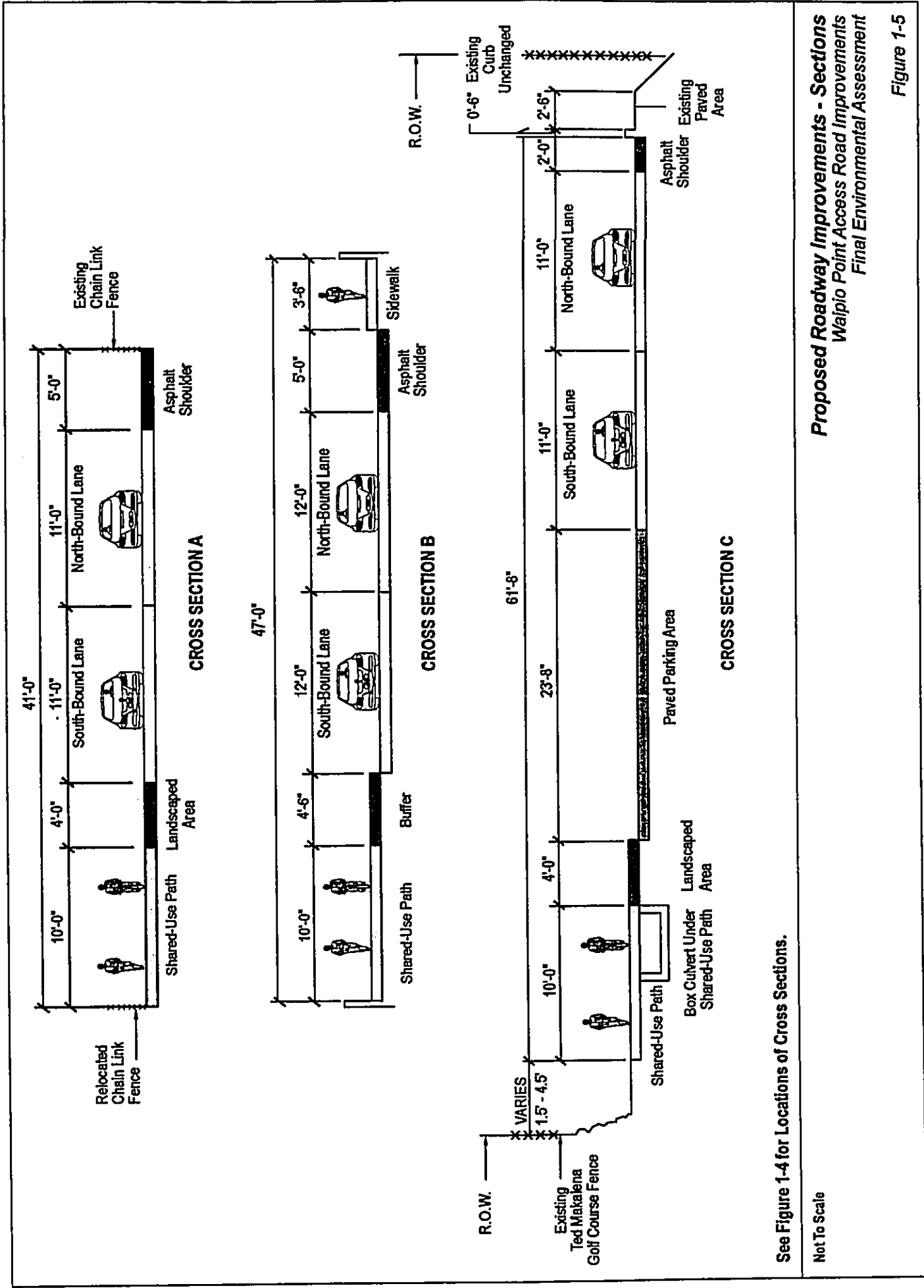
Golf Course
Paved Parking Lot

9th
Green

Ted Makalena Golf Course
(City & County of Honolulu)

Proposed Roadway Improvements
Waipio Point Access Road Improvements
Final Environmental Assessment

Figure 1-4

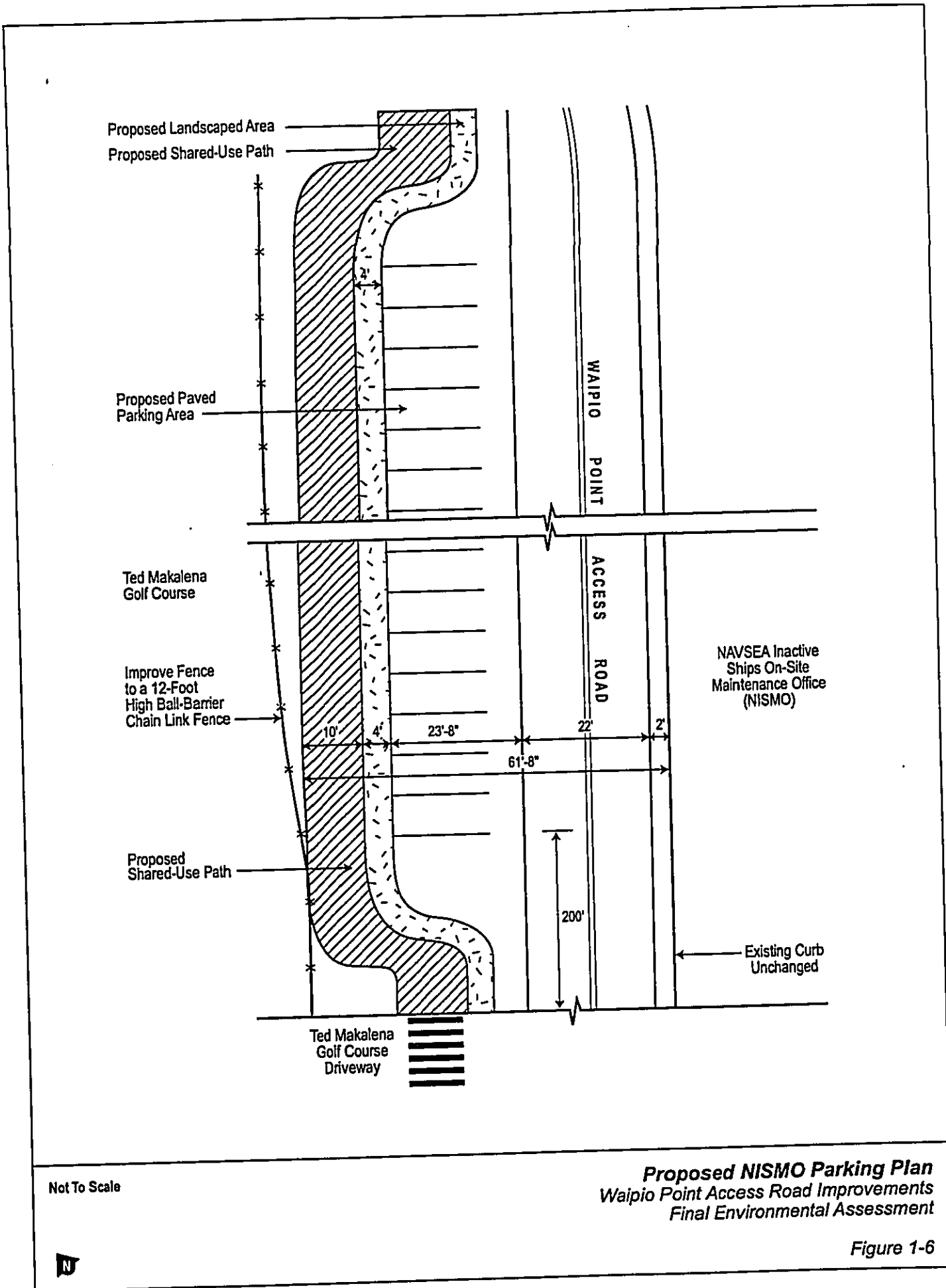


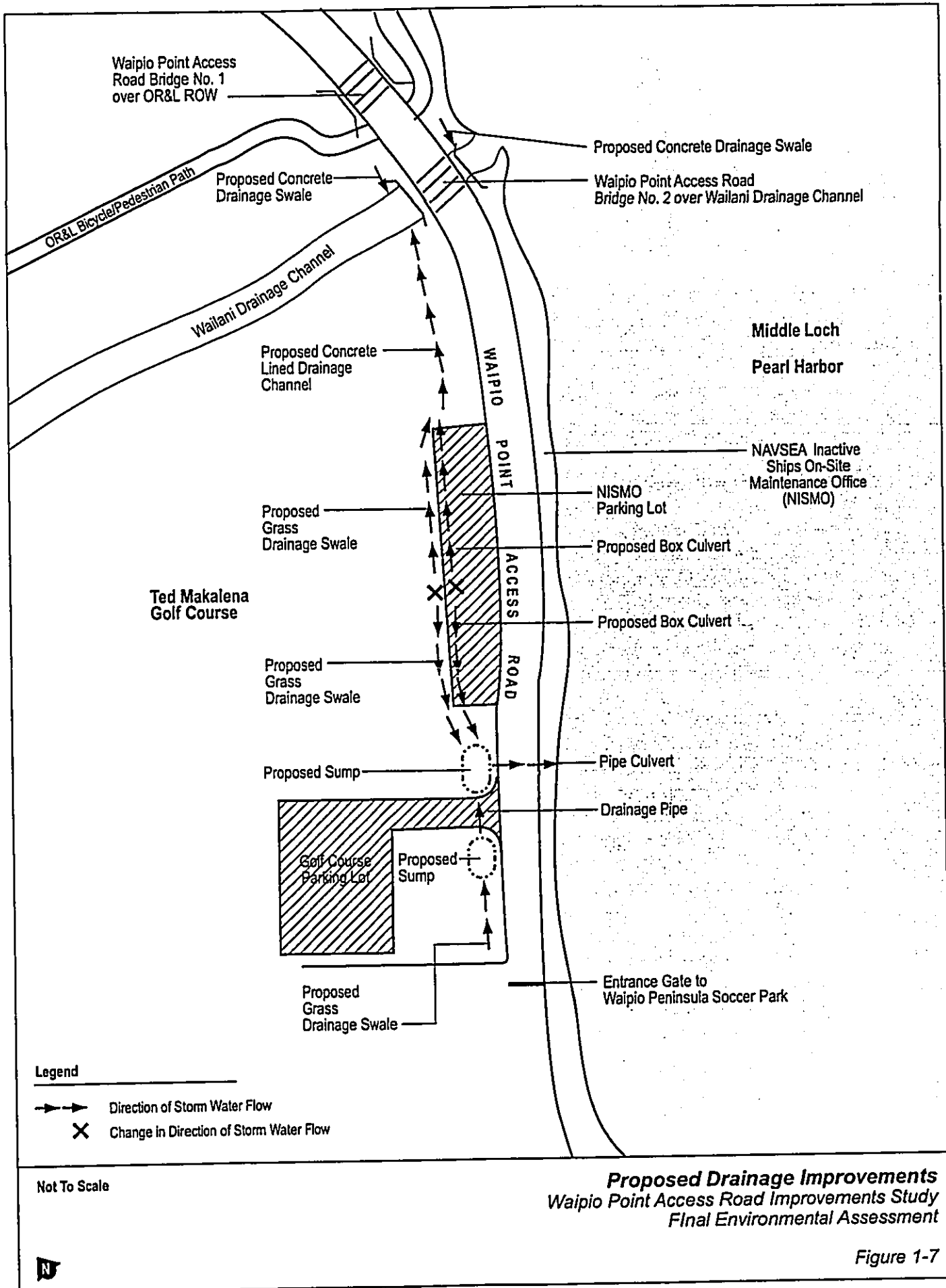
See Figure 1-4 for Locations of Cross Sections.

Not To Scale

Proposed Roadway Improvements - Sections
 Waipio Point Access Road Improvements
 Final Environmental Assessment

Figure 1-5





- Concrete drainage swales⁴ will be constructed on both the west and east sides of the roadway just north of Bridge No. 2 to accommodate drainage flowing from the north end of WPAR.

Storm water collected in the golf course grass drainage swale and box culvert will be directed to both Wailani Drainage Channel and the larger concrete drainage basin located in the golf course. At the north end of the NISMO parking area, the golf course grass drainage swale and box culvert will transition to an open concrete lined drainage channel, which would replace the existing unlined drainage ditch at this location. The storm water collected in the channel will discharge into the Wailani Drainage Channel, which empties into Middle Loch. At the south end of the golf course grass drainage swale and shared-use path box culvert, storm water collected in the concrete drainage basin will be discharged directly into Middle Loch via the culvert under WPAR.

This second discharge point into Middle Loch was provided because the concrete lined drainage channel would be subject to tidal influences at Wailani Drainage Channel, as described in Section 1.2.2.

1.3.3 Other Improvements

A ten-foot wide shared-use path on the west side of the road will be provided between the OR&L Bicycle/Pedestrian Path and the entrance to the soccer park. The path will bend around the NISMO parking area and be located adjacent to the golf course fence. Other proposed improvements are also shown in Figures 1-4, 1-5 and 1-6. Also, the OR&L Bicycle/Pedestrian Path crossing at WPAR will be made more visible to approaching motorists by providing additional signage warning motorists about the crossing, and delineating or marking the path crossing on the roadway.

The proposed project will provide the following additional features:

- Landscaping will be planted between the roadway and shared-use path on the west side of WPAR, providing a buffer between pedestrians and roadway. At the NISMO parking area, the landscaping will follow the alignment of the shared-use path and provide a buffer between parked cars and pedestrians.
- Bridge No. 2 will be upgraded by improving its approaches, and replacing its railings. These improvements might require that the project upgrade the bridge's seismic capabilities so that they are up to current standards.
- Consistent lighting will be provided along WPAR to improve the safety for vehicles and pedestrians traversing the roadway, based on City standards. Current lighting along WPAR consists of street lamps at inconsistent intervals.
- The golf course fence will be improved to a 12-foot high ball-barrier chain link fence in the area of the NISMO parking area, to prevent errant golf balls from hitting parked cars.

⁴ Both concrete drainage swales will be maintained by the Department of Facility Maintenance.

1.3.4 Property Requirements, Estimated Cost, and Schedule

The proposed project will acquire additional right-of-way by re-designating City property within the Ted Makalena Golf Course on the west side of the road at the following locations:

- Between the OR&L Bicycle/Pedestrian Path and Bridge No. 2 that would result in relocating the existing chain link fence approximately eleven feet to the west, a loss of approximately 0.03 acres;
- North of Bridge No. 2 for the concrete drainage swale, which would require moving the property line approximately seventeen feet west, a loss of about 0.05 acres; and

In a letter dated June 1, 2004, the City and County of Honolulu, Department of Enterprise Services (DES), the City agency responsible for operating the golf course, stated the willingness to relinquish the right-of-way needed for the project (see Appendix C).

At Bridge No. 2 the WPAR right-of-way would need to be increased about 25 feet east into Navy property to accommodate the concrete drainage swale, a loss of 0.05 acres. The City's roadway easement with the Navy would need to be amended to reflect this change.

Although the golf course drainage swale does not require the project to re-designate additional golf course property, it would still require use of some land. The area where the swale will be located is currently comprised of grass, trees, and shrubbery and is considered an unplayable "out of bounds" area, outside of the 18th hole fairway. The swale will be gently sloped (10:1 ratio). The other locations where golf course property would be re-designated for WPAR are not located in "playable" areas (i.e., not within a fairway or rough), or totally outside the golf course, such as the concrete drainage basin south of the golf course driveway.

The estimated cost for the proposed project is \$4.5 million. The present project schedule is shown on the table below. Design, permitting, and property re-designations are expected to last up to one year. These activities are scheduled to begin in the fall of 2005 after completion of environmental review. Construction of the project could begin as soon as 2006. However, the entire schedule is dependent upon funding availability for design and construction.

**Table 1-1
Proposed Project Schedule**

Activity	Period
Design and Permitting	Fall 2005
Property Re-designations	2005-2006
Construction	2006

Source: City and County of Honolulu, Department of Transportation Services, October 2004.

1.4 ALTERNATIVES

Two other improvement alternatives to the proposed action were considered, but were deemed not feasible. Both alternatives would provide bicycle lanes and sidewalks between the OR&L Bicycle/Pedestrian Path and the soccer park. Like the proposed project described in Section 1.3, both alternatives would provide parking for NISMO personnel and improved drainage. The difference between the alternatives and the proposed project is how NISMO parking and drainage are addressed. The following provides a description of the alternatives and the bases for why they were deemed not feasible.

1.4.1 Alternative 1: Paved Diagonal Parking

Alternative 1 would maintain the NISMO parking at the same location as existing, but would provide a paved parking area with diagonal parking. A one-way south bound circulation aisle on the roadway side of the parking area would accommodate parking maneuvers, and circulation in the area (See Figures B-1, B-2, and B-3 in Appendix B). This alternative would require the roadway cross-section be shifted towards the golf course fence, on top of the existing drainage ditch. Therefore, the drainage improvements under Alternative 1 would have been the same as the proposed action: golf course grass drainage swale, box culvert, etc.

Because Alternative 1 would require relocating the property line about 23 feet into the golf course, a twenty-five foot high vinyl coated fence to replace the existing golf course fence would have been required to prevent golf balls from hitting parked cars. A retaining wall along the new golf course property line would also be required because of a three-foot difference in elevation between the paved parking area and the golf course.

Alternative 1 was deemed not feasible because it would require a half-acre more property from Ted Makalena Golf Course than the proposed action. Moreover, Section 4(f) of the Department of Transportation Act allows federal transportation projects to use land from a significant publicly-owned park, only when FHWA has determined that there is no feasible and prudent alternative to the use of that land, and the project includes all possible planning to minimize harm to the property. Because Alternative 1 would have required use of land adjacent to the golf course's 18th hole fairway, and another feasible and prudent alternative proposing less impact to the property was available, Alternative 1 was rejected from further consideration.

1.4.2 Alternative 2: Relocation of Parking Area and Improved Drainage Ditch

Alternative 2 would relocate the NISMO parking area to a vacant grassy area next to the golf course parking lot (See Figures B-4, B-5 and B-6 in Appendix B). The grassy area would be converted to a paved parking area with access from the golf course driveway. Signage would be provided so that golfers would be aware that this parking would be only for NISMO personnel. The parking area would require approximately 0.96 acres from the golf course, but this area is not a "playable" part of the course. The affected area is currently used for overflow parking, but according to golf course personnel,

overflow parking is not needed during weekdays. Since NISMO personnel do not need the parking during weekends, the parking area could be shared by overflow cars.

Alternative 2 would be able to maintain the existing unlined drainage ditch because the parking would be removed from this area, but would convert it to a concrete lined drainage channel. The existing discharge point into the Wailani Drainage Channel would remain, but a second discharge point would be provided just north of the golf course entrance, which would be the same as the proposed action (see Section 1.3.2).

Alternative 2 was deemed not feasible because it would not fulfill the GOE requirement of maintaining parking adjacent to the NISMO facility. The parking area would be about 550 feet from the entrance to the NISMO. The Navy objected to this alternative because it would affect the daily operations of their facility.

CHAPTER 2 ■

WAIPIO POINT ACCESS ROAD Improvements

FINAL ENVIRONMENTAL ASSESSMENT

CHAPTER 2 ENVIRONMENTAL SETTING, IMPACTS, AND PROPOSED MITIGATION

This chapter describes the environment of the project area, potential impacts of the proposed project, and proposed mitigation. Sections in this chapter address the various environmental or social disciplines analyzed for the proposed action. In general, each section describes existing conditions, any existing regulatory requirements pertaining to the discipline, potential impacts of the action, and mitigation measures, if any.

2.1 PHYSICAL ENVIRONMENT

2.1.1 Topography, Geologic Conditions, and Hazardous Materials

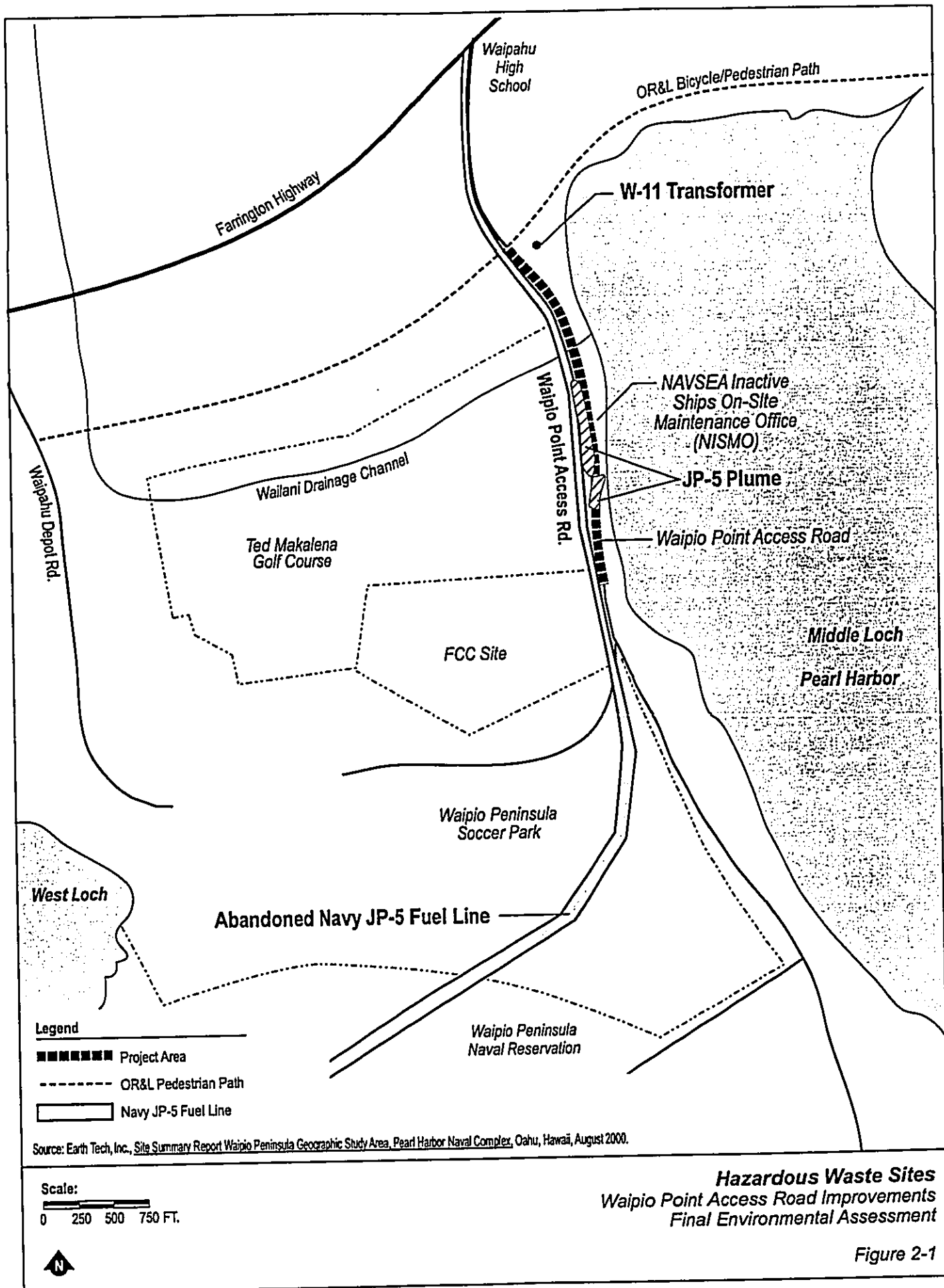
2.1.1.1 Existing Conditions

The project site is located on Waipio Peninsula, which extends into Pearl Harbor on the southern side of Oahu's large coastal plain (see Figure 1-1). The peninsula was originally a ridge separating river valleys that have since been submerged to become Middle Loch and West Loch. Large portions of the peninsula were originally marshy and/or had been made into fishponds by native Hawaiians prior to western contact. These areas were later filled for sugar cultivation. Like most of the Waipio Peninsula, the project site is basically flat and ranges from approximately four to eleven feet above mean sea level (msl). The north project limit at the OR&L Bicycle/Pedestrian Path has the highest elevation within the limits of the project, but is only about 26 feet above msl. From this point, the elevation quickly drops to about 10 feet above msl, just south of Bridge No. 2.

The geology of Waipio Peninsula is typical of that found along the southern flank of the coastal margin of the Koolau shield volcano. Generally, the subsurface conditions are dominated by fossilized coral/algae reef deposits and Koolau Basalt. The soil along WPAR is Pearl Harbor Clay (Ph). These soils are usually found on low coastal plains adjacent to the ocean. The permeability of this type of soil is generally slow and the possibility of erosion is slight (Soil Survey of Islands of Kauai, Oahu, Maui, Molokai, and Lanai, State of Hawaii, August 1972).

Based on a Site Summary Report Waipio Peninsula Geographic Study Area, Pearl Harbor Naval Complex (August 2000) report, two hazardous waste materials sites are in close proximity to the project area: an abandoned Navy JP-5 fuel pipeline and a transformer station (See Figure 2-1).

A fuel release from the Navy's Fleet and Industrial Supply Center (FISC) JP-5 pipeline situated along WPAR was identified in September 1996. Petroleum contamination was found in the road's drainage ditch and across the street in the NISMO facility where petroleum seeped through cracks in the asphalt. In December of 1996, remediation work was done to abandon, cap, and replace the leaking portion of the pipeline. In 1999, the entire pipeline was taken out of service. Thereafter, the Navy installed



Hazardous Waste Sites
Waipio Point Access Road Improvements
Final Environmental Assessment

Figure 2-1

groundwater monitoring wells within WPAR (see Figure 2-2), and has been monitoring groundwater quality annually. This monitoring resulted in the subsequent identification of two plumes: one along WPAR and one within the NISMO (see Figure 2-1 and 2-2). Since late 2003 the Navy has been in the process of remediating the contaminated groundwater and soil through a combination of sorbent socks with monitored natural attenuation to ensure the bio-gradation of petroleum contaminants in the groundwater (December 2, 2004 telephone conversation).

A pad-mounted transformer station near WPAR on the south side of the OR&L Bicycle/Pedestrian Path (Navy reference: Transformer Station W-11) was found to contain high concentrations of polychlorinated biphenyls (PCB) (see Figure 2-1). Periodic testing of the transformer resulted in PCB contamination on the adjacent ground when sampled transformer fluid was discarded on the ground. Due to this contamination, the W-11 transformer site underwent remedial work in 2004 by the Navy to cleanup the contamination. While performing this work, buried trash was found within the contaminated soil. The Navy has stated that the PCB contamination was excavated and the restoration of the site with treated or clean soil was completed (December 2, 2004 telephone conversation).

2.1.1.2 Potential Impacts

The proposed project will not require substantial excavation other than what is needed to provide drainage culverts, swales, and channels. In addition, the project will not require substantial earthmoving activities, or structures beyond the shared-use path. Therefore, the project will not affect the geologic conditions of the study area, nor will any topographic feature be changed.

See Section 2.4.2 regarding impacts to the construction of the project because of the JP-5 and transformer contamination.

2.1.1.3 Mitigation Measures

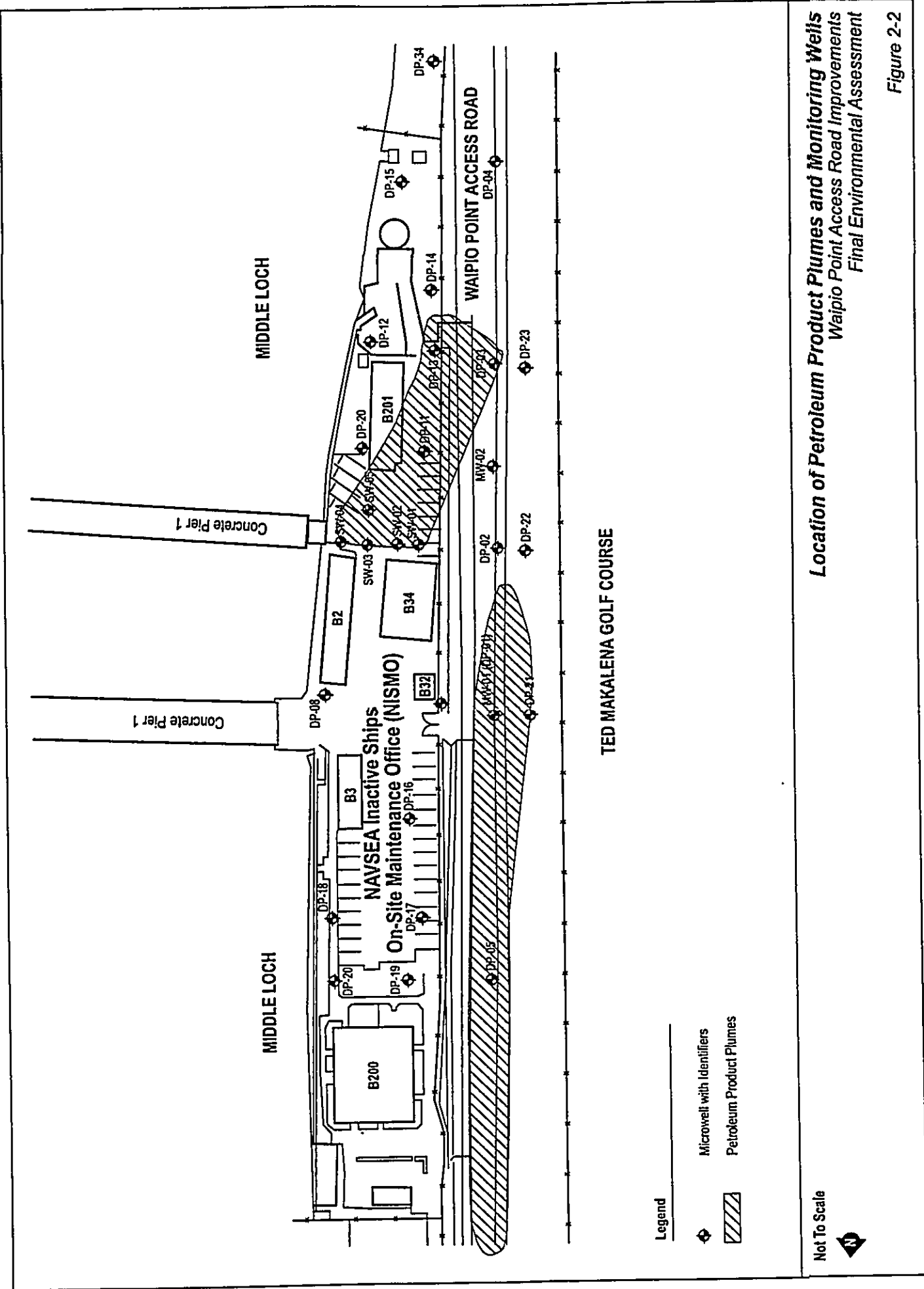
See Section 2.4.2 regarding mitigation measures that would be implemented during construction because of the JP-5 and transformer contamination.

2.1.2 Water Resources

2.1.2.1 Existing Conditions

Surface waters in the general vicinity of the project site include Wailani Drainage Channel, which crosses WPAR under Bridge No. 2 and Pearl Harbor's Middle Loch (see Figure 2-3). The project area does not contain nor is it near wetlands.

Wailani Drainage Channel is concrete lined and discharges into Middle Loch, a tributary of Pearl Harbor. Pearl Harbor receives fresh water from perennial and intermittent streams, artesian springs, and shallow aquifers. Its three primary tributaries, West Loch, Middle Loch, and East Loch are drowned riverbeds that formed a single ancient river when the sea level rose. This river flowed into the Pacific Ocean south of the Schofield Plateau, flooding the valley to form the entrance channel to the



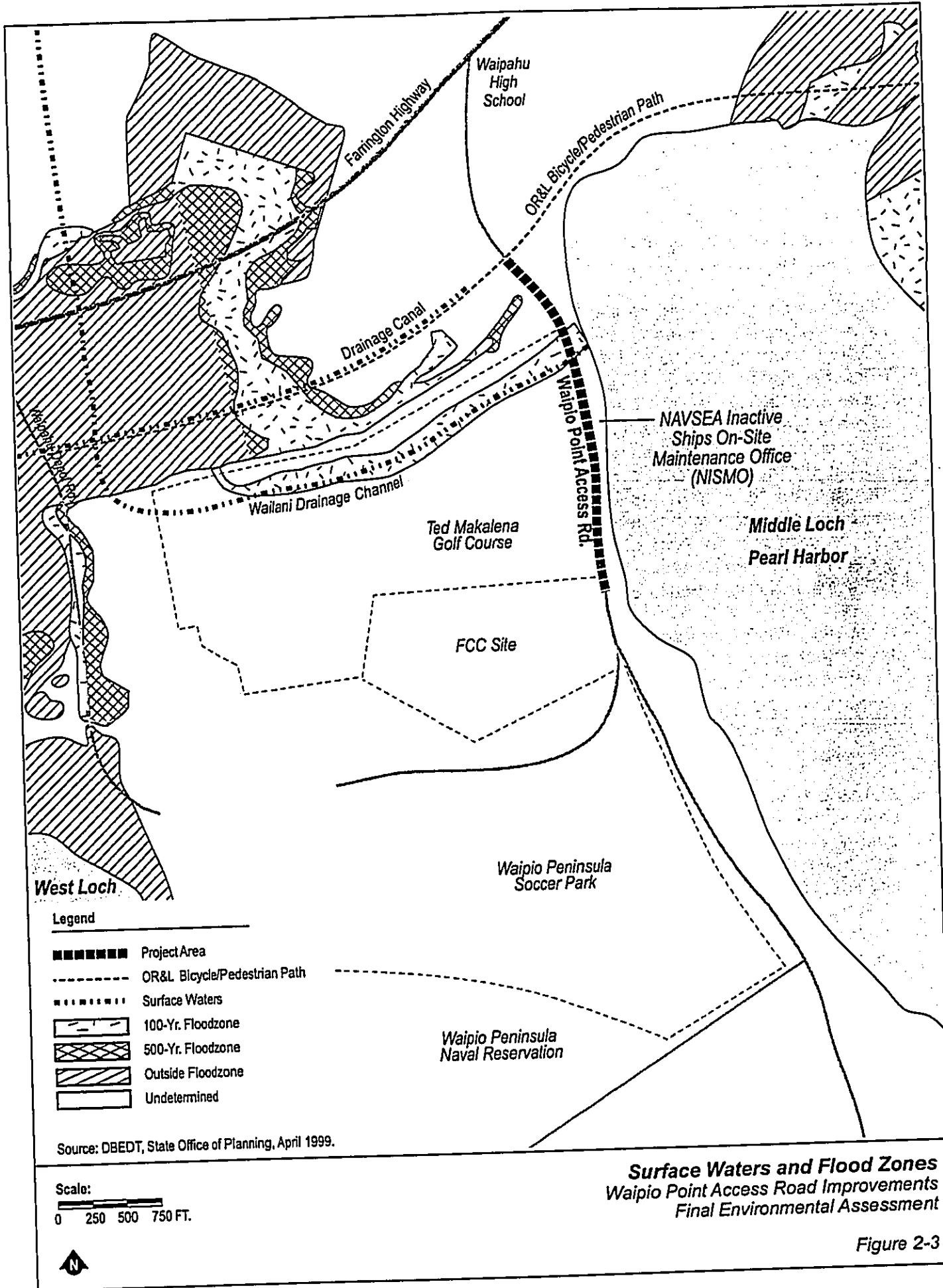
Legend

- ◆ Microwell with Identifiers
- ▨ Petroleum Product Plumes

Not To Scale

Location of Petroleum Product Plumes and Monitoring Wells
 Waipio Point Access Road Improvements
 Final Environmental Assessment

Figure 2-2



harbor. Pearl Harbor's drainage area is comprised of five perennial streams: Halawa, Kaolauao, Waimalu, Waiawa, and Waikele, and encompasses approximately 110 square miles. Depending on the season, an estimated volume of 50 to 100 million gallons of fresh water and 350 tons of sediment enter Pearl Harbor per day.

The State of Hawaii classifies Pearl Harbor as a Class 2 inland estuary. Industrial, agricultural, recreational, and fishery activities are permitted in Class 2 waters, but new industrial discharges are restricted (HAR Title 11, Chapter 54).

The Southern Oahu Basal Aquifer (SOBA), which underlies all of southern Oahu, is an extensive basal aquifer containing large supplies of fresh ground water. Because the SOBA is the primary source of drinking water on Oahu, the U.S. Environmental Protection Agency (EPA) designated it as a sole or principal source aquifer in November 1987. The SOBA is under artesian pressure in the porous basalt below the caprock, a wedge of alluvial sediments and limestone. The caprock layer thins with distance from the shoreline, and at varying distances inland, the caprock layer ends leaving just the basalt layer containing the SOBA. The project site is within the SOBA's Waipahu Aquifer System. According to Hawaii status codes to protect drinking water, this system is rated as a currently used aquifer, which is ecologically important, of low salinity, irreplaceable, and vulnerable to contamination. However, there are no drinking water wells in the vicinity of the project site because groundwater is brackish due to its close proximity to Middle Loch and its low elevation.

According to the Flood Insurance Rate Map (FIRM), the flooding hazard along most of the project site is undetermined (flood zone D) (See Figure 2-3). The Wailani Drainage Channel and surrounding area are located within a designated 100-year flood zone. According to Navy personnel, WPAR has a history of flooding that has affected the NISMO facility, especially at the entrance gate, which is located at the low point of the roadway.

As noted in Section 1.2.2, the existing 1,500-foot unlined drainage ditch along WPAR varies in width from approximately 24 feet at the bridge to 13 feet at the NISMO entrance. The drainage ditch is often unable to contain storm waters that sheet flow from the golf course. The ditch overtops and floodwaters flow over the roadway and into the NISMO facility. This flooding situation is worse during high tide when the height of the water in Wailani Drainage Channel rises above the bottom of the drainage ditch, resulting in channel water backing up the ditch, and impeding the outfall of storm water into the channel. According to the Navy, the drainage ditch also does not convey storm water flows during low tide if the intensity and duration of rainfall is high, and the golf course is saturated. The flooding of the roadway is exacerbated by storm waters north and south of the NISMO entrance flowing toward the NISMO entrance gate, which is located at the low point of the roadway.

A 1994 drainage study conducted for the Navy recommended increasing the capacity of the WPAR drainage ditch. The study also noted the undefined drainage patterns north of the Bridge No. 2. Storm water within this basin starts in the vicinity of Waipahu High School near Farrington Highway and generally flows along WPAR towards Wailani Drainage Channel. To address this flooding situation, the drainage study

recommended that new roadside asphalt concrete swales and drainage cutoff outlets be provided north of the bridge. However, these proposed recommendations were never constructed. NISMO personnel have reported that during storm conditions, runoff from the north side of the Bridge No. 2 flows over the bridge and discharges into the unlined drainage ditch. (April 6, 2004 meeting with the Navy)

2.1.2.2 Potential Impacts

Under the No Build condition, no changes would be made to the drainage system of WPAR. Therefore, the NISMO facility would continue to be vulnerable to flooding under relatively common storm events. NISMO personnel would also have to maintain the ditch continually by removing trash and debris so that the capacity of the ditch remains as high as possible.

As stated in Section 1.3.2, the proposed project will provide new drainage structures and systems on and near WPAR to handle up to a 50-year storm event. The project will provide the concrete swales north of Bridge No. 2 as recommended in the 1994 study. The project, however, will provide a different solution to alleviate the flooding hazard caused by storm water flows from the golf course. By providing a drainage swale in the golf course property and splitting the flows from the swale to two areas, the NISMO facility will be less likely to experience flooding during heavy rains. In addition, flooding will be alleviated by reconstructing roadway grades to allow storm waters to flow toward the west side of the road into the new drainage structures.

In a letter dated May 26, 2004, the U.S. Army Corps of Engineers (USACE) stated that the proposed culvert drainage outlet from the concrete drainage basin next to the golf course will require a Department of Army (DA) permit pursuant to Section 404 of the Clean Water Act and Section 10 of the Rivers and Harbors Act (see Appendix C).

In accordance with Section 1424(e) of the Safe Drinking Water Act, a water quality assessment was submitted to the EPA on April 12, 2004 for review (see Appendix D). In a letter dated April 19, 2004, the EPA stated that it is unlikely that the project will affect the SOBA (see Appendix D).

Since the proposed project will not affect the number of vehicles using WPAR, the level of roadway-related pollutants (petroleum products, rubber, etc.) entering nearby surface waters, due to roadway run-off will be the same with or without the project.

2.1.2.3 Mitigation Measures

No mitigation measures are required other than implementing the proposed drainage features of the project.

2.1.3 Biological Resources

2.1.3.1 Existing Conditions

Roadside flora resources include the following:

- Overgrown grass, shrubs, and other weedy species along the road, including the drainage ditch and un-maintained areas of the golf course;

- Various trees interspersed within the NISMO parking area
- Two large potted plants, flanking the golf course driveway;
- Coconut tree just north of the golf course driveway;
- Monkeypod trees along the golf course parking lot fence, south of the golf course driveway; and
- Bougainvillea hedge at the Waipio Peninsula Soccer Park sign.

Golf course vegetation immediately on the west side of the fence fronting the NISMO parking area consists of typical golf course managed landscaping including trees, shrubs, and grass. Several species of trees are planted adjacent to the fence. This golf course landscaping is maintained by the golf course and is periodically mowed, trimmed, and watered.

A total of 19 introduced avifaunal species were identified in a survey conducted for the Waipio Peninsula Soccer Park Conceptual Master Plan EIS. The most abundant species observed were the Zebra Dove (*Geopelia striata*), Common Myna (*Acridotheres tristis*), Red-whiskered Bulbul (*Pycnonotus jocosus*), and House Finch (*Carpodacus mexicanus*). Only one native species, the Northern Shoveler (*Anas clypeata*), was observed flying over the site heading northeast. Other faunal species likely to be in the general vicinity are the domestic dog (*Canis familiaris familiaris*), small Indian mongoose (*Herpestes auropunctatus*), and rodents such as roof rats (*Rattus rattus*), Norway rats (*Rattus norvegicus*), and house mice (*Mus musculus*). The mammalian species likely to be in the project area are introduced and are a threat to the avian population.

Consultation with the U.S. Fish and Wildlife Service (USFWS) was initiated per requirements of Section 7 of the federal Endangered Species Act (ESA), and copies of correspondence are located in Appendix D. ESA Section 7 requires that federal agencies consult with the USFWS and/or the National Marine Fisheries Service (NMFS) to ensure that actions they authorize, fund, or carry out are not likely to jeopardize the continued existence of any federally listed threatened or endangered species, or result in the destruction or adverse modification of a critical habitat. "Endangered" species are those that are in danger of extinction throughout all or a significant part of their ranges. A "threatened" species is one which is likely to become an endangered species in the foreseeable future.

In a letter dated June 18, 2004, the USFWS stated that the proposed project site contains no federally listed endangered, threatened or proposed threatened or endangered species. The USFWS also stated that the project site does not contain nor is part of a proposed or designated critical habitat (see Appendix D).

2.1.3.2 Potential Impacts

Under the No Build condition, the existing floral and faunal environment would remain the same as today. For example, some areas adjacent to WPAR with grass and weedy species would remain un-maintained. Therefore, the habitat for the introduced species that frequent the area would remain the same.

The proposed project will require the removal of patches of grass and weedy vegetation along WPAR and within the drainage ditch. However, notable vegetation displacements will also be required. Four Monkeypod trees immediately south of the golf course driveway and one coconut tree immediately north of the driveway will be displaced because of the proposed concrete drainage basins. Excavation of the golf course grass drainage swale will also require the relocation of 26 trees on golf course property. The grass drainage swale will be located outside of the golf course's 18th hole fairway. The species of the displaced trees include Ironwood (*Casuarina equisetifolia*), Royal Poinciana (*Delonix regia*), Plumeria, Cook Pine (*Araucaria Columnaris*), Milo (*Thespesia populnea*), Vertical Wili Wili, Be-Still (*Cascabela peruviana*), Tulip Wood (*Harpullia pendula*), and Eastern White Cedar (*Thuja occidentalis*).

The project is not anticipated to cause adverse effects to avian species previously observed in the vicinity of the project site, which are mostly introduced. Although tree displacements described above may possibly displace the nests of birds that frequent the area, they would likely nest in adjacent trees not affected by the project.

2.1.3.3 Mitigation Measures

The proposed project will provide landscaping within a four foot wide strip along WPAR. All the trees displaced in the golf course will be relocated elsewhere in the property in coordination with the Department of Enterprise Services (DES), the City agency responsible for operating the golf course. Relocation plans will be reviewed by the DES to determine the appropriate replacement areas in the golf course.

2.1.4 Air Quality and Noise

2.1.4.1 Existing Conditions

Air quality throughout the island is generally good due to prevalent trade winds and on-shore breezes that help disperse most urban air pollutants. Data collected by the State of Hawaii Department of Health (SDOH) indicate that the State has some of the best air quality conditions in the nation. To monitor air quality, the SDOH operates a network of stations at various locations throughout the State. Each station, however, typically does not monitor the full complement of air quality parameters.

The SDOH monitoring stations closest to the project area are located in Pearl City, a mile east of the project site and Kapolei in Ewa, which is several miles west of the project site. The Pearl City station measures only PM-10 (particulate matter 10 microns or less) and PM-2.5 (particulate matter 2.5 microns or less), pollutants that are not relevant to the operation of a roadway (i.e., operation of motor vehicles). The Kapolei monitoring station measures carbon monoxide (CO), a pollutant of motor vehicles. The latest available data from SDOH for the years 2001 and 2002 indicate no period in which National and State Ambient Air Quality Standards (AAQS) were exceeded. Although specific data at the project site is not available, it is likely that all National and State AAQS are currently being met because WPAR carries low vehicular volumes (see Section 2.2.1).

As noted in Section 2.3.1, the land uses near WPAR do not support industrial uses or other uses that produce high noise levels. The NISMO facility consists mostly of offices, and almost all ship-related activities occur on the vessels moored in Middle Loch (see Section 2.3.2). Although some vessel activities produce high noise levels, they usually cannot be heard from WPAR. The main sources of noise in the project area are vehicles using WPAR. However, the posted speed limit on WPAR is only 20 mph, and the road carries small traffic volumes (see Section 2.2.1). Speed and traffic volume are two major factors in how much noise would be produced from a roadway. Nevertheless, there are no noise sensitive land uses within the project limits. Residences and a school, two types of noise sensitive land uses, are located next to WPAR north of the project limits.

2.1.4.2 Potential Impacts

Ambient air quality and noise conditions along WPAR will be the same with or without the proposed project because the main source of air pollutants and noise from traffic will be unaffected by the project (see Section 2.2.1).

2.1.4.3 Mitigation Measures

Because air quality and noise impacts are not anticipated, no mitigation measures are required.

2.2 INFRASTRUCTURE

2.2.1 Roadway and Traffic System

2.2.1.1 Existing Conditions

WPAR is classified as a urban major collector with a posted speed limit of 20 mph within the project limits. At its north boundary, WPAR intersects with Farrington Highway, about 1,400 feet north of the project limits. The south boundary of WPAR is the entrance to Waipio Peninsula Soccer Park, which is also the south end of the project limits. Farrington Highway, a four-lane divided facility, is a major arterial roadway that runs throughout Waipahu town, and extends further west to the Waianae Coast. Farrington Highway connects with the H-1 Freeway and Kamehameha Highway at Waiawa Interchange, which is located a little less than a half-a-mile east of the WPAR/Farrington Highway intersection (see Figure 1-1). The SDOT is currently planning a secondary access road to Leeward Community College, which is located a short distance northeast of the project site. The new road will connect with WPAR just north of Bridge No. 1, the bridge just north of the OR&L Bicycle/Pedestrian Path crossing. This project is currently on-hold.

During weekdays, traffic is mostly generated by the NISMO facility and Ted Makalena Golf Course, and together, their total volumes are relatively low. The soccer park generates relatively small traffic volumes during weekdays, except during tournaments, which occur about 15 to 20 times a year. Generally less than half of the tournaments occur during weekdays (December 2, 2004 telephone conversation with Department of Parks and Recreation).

The peak south-bound and north-bound traffic volumes, which occur at 6:30 a.m. and 3:30 p.m., respectively, coincide with the hours of operation of the NISMO facility since most workers start and finish work at the same time. However, traffic conditions evaluated during a major soccer tournament held on July 2 to 6, 2003 indicated that the north-bound or south-bound peak hour traffic volumes associated with the tournament did not coincide with the peak traffic volumes generated by the NISMO facility or Ted Makalena Golf Course (see Appendix F). During the soccer tournament, the peak hour for south-bound (to the soccer park) traffic (517 vehicles) occurred at 9:30 a.m. to 10:30 a.m., which is three hours later than when most NISMO personnel start work. The peak hour for north-bound (away from the soccer park) traffic (670 vehicles) occurred at 6:30 p.m. to 7:30 p.m., which is three hours later than when most NISMO personnel finish work. The two-way peak hour (420 vehicles south-bound and 394 north-bound) occurred at 3:30 p.m. to 4:30 p.m., which coincides with the NISMO working hours. Workers who park in the area across from the NISMO facility have reported that during soccer tournaments they sometimes have difficulty pulling into WPAR when they leave. During normal weekdays (i.e., no soccer tournament), traffic on WPAR is free-flowing with very low volumes.

Based on the traffic volumes collected during the July 2003 soccer tournament, WPAR operates at a LOS D (the second worst traffic condition, with LOS F being the worst) during the south-bound, north-bound, and two-way peak hours. Operational difficulties at the Farrington Highway / WPAR intersections often occur during soccer tournaments, which result in traffic queues that extend south within the project limits, despite the project site being 1,400 feet from the intersection. Although LOS D represents long traffic delays, these kinds of conditions occur relatively infrequently.

2.2.1.2 Potential Impacts

As noted in Sections 1.2 and 1.3, the proposed project will not increase the vehicular capacity of WPAR. It will retain its current two-lane configuration regardless of whether the project is implemented or not. Therefore, the traffic impact analysis provided in this section applies to both the proposed project and the No Build condition.

Traffic operations on WPAR was analyzed based on a year 2025 full build-out condition of the soccer park, which would include 13 additional soccer fields, an expansion of the soccer stadium to accommodate an additional 6000 spectators, and include training facilities and administrative offices. A major soccer tournament under a year 2025 build-out condition would increase south-bound, north-bound, and two-way peak hour volumes to 855, 1105, and 1345 vehicles, respectively, which are about 65 percent greater than present tournament conditions. Despite this expected increase in tournament traffic conditions, operations on WPAR was analyzed to operate at LOS D, the same as under current tournament conditions. However, the percent time spent following (PTSF) factor, which measures operational mobility of vehicles using the road, increased from 71.7 percent to 77.6 percent, 78.2 percent to 79.9 percent, and 73.5 percent to 80.5 percent for the south-bound, north-bound, and two-way peak hours, respectively. A higher PTSF percentage means that traffic movements are more constrained than a lower PTSF percentage. Therefore, although traffic conditions associated with soccer tournaments would cause adverse traffic conditions on WPAR,

these events would continue to be relatively infrequent, such as one or two a month. Refer to Appendix F for the traffic evaluation.

The use of or land uses associated with the NISMO facility and Ted Makalena Golf Course are not anticipated to change substantially. Therefore, the traffic they generate is also not expected to change substantially. Also, other than the proposed amenities and facilities at the soccer park, no new land uses are proposed along WPAR. Therefore, current traffic conditions on WPAR are expected to continue on normal weekdays in the future, regardless of whether the proposed project is implemented or not.

2.2.1.3 Mitigation Measures

Because traffic conditions on WPAR are not anticipated to change, the project does not propose to increase the vehicular capacity of the road. Therefore, no mitigation measures are required.

2.2.2 Bicycle and Pedestrian Facilities

2.2.2.1 Existing Conditions

WPAR currently has no defined bicycle facility and limited pedestrian facilities, which include 3.5-foot sidewalks on both sides of Bridge No. 2. The OR&L Bicycle/Pedestrian Path currently crosses the roadway about 240 feet north of Bridge No. 2, but it is not demarcated in any way other than signage warning motorists of the crossing.

2.2.2.2 Potential Impacts

Under the No Build condition, the existing pedestrian facilities would remain the same, and no additional bicycle facilities would be provided. In comparison, the proposed project will provide a ten-foot wide shared-use path on the west side of the roadway to accommodate bicycles and pedestrians. In addition, the OR&L Bicycle/Pedestrian Path crossing will be demarcated and additional signage provided to warn motorists of the crossing (see Section 1.3.3). These bicycle and pedestrian improvements will support alternative modes of transportation to and from the soccer park and other land uses adjacent to WPAR. By making the OR&L Bicycle/Pedestrian Path crossing more visible, the safety of cyclists and pedestrians using the path to cross WPAR will improve.

2.2.2.3 Mitigation Measures

The proposed project will improve bicycle access and pedestrian safety on WPAR. Therefore, no mitigation measures are required.

2.2.3 Parking Facilities

2.2.3.1 Existing Conditions

The only allowable parking provided along WPAR is the NISMO parking area, which is located on the west side of WPAR, across from the facility's entrance. The area, which

has space for roughly 75 vehicles parked perpendicular to the roadway, is used by Navy contractors and visitors. The area is not paved, nor stalls marked, but it is stabilized with gravel, and defined by concrete tire stops imprinted with "Reserved for NISMO Parking". The number of parked vehicles at any given time depends on the workload of the NISMO.

Fishermen using Bridge No. 2 to cast fishing lines have been observed illegally parking their cars within the bridge's shoulder lane, north of the bridge on both sides of the road, and southwest of the bridge.

The soccer park and golf course currently have ample parking. In addition, the golf course has an overflow area next to its paved lot.

2.2.3.2 Potential Impacts

Under the No Build condition, the NISMO parking area would remain the same. In comparison, as stated in Section 1.3.1, the NISMO parking area will be generally at the same location. The area will also be paved, and the parking stalls will be marked, including four ADA compliant stalls and stalls for delivery/service vehicles. The total number of stalls will increase from approximately 75 to between 90 to 100. Vehicles will still have to perform the same backing and pull-out maneuvers as the No Build. The project provides a long term solution to the NISMO parking situation (see Section 1.2.1).

2.2.3.3 Mitigation Measures

The proposed project will improve the NISMO parking area. Therefore, no mitigation measures are required.

2.2.4 Utilities

2.2.4.1 Existing Conditions

The WPAR right-of-way contains a three-inch underground water line running mostly along the west side of WPAR, which provides water to the NISMO facility and to a Federal Communications Commission (FCC) facility (see Section 2.3.1). A Navy four-inch underground pressure sewer line runs from the NISMO facility to a receiving City and County of Honolulu manhole located in the Ted Makalena Golf Course. In addition, overhead electric and telephone utility lines owned by Hawaiian Electric Company and Verizon Hawaii, respectively, run along the west side of WPAR. The wooden poles supporting the utility lines also support street lamps at inconsistent intervals. Also, Verizon Hawaii owns an underground cable extending from just north of the golf course driveway traversing into the golf course. A section of the utility corridor containing petroleum pipelines from Barbers Point on the Leeward Coast to Honolulu runs along the OR&L Bicycle/Pedestrian Path.

2.2.4.2 Potential Impacts

Under the No-Build condition, the location of the existing utilities within the WPAR right-of-way would remain the same. There are no plans for new utilities within the right-of-way. The proposed project will require relocating nine wooden utility poles to within the

proposed four-foot wide landscaping/utility strip. A utility pole on the east side of WPAR, north of Bridge No. 2 will need to be relocated to avoid conflict with the concrete drainage swale. Those poles located within the NISMO parking area may remain in place, and the new parking area will be marked and landscaped accordingly to accommodate the poles. In addition, the underground telephone cable north of the golf course entrance may need to be relocated due to the proposed concrete drainage basins. The three-inch water line may require relocation depending on excavation plans developed during the project's design phase. The Navy's four-inch pressure sewer line is believed to be relatively shallow and may conflict with the proposed box culvert and therefore, may require relocation or adjustment.

2.2.4.3 Mitigation Measures

Utility relocations will occur during construction. See Section 2.4.5 regarding utility mitigation measures during construction.

2.3 SOCIAL ENVIRONMENT

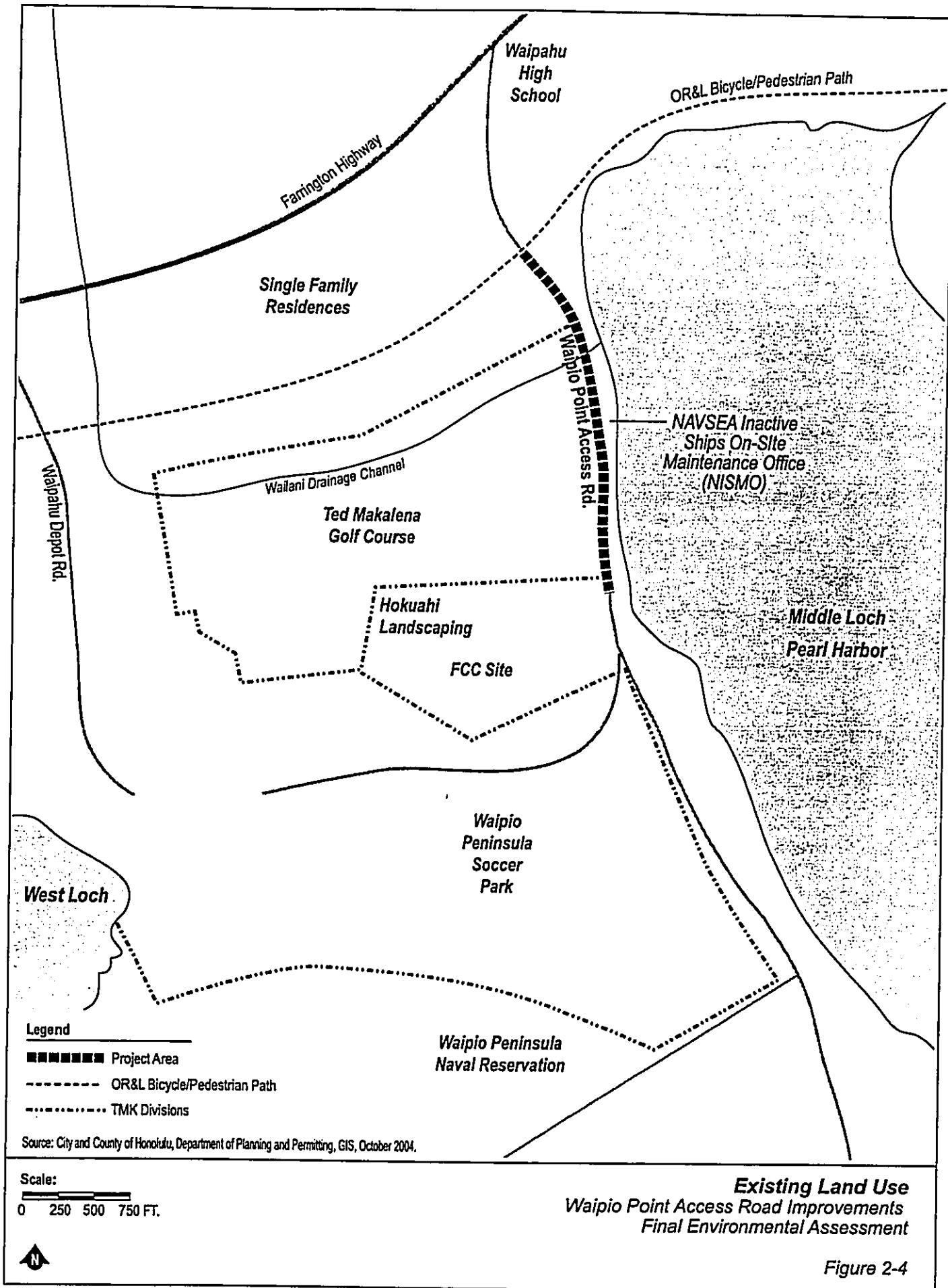
2.3.1 Land Use

2.3.1.1 Existing Conditions

The project site is located in Waipahu, one of Central Oahu's oldest communities. Waipahu is generally bounded by Waiawa Interchange to the east, Pearl Harbor West Loch to the south, the H-1 Freeway to the north, and Fort Weaver Road to the west. Originally a set of plantation villages built around the Waipahu Sugar Mill and segregated by ethnicity, Waipahu has transformed into a suburban community with residential, commercial, and industrial land uses.

The land uses adjacent to WPAR within the project limits include Waipio Peninsula Soccer Park (which occupies approximately 300 acres of the Waipio Peninsula), Ted Makalena Golf Course, the NISMO facility, and an FCC facility combined with Hokuahi Landscaping (see Figure 2-4). Information about Waipio Peninsula Soccer Park and Ted Makalena Golf Course is provided in Section 2.3.4, and Section 2.3.2 provides information about the function of the NISMO facility. The FCC facility and Hokuahi Landscaping, a private business, share a common driveway off of WPAR next to the entrance to the soccer park. The FCC facility contains high frequency direction finding radar that assists with rescues, detecting foreign interference with communication networks, and regulating civilian radio frequency communications.

Land uses further from the project site include the U.S. Navy's NAVMAG (Naval Magazine) facility, which is accessed from a government road within the soccer park property; Waipahu High School, located at WPAR's intersection with Farrington Highway; and a single-family residential neighborhood that extends south from Farrington Highway to Bridge No. 1, outside of the project limits (see Figure 2-4).



2.3.1.2 Potential Impacts

As stated in Section 1.3.4, the proposed project requires a small amount of additional property from Ted Makalena Golf Course. In addition, the proposed project includes a drainage swale within the golf course. Despite these requirements, no "playable" portion of the golf course will be affected.

Regardless of whether the proposed project is implemented, changes in land uses in the general vicinity of the project are not expected because much of the area is in government ownership or use, such as the NISMO facility, the golf course, the soccer park, the FCC facility, and Waipahu High School. It is possible in the future that the one private business next to WPAR, Hokuahi Landscaping, may change, or that the single-family residential neighborhood may convert to other land uses or become higher density. Nevertheless, these changes, if any, will not be influenced by the proposed project because the project will not provide the kind of improvements that affect land use decisions, such as increase of vehicular capacity.

2.3.1.3 Mitigation Measures

Because the proposed project will not result in land use displacements nor cause changes to long term land use patterns, no mitigation measures are required.

2.3.2 Cultural, Social and Economic Conditions

2.3.2.1 Existing Conditions

Potential cultural activities practiced within the project limits may involve fishing and gathering activities in Middle Loch, some of which occur from Bridge No. 2, and from the OR&L Bicycle/Pedestrian Path east of WPAR. As disclosed in Section 2.3.3, a buried historic fishpond, "Loko Eo", is located in the project area. However, there are no known cultural practices associated with the fishpond

Most social activities in the project area are associated with the two recreational facilities, Ted Makalena Golf Course and Waipio Peninsula Soccer Park. Information about these two recreational resources is provided in Section 2.3.4. Although Waipahu High School is near the project site, its social influence does not extend to the project site because there are no residences south of Bridge No. 1. Police, ambulance, and fire services are provided from stations in Waipahu and in Pearl City, the community to the east of Waiawa Interchange.

Economic activities within the project limits involve operation of the FCC facility, Hokuahi Landscaping (a private business), the NISMO facility, and Ted Makalena Golf Course. The mission of the NISMO facility is to be the custodians of and to provide for the inactivation, safe stowage, security, maintenance, cannibalization, disposal, and readiness and preparation for activation of decommissioned or retired U.S. Naval ships and crafts. These vessels are moored offshore from the NISMO facility within Middle Loch, and almost all industrial work associated with the NISMO facility is done on the vessels.

2.3.2.2 Potential Impacts

Existing cultural, social, and economic activities will be unaffected regardless of whether the proposed project is implemented. Recreational fishing will continue. In addition, the OR&L Bicycle/Pedestrian Path will not be altered by the proposed project. Emergency police, ambulance, and fire response to the area will not be affected because WPAR will remain a two-lane facility. Other economic activities identified will continue their current operations with or without the proposed project.

2.3.2.3 Environmental Justice

As described in Section 2.3.1, no residences are located within the project limits. Therefore, no minority or low-income populations as defined in "FHWA Actions to Address Environmental Justice in Minority and Low-Income Populations (December 2, 1998)" will experience disproportionately high or adverse effects from the project.

2.3.2.4 Mitigation Measures

Because the project will not affect existing cultural, social, and economic activities, no mitigation measures are required.

2.3.3 Historic Properties

2.3.3.1 Regulatory Requirements

Section 106 of the National Historic Preservation Act requires that federal agencies consider the effect of their projects on any resource listed on or eligible for the National Register of Historic Places, in coordination with the State Historic Preservation Officer (SHPO). The Advisory Council on Historic Preservation (ACHP) is given an opportunity to review project impacts if appropriate. The Section 106 process has two basic steps: (1) identify historic properties in the project's area of potential effects (APE); and (2) assess effects to historic properties in the APE, and if necessary, mitigate adverse impacts. The APE means the geographic area or areas within which an undertaking may directly or indirectly cause changes in the character of historic properties, if any such properties exist. HRS Section 6E-8 places similar responsibilities on State or county agencies to evaluate their projects. Since the project involves both federal and State agencies, both regulations apply to the project.

2.3.3.2 Existing Conditions

An historic property is any district, site, building, structure, or object that is on or eligible for the National Register of Historic Places. Under State law, historic properties include burial sites, which are defined as, "any specific unmarked location where prehistoric or historic human skeletal remains and their associated burial goods are interred." The FHWA determined that the APE for this project is the area required to construct the proposed improvements, including the depth of excavation.

Consultation with the State Historic Preservation Division (SHPD) was conducted to assist in identifying historic properties within the APE. Using the report, Ancient Hawaiian Fishponds of Pearl Harbor: Archaeological Studies on U.S. Navy Lands,

Hawaii (July 2000), a very large buried fishpond, Loko Eo, was identified on the east side of the upper part of Waipio Peninsula (see Figure 2-5). As an archaeological resource, the fishpond is likely eligible for the National Register, and is probably considered an historic property. Ted Makalena Golf Course was built over most of the fishpond, and the NISMO facility was built over its eastern side. Previous core samples taken from the golf course identified intact fishpond sediments at a depth of 21 feet below the existing surface, which indicates that the upper portions of Loko Eo--above 21 feet--have been disturbed or filled. No other archaeological site was identified in the APE.

2.3.3.3 Potential Impacts

In assessing the effects of a project on a historic property, there can only be one of the following three findings under Section 106:

- No historic properties affected;
- No adverse effect; and
- Adverse effect.

Because no construction activities would occur under the No Build condition, fishpond sediments associated with Loko Eo would remain intact. For the proposed action, fishpond sediments will not be affected because the deepest depth of required excavation ranges from seven to ten feet to accommodate new drainage facilities. As noted above, intact fishpond sediments occur more than 21 feet below the surface. Therefore, Loko Eo falls outside the APE of the project. This assessment was concurred with by the State Historic Preservation Division in their letter of January 27, 2005 (see Appendix D). Based on this information, it is anticipated that the FHWA would render a "no historic properties affected" determination.

2.3.3.4 Mitigation Measures

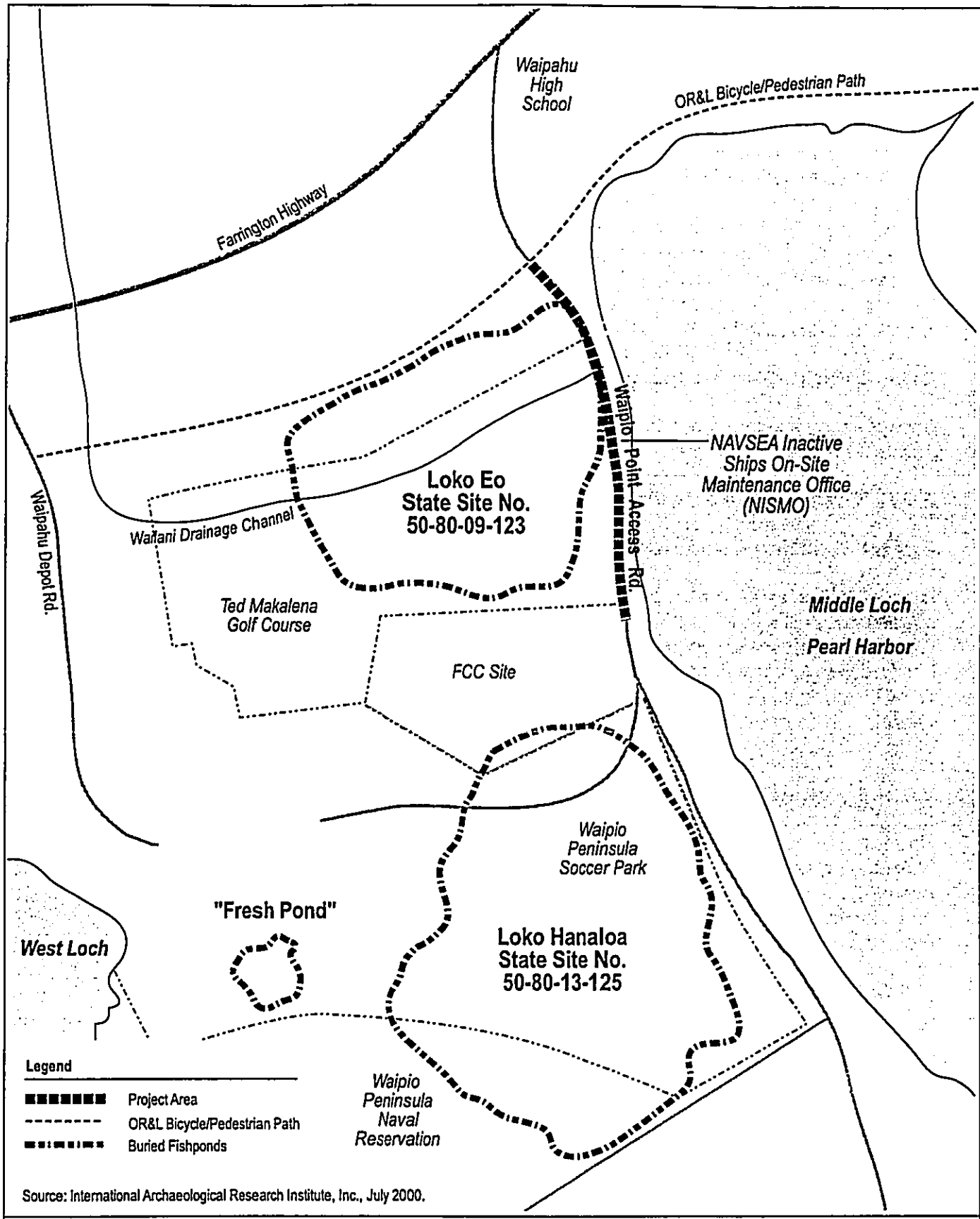
Because the project will not affect historic properties, no mitigation measures are required.

2.3.4 Parks and Recreational Resources

2.3.4.1 Existing Conditions

The two recreational resources in the project area are Ted Makalena Golf Course and Waipio Peninsula Soccer Park (see Figure 2-4). The golf course is a 150-acre 18-hole public municipal course owned by the City and County of Honolulu.

The soccer park, on approximately 300 acres, was developed to promote Oahu and the State of Hawaii as a sports center destination for regional, national, and international markets. The soccer park plays a vital role in increasing the number of soccer practice and game fields in the Central Oahu district. Youth soccer organizations of the American Youth Soccer Organization and Hawaii Youth Soccer Association regularly hold tournaments at the soccer park. During the year on weekends, the soccer fields are used by various soccer organizations. As stated in Sections 1.1.1 and 2.2.1, the soccer park is planned to be expanded in the long term to include more fields, a larger stadium, and other facilities.



Legend

- ■ ■ ■ ■ ■ ■ ■ ■ ■ Project Area
- - - - - OR&L Bicycle/Pedestrian Path
- ■ ■ ■ ■ ■ ■ ■ ■ ■ Buried Fishponds

Source: International Archaeological Research Institute, Inc., July 2000.

Scale:
 0 250 500 750 FT.



Buried Fishponds
 Waipio Point Access Road Improvements
 Final Environmental Assessment

Figure 2-5

2.3.4.2 Potential Impacts

The recreational amenities and services provided by Ted Makalena Golf Course and Waipio Peninsula Soccer Park will essentially be the same, regardless of whether or not the proposed project is implemented. Under the proposed project, non-playable golf course areas (i.e., not within a fairway or rough) will be re-designated for roadway right-of-way and for drainage purposes (see Sections 1.3.2 and 1.3.4). The golf course drainage swale will be excavated in an unplayable "out of bounds" area outside of the 18th hole fairway. The plans for the soccer park will proceed regardless of improvements to WPAR.

2.3.4.3 Mitigation Measures

Because the project will not adversely affect parks or recreational resources, no mitigation measures are required.

2.3.5 Visual and Aesthetic Resources

2.3.5.1 Existing Conditions

From WPAR, scenic views are available in the west direction because of the open space provided by the Ted Makalena Golf Course, and the backdrop provided by the Waianae Mountain Range. However, expansive views of the golf course are partially blocked due to vegetation along the golf course fence line. Scenic views to the east of WPAR include limited views of Pearl Harbor's Middle Loch, and the retired naval vessels, which could be considered a blemish to Middle Loch's aesthetic environment. Views of the roadway itself are scenically unremarkable.

2.3.5.2 Potential Impacts

Under the No Build condition, the overall scenic condition of the roadway and the surrounding area would remain the same as today. With the proposed project, the overall visual and aesthetic quality of WPAR will be improved with the addition of a four-foot wide landscaping strip and the enhancement of roadway facilities and pavement (see Section 1.3.3). Scenic views to the west and east of WPAR will remain the same.

2.3.5.3 Mitigation Measures

The proposed project will improve the scenic and aesthetic condition of WPAR by providing improved landscaping, and will not affect other scenic views. Therefore, no mitigation measures are required.

2.4 CONSTRUCTION IMPACTS

Construction of the proposed project will cause short-term environmental impacts. Since some construction-related impacts are expected, measures to minimize or mitigate these adverse effects are also proposed in this section.

No improvements to the existing WPAR would be made under the No Build condition, although third parties may conduct work on or near the roadway, such as utility installation or maintenance.

2.4.1 Maintenance of Traffic

Construction of the proposed project will require lane closures on WPAR, which will cause motorists to experience some delay and inconvenience. Construction activities will be staged so that no more than one lane is closed at any one time, and access to the NISMO, the golf course, the FCC facility, Hokuahi Landscaping and the soccer park will be maintained during all phases of the construction work. However, travel to and from these destinations may be delayed. Pedestrian movements will be maintained, but may be temporarily relocated to provide safe passage through work areas. Construction of the NISMO parking area will be phased throughout the duration of the construction period so as to maintain a minimum amount of parking, per coordination with Navy officials. If approved by Ted Makalena Golf Course, some NISMO personnel might be directed to park in the golf course property.

Flagmen and/or police officers will be posted to direct traffic safely around lane closures and other potential safety hazards. Lane closures will not be conducted during major tournaments at the soccer park.

2.4.2 Hazardous Materials

Excavation activities may uncover soil and groundwater contaminated by past petroleum releases associated with the JP-5 pipeline along WPAR (see Section 2.1.1). Before construction, test borings along the WPAR will be recovered to assess the likelihood of encountering contaminated media (soil or groundwater) from the JP-5 plume. If it were determined that contaminated soil or groundwater exists within the area affected by excavation, a hazardous waste removal plan will be developed to address the removal and disposal of the contaminated media.

The Navy is currently using groundwater monitoring wells within the roadway to monitor and remediate the JP-5 contamination through . The contractor will be alerted to the locations of the monitoring wells.

During construction, the contractor will be required to practice best management practices, such as ensuring that:

- All waste materials are collected and stored in securely lidded metal dumpsters and not buried on site;
- Materials stored on-site be stored in a neat, orderly manner in appropriate containers (i.e., per manufacturers' recommendations);
- Excavated materials be disposed outside the project site at an approved location;
- All on-site vehicles be monitored for leaks and receive regular preventive maintenance to reduce the chance of leakage; and
- A spill prevention and clean-up plan is prepared and implemented if needed.

2.4.3 Water Resources

Excavation activities will expose areas of un-vegetated soil to the elements (wind and rain). The primary concern would be the potential for erosion by storm water passing through un-vegetated areas, which could result in sedimentation and degradation of water quality in Wailani Drainage Channel and Middle Loch. A National Pollutant Discharge Elimination Systems (NPDES) permit is intended to prevent such storm water discharges during construction. The total construction area will be about four acres, far exceeding the one-acre threshold in which a construction storm water NPDES permit is required. In addition, required utility relocations (see Section 2.4.5) may require dewatering and hydrotesting of new lines. If these waters require discharge into the storm drain system or Waters of the United States, such as Pearl Harbor, appropriate NPDES permits would be required.

The NPDES permit will require the implementation of erosion control measures or Best Management Practices (BMP) during construction. The intent of the BMPs is to prevent erosion from the site, thereby maintaining the quality of nearby water bodies. The BMP plan will be reviewed by the SDOH, Clean Water Branch, during the review of the project's NPDES permit application or Notice of Intent. Generally accepted BMPs applicable to this project include:

- Erecting silt curtains and silt fences;
- Minimizing areas of disturbance;
- Covering stockpiles;
- Immediate planting of vegetation and/or mulching on highly erodible or critical areas; and
- Constructing dikes or diversions to avoid runoff across erodible areas.

2.4.4 Air Quality and Noise

Air quality impacts during construction generally will consist of fugitive dust and mobile source of emissions from construction equipment.

Fugitive dust is airborne particulate matter, of usually large particle size, such as those generated by construction vehicles operating around construction sites and material blown from uncovered haul trucks, stockpiles, and exposed areas. The contractor will be required to adhere to State rules and regulations governing air quality, such as no visible emissions outside the affected parcel. To prevent fugitive dust from affecting areas beyond the project site, contractors will be required to minimize land disturbance, and watering will be used during dry conditions. If needed, windscreens will be used if fugitive dust were likely to disturb nearby residences on the north end of the project. However, the prevailing northeast trade winds will blow fugitive dust towards the golf course and soccer park. Landscaping will be established as early as possible. To prevent haul trucks from tracking dirt onto paved streets, tire washing or road cleaning may be appropriate. State regulations stipulate that open-bodied trucks must be covered at all times when in motion if they are transporting wind-erodible materials.

Construction vehicles emit engine exhaust that will contribute to air pollutants in the vicinity of the work area. Most of the equipment and vehicles are diesel-powered, and

will emit relatively high levels of nitrogen oxide (NOx) in comparison to gasoline-powered equipment. However, standards for such pollutants are set on a regional basis and will therefore not be violated by short-term construction equipment emissions.

Construction will involve the use of heavy machinery that may cause temporary noise impacts to adjacent noise sensitive land uses. Construction will normally occur during daylight hours when occasional loud noises are more tolerable. In addition, most of the construction site will not be near noise sensitive land uses. Therefore, extended noise disruptions to normal activities are not anticipated. SDOH maintains community noise control standards (HAR Section 11-46) that apply to construction noise. The contractor is likely to apply for a noise permit that will allow construction activities to exceed noise control standards during normal business hours.

2.4.5 Utilities

In the design phase, utility relocations will be coordinated with the Navy and applicable utility companies. Substantial planning, including coordination with utility providers, will be conducted to minimize interruptions in utility service to customers. Disruptions to utility service, if necessary, will be restricted to short-term localized events. Careful scheduling of these disruptions and prior notification of properties that would be affected will be implemented to mitigate some of the utility relocation impacts.

2.5 CONSISTENCY WITH GOVERNMENTAL PLANS, POLICIES AND CONTROLS

2.5.1 Hawaii State Plans and Controls

2.5.1.1 Hawaii State Plan

The Hawaii State Plan (June 1991) consists of comprehensive goals, objectives, policies and priorities in all areas of government functions. These functions include the protection of the physical environment, the provision of public facilities, and the promotion and assistance of socio-cultural advancement.

In accordance with the Plan's economic objectives and policies, the project will facilitate transportation objectives by providing improved service for pedestrians and bicycles. It will also contribute to the economy of the City and County of Honolulu and the State of Hawaii by providing largely federally funded construction jobs. An estimated construction expenditure of \$4.5 million will be made.

2.5.1.2 Hawaii State Land Use Controls

Chapter 205, Hawaii Revised Statutes (HRS), relating to the State Land Use Commission (SLUC), regulates land use through classification of State lands into four districts: Urban, Agriculture, Conservation, and Rural. The intent of the land classification system is to accommodate growth and development while retaining the natural resources of the state. Each district has specific land use objectives and development constraints.

WPAR within the project limits is in lands classified for Agriculture, which also encompasses the golf course, soccer park, and the Navy military reservation on the south end of Waipio Peninsula. Despite the State classification, the peninsula currently does not support agricultural activities. Land north of the project limits is classified as Urban, which is consistent with predominately residential land uses.

2.5.1.3 Coastal Zone Management Act (CZM)

The objectives and policies of the Hawaii Coastal Zone Management (CZM) Program are designed to protect and manage Hawaii's valuable coastal areas and resources. The proposed improvements to WPAR are located within the State's CZM area. Pursuant to 15 Code of Federal Regulations (CFR) 930.32, federally-permitted, licensed, or assisted activities undertaken in or affecting Hawaii's coastal zone must be consistent with the CZM objectives and policies.

The following discussion summarizes the project's consistency with the objectives and policies of the State's CZM Program. A more detailed assessment will be submitted for review to the Department of Business, Economic Development and Tourism (DBEDT) Office of Planning, the agency administering the State's CZM program, for review.

Recreation Resources

The proposed project will not affect recreational services and facilities provided at Waipio Peninsula Soccer Park and Ted Makalena Golf Course. By providing a shared-use path, the project will improve access to the soccer park. See Section 2.3.4 for additional information.

Historic Resources

The proposed project will not affect historic properties. The historic fishponds on Waipio Peninsula will not be disturbed by the project construction. See Section 2.3.3 for additional information.

Scenic and Open Space Resources

The proposed project will provide a landscaped strip and a shared-use path, which will improve the visual and aesthetic condition of WPAR. Existing view planes of the golf course and Middle Loch will be unaffected. See Section 2.3.5 for additional information.

Coastal Ecosystems

During construction, BMPs will be implemented to prevent sediments in construction site runoff from affecting water quality in Wailani Drainage Channel and Middle Loch. Once completed, the project will not affect the number of vehicles using WPAR. Therefore, the amount of pollutants generated from vehicles using the roadway will be the same with or without the proposed project. See Sections 2.3.2 and 2.4.3 for additional information.

Economic Uses

The proposed project will not affect business operations at the NISMO, the FCC facility, Hokuahi Landscaping, and Ted Makalena Golf Course. See Section 2.3.2 for additional information.

Coastal Hazards

WPAR is not located in a tsunami evacuation zone or other coastal hazard areas.

Managing Development

The proposed project will require State and City and County of Honolulu permits and approvals that include provisions for public participation and ensure protection of coastal resources. The project will not affect the existing zoning of the adjacent land areas. Zoning information is provided in Section 2.5.2.3.

Public Participation

The public was provided with a 30-day review period to comment on the Draft EA. The Waipahu Neighborhood Board was provided with a presentation of the project on March 17, 2005.

Beach Protection

Although WPAR is near the Middle Loch shoreline, there are no active beaches along the roadway.

Marine Resources

See Coastal Ecosystems above. No other marine resources will be affected because the project will be land-based.

2.5.1.4 Bike Plan Hawaii

Bike Plan Hawaii, A State of Hawaii Master Plan (September 2003) (Bike Plan) prepared by SDOT provides recommendations for improving bikeway systems statewide. It serves as guidance to the SDOT and county transportation agencies when new roadway construction or improvements to existing roadways are contemplated. With respect to the WPAR, the Bike Plan recommended bikeways from Farrington Highway to the Waipio Peninsula Soccer Park. The project is in accordance with this recommendation by providing a shared-use path on the west side of WPAR from the OR&L Bicycle/Pedestrian Path to Waipio Peninsula Soccer Park.

2.5.2 City and County of Honolulu Plans and Controls

2.5.2.1 General Plan

The General Plan of the City and County of Honolulu (1992) is a statement of long-range social, economic, environmental, and design objectives for the Island of Oahu, as well as policies necessary to meet these objectives.

The project is consistent with these objectives and policies in these respects:

- Economy. The proposed project will generate employment opportunities in engineering, construction, and technical support fields.
- Transportation. The project improves bicycle and pedestrian access to the Waipio Peninsula Soccer Park and is consistent with the priority the General Plan places on improving existing roads and utilities.

2.5.2.2 Central Oahu Sustainable Communities Plan

The planning area of the Central Oahu Sustainable Communities Plan (December 2002) includes prime agricultural lands, military installations, and major residential communities. Waipio Peninsula is within the Central Oahu Planning Area. Over the last two decades, the land use focus of Central Oahu has been primarily residential development, although the Sustainable Plan provides for continuity of industrial uses in Waipahu. The project is already shown on the Central Oahu Public Infrastructure Map, as an element of the Waipio Peninsula Recreation Complex PIM project. The proposed project is consistent with the Sustainable Plan in these respects:

- Recreational Access. Access to Waipio Peninsula Soccer Park will be maintained and improved by providing a shared-use path for pedestrians and bicycles.
- Landscaped Treatment. The visual quality of the area will be improved by the addition of a landscaping strip within the project limits.

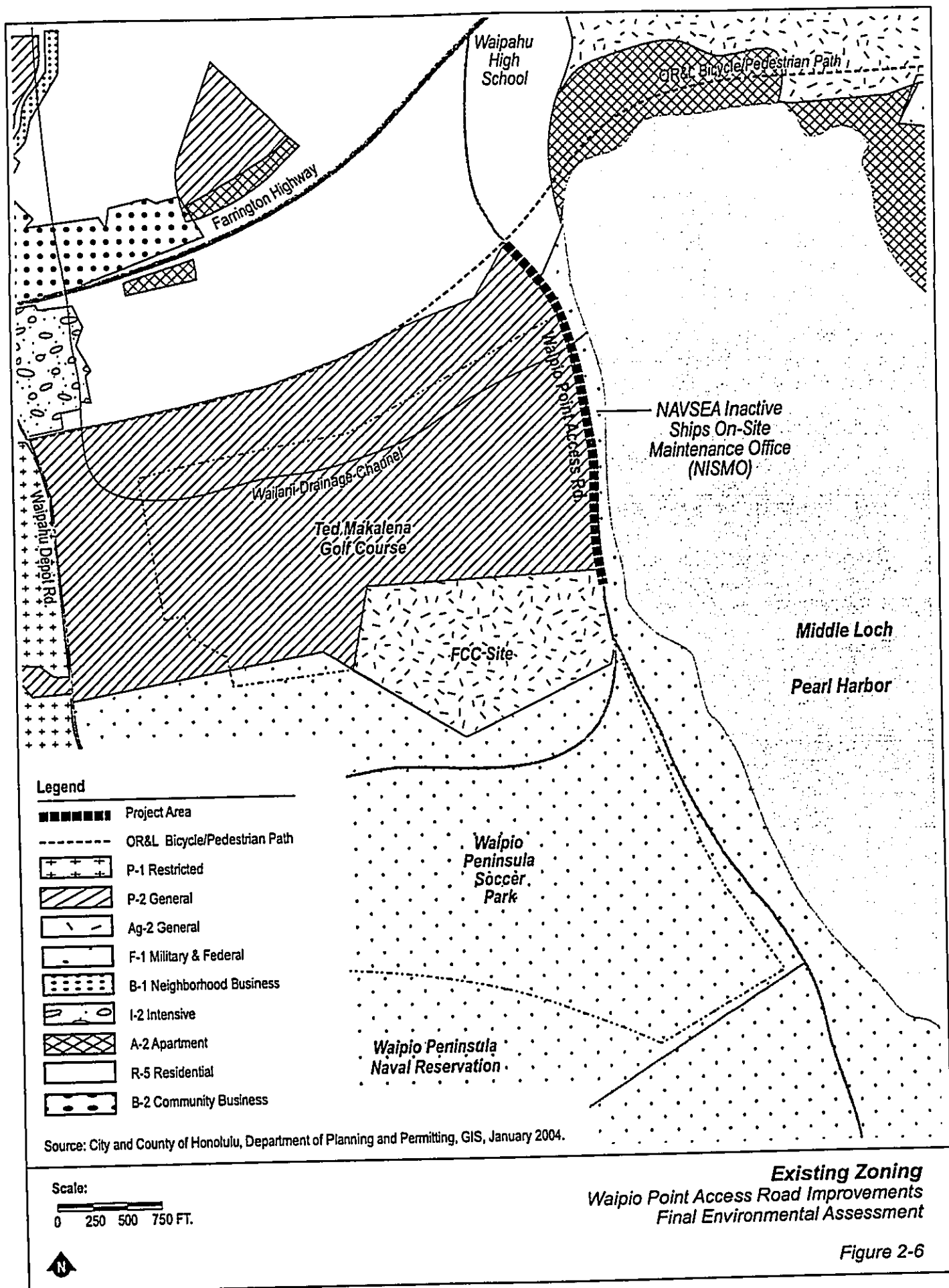
2.5.2.3 City and County of Honolulu Zoning

The City and County of Honolulu, Department of Planning and Permitting (DPP) regulates land use on Oahu in accordance with zoning, as specified in official zoning maps and the Land Use Ordinance (LUO). Zoning maps and the LUO are used to encourage orderly development in accordance with adopted land use policies, including the Oahu General Plan and development plans or sustainable community plans, and to promote and protect public health, safety, and welfare.

As shown on Figure 2-6, the lands occupied by Ted Makalena Golf Course, the NISMO facility and the FCC facility are zoned P-2 (General Preservation), F-1 (Military) and Ag-2 (General Agricultural). The soccer park is zoned F-1 despite its use for recreation open to the general public. DPP does not regulate land uses in the F-1 zone.

2.5.2.4 Special Management Area

The Hawaii CZM program designated the areas along the shoreline for "special controls on developments to avoid permanent losses of valuable resources and the foreclosure of management options, and to ensure that adequate access, by dedication or other



means, to publicly owned or used beaches, recreation areas, and natural reserves is provided." [HRS Section 205A-21] To accomplish these objectives, HRS Chapter 205A established the Special Management Area (SMA), and authorized the counties to develop and administer permitting systems to control development within the SMA. The SMA is a regulated zone extending inland from the shoreline to a landward boundary delineated by the counties. DPP administers the SMA use permit program for the City and County of Honolulu. A "development", as defined in Chapter 25 of the Revised Ordinances of Honolulu, is subject to permitting under the SMA program if the development is within the SMA.

As shown on Figure 2-7, the entire Waipio Peninsula, including the project site, is in the SMA. Since most of the elements of the proposed project is considered a "development", the project will require an SMA use permit.

A portion of the project area is within 40 feet from the shoreline, therefore a Shoreline Setback Variance will be required.

2.6 PERMITS AND APPROVALS

The following permits or approvals may be required following completion of the environmental review process under HRS Chapter 343 and NEPA. The agency responsible for the permit or approval is indicated below.

Federal

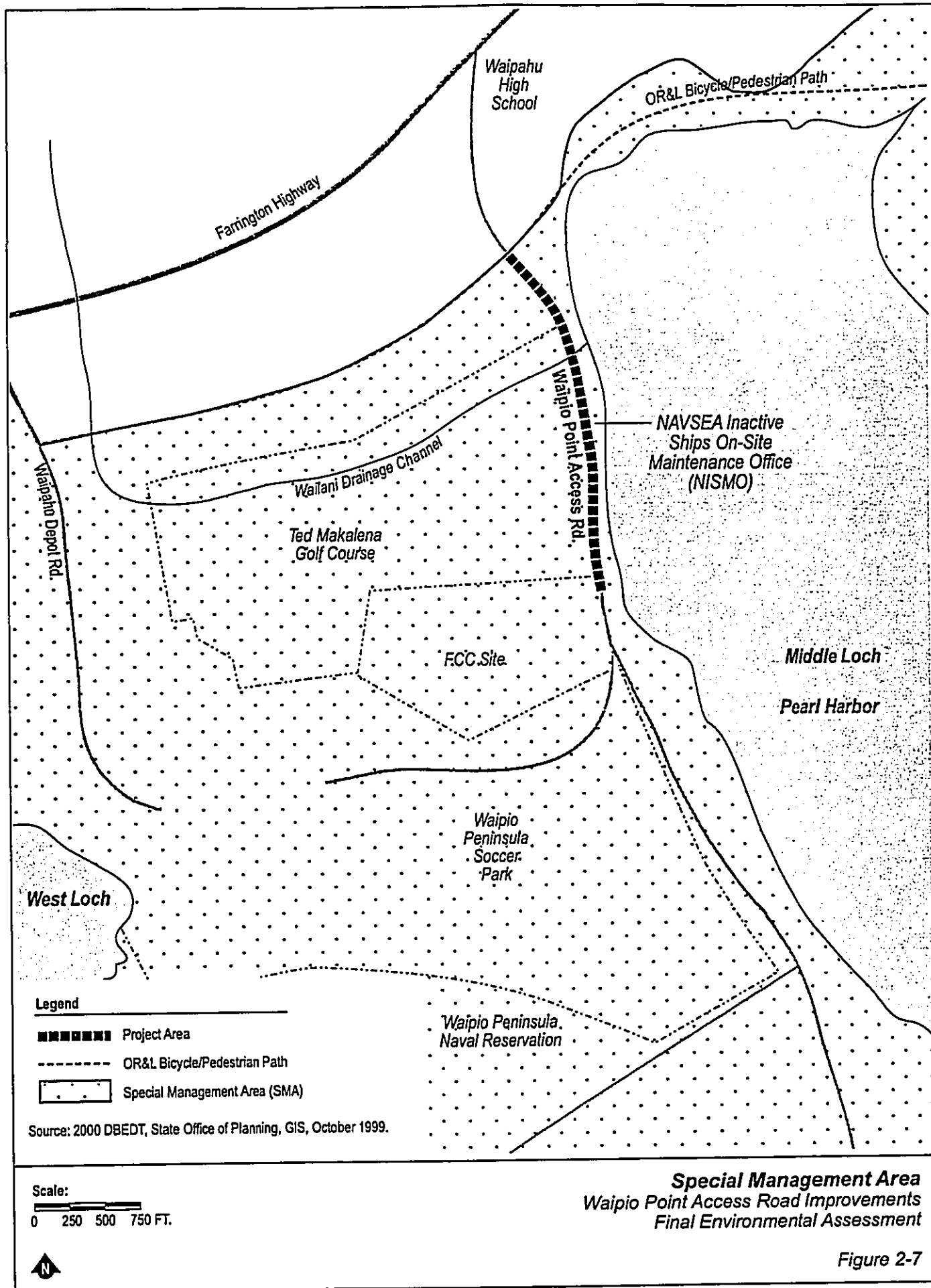
- USACE – Department of Army (DA) permit pursuant to Section 404 of the Clean Water Act and Section 10 of the Rivers and Harbors Act

State of Hawaii

- DBEDT, Office of Planning – Coastal Zone Management federal consistency determination
- SDOH, Clean Water Branch – National Pollutant Discharge Elimination System Permit for storm water discharges relating to construction activities
- SDOH, Clean Water Branch – National Pollutant Discharge Elimination System Permit for storm water discharges associated with construction dewatering
- SDOH, Clean Water Branch - National Pollution Discharge Elimination System (NPDES) Permit authorizing the discharge of hydrotesting waters.
- SDOH, Clean Water Branch – Water Quality Certification pursuant to Section 401 of the Clean Water Act
- State of Hawaii Department of Land and Natural Resources – Stream Channel Alteration Permit

City and County of Honolulu

- DPP – SMA Use Permit
- DPP – Shoreline Setback Variance
- DPP – Grading, Grubbing, Stockpiling and Excavation permit



CHAPTER 3 ■

WAIPIO POINT ACCESS ROAD Improvements

FINAL ENVIRONMENTAL ASSESSMENT

CHAPTER 3 COMMENTS AND COORDINATION

This chapter summarizes public and agency consultation and coordination activities associated with this project that have been conducted to date. Project scoping and coordination activities included meetings and correspondence with government agencies, adjacent landowners, and neighborhood board. A summary of these activities is provided in this chapter.

3.1 SCOPING AND AGENCY CONSULTATION

The following agencies, elected officials, and other organizations were contacted by letter prior to release of the Draft EA, and were asked if they were aware of any environmental or social issue associated with the proposed project. An asterisk (*) appears next to those parties that responded to the request for comments letter. Copies of the request letters and responses are provided in Appendix C. The responses helped prepare the EA.

Federal Agencies

- U.S. Department of Army Corps of Engineers*
- U.S. Department of Defense, Navy*
- U.S. Environmental Protection Agency
- U.S. Federal Communication Commission, Compliance and Information Bureau*

State of Hawaii Agencies

- Department of Business, Economic Development & Tourism, Office of Planning
- Department of Defense
- Department of Education*
- Department of Health*
- Department of Health, Office of Environmental Quality Control*
- Department of Land and Natural Resources, Chairperson
- Department of Land and Natural Resources, Commission on Water Resource Management*
- Department of Land and Natural Resources, Division Forestry and Wildlife*
- Department of Land and Natural Resources, Land Division*
- Department of Transportation, Highways Division*
- Leeward Community College (Referred to and reply received from Kimura International*, consultant for Leeward Community College Secondary Access Road Project)
- Office of Hawaiian Affairs*

City and County of Honolulu Agencies

- Board of Water Supply*
- Department of Design and Construction*
- Department of Enterprise Services*
- Department of Facility Maintenance*
- Department of Parks and Recreation*

- Department of Planning and Permitting
- Honolulu Fire Department*
- Honolulu Police Department *
- Oahu Civil Defense Agency*

Elected Officials

- Cal Kawamoto, State Senator, 18th District*
- Tulsı Gabbard Tamayo, State Representative, 42nd District
- Nestor Garcia, City Council

Businesses and Organizations and Individuals

- Hawaiian Electric Company
- Verizon Hawaii*
- Hawaiian Railway Society
- Waipahu 2000 Community Council
- Waipahu Business Association
- Waipahu Neighborhood Board No. 22

In addition to the above formal correspondence activities, DTS initiated informal discussions with representatives from the American Youth Soccer Organization (AYSO) and The Bulls, a local soccer organization, in July 2003. These organizations raised concerns regarding flooding, the lack of pedestrian facilities, and the need for landscaping on WPAR.

3.2 REGULATORY COORDINATION

Since the project must comply with certain federal and State environmental laws and regulations, coordination and consultation activities were conducted with the following agencies and organizations. See Appendix D for records of all correspondence and communications.

Section 7 of the Endangered Species Act and Chapter 195D of the Hawaii Revised Statutes

Section 7 of the Endangered Species Act (ESA) requires that federally-funded actions not jeopardize any species listed as threatened or endangered, or adversely modify designated critical habitat. Chapter 195D of the Hawaii Revised Statutes (HRS), the State counterpart law to the ESA, requires evaluation of the potential impact of State projects on threatened and endangered species.

The following consultation and coordination activities were conducted with the U.S. Fish and Wildlife Service (USFWS):

- May 17, 2004 letter from Parsons Brinckerhoff (PB) to USFWS requesting a species list.
- June 18, 2004 letter from USFWS to PB indicating that no federally listed endangered, threatened, or proposed threatened or endangered species, or proposed or designated critical habitat exist within the proposed project site.

Section 1424(e) of the Safe Drinking Water Act

Section 1424(e) of the Safe Drinking Water Act (SDWA) allows EPA to prohibit federal funding for projects in areas that overlie a sole-source aquifer, if the project threatens the aquifer. The SOBA is one of two sole source aquifers in Hawaii. For projects that trigger Section 1424(e) review, a water quality assessment must be prepared and be subject to EPA review.

The following consultation and coordination activities were conducted with EPA:

- April 12, 2004 letter from PB to EPA submitting a water quality assessment.
- April 19, 2004 letter from EPA to PB approving the project pursuant to SDWA Section 1424(e).

Section 106 of the National Historic Preservation Act and Section 6E-8 of the Hawaii Revised Statutes (HRS)

The National Historic Preservation Act (NHPA) requires that actions that are federally funded, authorized, or implemented take into account the effect of such actions on any district, site, building, structure, or object that is included in or eligible for inclusion in the National Register of Historic Places. Such resources are called historic properties. The Section 106 process involves coordination and consultation with the State Historic Preservation Officer (SHPO), and other agencies and organizations that have an interest in or is mandated to protect historic properties. In addition, the Advisory Council on Historic Preservation is afforded the opportunity to comment on actions that may potentially affect historic properties. The corresponding State law is Section 6E-8, HRS, which places similar responsibilities on State agencies to evaluate their projects. Completion of the Section 106 process normally satisfies the requirements of HRS Section 6E-8.

The following consultation and coordination activity was conducted with the State Historic Preservation Division (SHPD) and the SHPO:

- Meeting on March 17, 2004 involving SHPD and PB to discuss potential project impacts on historic properties
- January 27, 2005 letter from the SHPD to DTS indicating that no historic properties will be affected by the proposed project pursuant to HRS Section 6E(8) (letter submitted in response to Draft EA, see Appendix D)

The following consultation activities with other organizations were also conducted:

- May 12, 2004 letter to the Office of Hawaiian Affairs (OHA) requesting NHPA Section 106 consultation
- June 10, 2004 letter from OHA to PB indicating that the proposed project does not appear to impact the buried Loko Eo fishpond
- May 12, 2004 letter from PB to the Oahu Council of Hawaiian Civic Clubs requesting NHPA Section 106 consultation
- May 12, 2004 letter from PB to Hui Malama I Na Kupuna O Hawaii Nei requesting NHPA Section 106 consultation
- June 2, 2004 letter from PB to Oahu Island Burial Council requesting NHPA Section 106 consultation

3.3 DRAFT ENVIRONMENTAL ASSESSMENT

3.3.1 Availability of Draft Environmental Assessment

The project's Draft EA was announced in the December 23, 2004 edition of the Environmental Notice, which initiated the 30-day public comment period that ended on January 22, 2005. Copies of the Draft EA were mailed to federal, State and County agencies; elected officials; utility companies; and community organizations who may have an interest in the project (see Table 3-1). In addition, copies of the Draft EA were provided to the Waipahu Library, Hawaii State Library, University of Hawaii Hamilton and Sinclair Libraries, and the City's Library Department of Customer Services. All parties who were sent copies of the Draft EA were asked to provide comments.

**Table 3-1
Summary of Draft Environmental Assessment Coordination and Comments**

Agency or Organization	Provided Comments During Draft EA Comment Period
Federal Agencies	
Naval Facilities Engineering Command	X
Navsea Inactive Ships On-Site Maintenance Office	
Navy Fleet and Industrial Supply Center	
Navy Region Hawaii	X
Pacific Division Naval Facilities Engineering Command	
U.S. Army Corps of Engineers, Regulatory Branch	
U.S. Department of the Interior, Fish and Wildlife Service	
U.S. Department of Transportation, Federal Highway Administration, Hawaii Division	
U.S. Environmental Protection Agency	
U.S. Federal Communications Commission, Equipment Development Group	X
State Agencies	
Department of Business, Economic Development & Tourism, Office of Planning	
Department of Education	X
Department of Hawaiian Home Lands	
Department of Health (DOH)	
DOH, Clean Air Branch	X
DOH, Office of Environmental Quality Control	X
DOH, Solid and Hazardous Waste Branch	X
Department of Land and Natural Resources (DLNR)	
DLNR, Commission on Water Resource Management	
DLNR, Division of Forestry and Wildlife	
DLNR, Land Division	X
DLNR, State Historic Preservation Division	X
Department of Transportation	X
Leeward Community College	
Office of Hawaiian Affairs	

Agency or Organization	Provided Comments During Draft EA Comment Period
University of Hawaii, Environmental Center	
County Agencies	
Board of Water Supply	X
Department of Design and Construction	X
Department of Enterprise Services	
Department of Environmental Services	
Department of Facility Maintenance	X
Department of Parks and Recreation	X
Department of Planning and Permitting	X
Honolulu Fire Department	X
Honolulu Police Department	X
Oahu Civil Defense Agency	
Elected Officials	
Clarence K. Nishihara, State Senator, 18 th District	
Rida T.R. Cabanilla, State Representative, 42 nd District	
Nestor Garcia, City Council	
Utility Companies	
Hawaiian Electric Company	
Verizon Hawaii	
Community Organizations	
Earth Justice	
Hawaii Audubon Society	
Hawaii Bicycling League	
Hawaiian Railway Society	
Historic Hawaii Foundation	
Hui Malama I Na Kupuna O Hawaii Nei	
Life of the Land	
Native Hawaiian Legal Corporation	
Nature Conservancy	
Oahu Council of Hawaiian Civic Clubs	
Oahu Metropolitan Planning Organization	
Sierra Club Hawaii Chapter	
The Outdoor Circle	
Waipahu Business Association	
Waipahu Neighborhood Board No. 22	

3.3.2 Comments

This section provides a record of all comment received during the Draft EA comment period and at the Waipahu Neighborhood Board Meeting. Seventeen letters providing comment were received during the Draft EA public comment period. Table 3-1 lists the agencies and organizations who mailed or provided written comments. The comments and their associated responses are provided in Appendix E and are arranged in the following order, which is the same order in which the commentators are listed on Table 3-1:

- Federal agencies
- State agencies
- County agencies

- Elected Officials
- Utility Companies
- Community Organizations

A project presentation was held before the Waipahu Neighborhood Board on March 17, 2005. The only comment provided by the board was an inquiry on whether formal parking near Bridge No. 2 will be provided for fishermen.

CHAPTER 4 ■

WAIPIO POINT ACCESS ROAD Improvements

FINAL ENVIRONMENTAL ASSESSMENT

CHAPTER 4 FINDING OF NO SIGNIFICANT IMPACT

In accordance with Chapter 343 and Hawaii Administrative Rules (HAR), Sections 11-200-9 and 11-200-11.2, DTS has issued a FONSI for the proposed project. The Final EA/FONSI will be announced in the State's Environmental Notice. This FONSI determination is based on an evaluation of project impacts in relation to the "Significance Criteria" specified in HAR 11-200-12 (b).

Below is the significance evaluation to support the FONSI determination. The Significance Criteria appear below in italics, followed by a brief discussion of the project in relation to the specific criterion. The nature of the project's potential impacts, and committed mitigation measures to minimize adverse impacts, is discussed in detail in Chapter 2.

1. *Involves an irrevocable commitment to loss or destruction of any natural or cultural resource* – The proposed project will not cause the loss or destruction of any natural, historic or cultural resource. Most of the proposed project will be constructed within the existing WPAR right-of-way. Although relatively small portions of Ted Makalena Golf Course will be re-designated as additional right-of-way, these areas are not playable parts of the golf course, and DES did not object to the proposed acquisitions. A portion of the project will overlie a large buried fishpond, Loko Eo, which is eligible for the National Register. However, intact fishpond sediments are buried far below the limits of the project's excavation activities, and therefore, will not be affected by the project.
2. *Curtails the beneficial uses of the environment* – The proposed project will not affect use of Waipio Peninsula Soccer Park, Ted Makalena Golf Course, and the NISMO facility.
3. *Conflicts with the State's long term environmental policies or goals and guidelines expressed in Chapter 344, HRS, and any revisions thereof and amendments thereto, court decisions, or executive orders* – The proposed improvements are consistent with the policies and guidelines specified in HRS Chapter 343, as demonstrated by the discussion in this chapter and Chapter 2.
4. *Substantially affects the economic or social welfare of the community or State* – The proposed roadway improvements are intended to support the NISMO facility as defined in the GOE. The addition of a shared-use path will provide the public with safer bicycle and pedestrian access to the Waipio Peninsula Soccer Park, and roadway landscaping will improve the aesthetics of the roadway.
5. *Substantially affects public health* – The proposed project will improve public health by addressing existing draining and flooding problems on WPAR; upgrading the railings and approaches at Bridge No. 2 to current standards; demarcating the OR&L Bicycle/Pedestrian Path crossing of WPAR; and providing a continuous shared-use path within the project limits.

6. *Involves substantial secondary impacts* – The purpose and proposed action of the project does not lend itself to substantial secondary impacts. For example, the proposed project will not increase the vehicular capacity of WPAR.
7. *Involves substantial degradation of environmental quality* – The proposed roadway improvements will not result in substantial degradation of environmental quality. During construction, BMPs will be used to prevent excess sedimentation from affecting Wailani Drainage Channel and Pearl Harbor's Middle Loch.
8. *Substantially affects a rare, threatened, or endangered species, or its habitat* – No rare, threatened, or endangered species, or habitat exist within the project area.
9. *Is individually limited but cumulatively has considerable effect upon the environment or involves a commitment for larger actions* – The proposed project will address the GOE conditions, and therefore is a complete project.
10. *Detrimentially affects air or water quality or ambient noise levels* – The proposed project will have no effect on the number of vehicles using WPAR, which are the major source of air quality, water quality, and noise impacts. During construction, BMPs will be used to prevent excess sedimentation from affecting Wailani Drainage Channel and Pearl Harbor's Middle Loch.
11. *Affects or is likely to suffer damage by being located in an environmentally sensitive area such as a floodplain, tsunami zone, beach, erosion-prone area, geologically hazardous land, estuary, fresh water, or coastal waters* – The project site, WPAR, is not in an environmentally sensitive area. Although Middle Loch could be considered an environmentally sensitive area, the project will not affect this water body any more than under the No Build condition. Also, noted above, BMPs will be used to prevent excess sedimentation from affecting the water body. Although the project area is prone to flooding, the proposed project provides drainage improvements to address this problem.
12. *Substantially affects scenic vistas and viewplanes identified in county or state plans or studies* – The proposed project will not block or significantly affect any existing scenic views because the improvements will be at ground level along the existing roadway alignment. There will be no structures that would rise substantially above grade.
13. *Requires substantial energy consumption* – The proposed project will not result in substantial energy consumption because the same number of vehicles will use WPAR regardless of whether the project is implemented or not.

CHAPTER 5 ■

WAIPIO POINT ACCESS ROAD Improvements

FINAL ENVIRONMENTAL ASSESSMENT

CHAPTER 5 REFERENCES

Belt Collins Hawaii, Final Environmental Impact Statement for Waipio Peninsula Soccer Park Conceptual Master Plan, October 1998.

City and County of Honolulu, Central Oahu Sustainable Communities Plan, December 2002.

City and County of Honolulu, General Plan, 1992.

Earth Tech, Inc., Site Summary Report Waipio Peninsula Geographic Study Area, Pearl Harbor Naval Complex, Oahu, Hawaii, August 2000.

Wendy Goodman, B.A., Wendy and Paul L. Cleghorn, Ph.D., Pacific Legacy, Inc., Archaeological Inventory Survey for the Proposed Waipio Sports Complex Waikele and Waipio Ahupua'a, Waipio Peninsula, Ewa, Oahu, June 1998.

Hawaii Pacific Engineers, Engineering Study on Drainage Improvements for Naval Inactive Ships Maintenance Facility, November 1994.

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Kepler, Angela Kay, Trees of Hawaii, 1990.

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State of Hawaii Department of Transportation, Bike Plan Hawaii, A State of Hawaii Master Plan, September 2003.

State of Hawaii, Hawaii Administrative Rules, as amended.

State of Hawaii, Hawaii Revised Statutes, as amended.

United States Department of Agriculture Soil Conservation Service, Soil Survey of Islands of Kauai, Oahu, Maui, Molokai, and Lanai, State of Hawaii, August 1972.

U.S. Department of the Navy, Proposed Amendment to Lease of U.S. Navy Property at Waipio Peninsula to the City and County of Honolulu for Use as a Soccer Park, Supplemental Environmental Assessment, May 2000.

APPENDIX A ■

GRANT OF EASEMENT

WAIPIO POINT ACCESS ROAD Improvements

FINAL ENVIRONMENTAL ASSESSMENT

OFFICE OF THE
ASSISTANT ATTORNEY GENERAL, LAND COURT
HONOLULU, HAWAII

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11-19512

LAND COURT SYSTEM
AFTER RECORDATION, RETURN BY MAIL () PICKUP ()
Commander, Pacific Division, Naval Facilities
Engineering Command, Pearl Harbor, HI 98860

REGULAR SYSTEM

TITLE OF DOCUMENT:

GRANT OF EASEMENT

(Navy Identification No. N6274200RP00007)

PARTIES TO DOCUMENT:

GRANTOR: UNITED STATES OF AMERICA, represented by the
Commander, Pacific Division, Naval Facilities
Engineering Command, Pearl Harbor, HI 98860-3134

GRANTEE: CITY AND COUNTY OF HONOLULU
Honolulu Hale, 530 South King Street
Honolulu, HI 96813

PROPERTY DESCRIPTION:

Waipio Point
Access Road

TMK (1) 9-3-02:1 (portion)

LIBER/PAGE:

Liber
Page

DOCUMENT NO.:

GRANT OF EASEMENT

THIS INDENTURE, made this 24th day of December, 1998, between the United States of America, herein called the "Government", acting by and through the Department of the Navy represented by the Commander, Pacific Division, Naval Facilities Engineering Command, and the City and County of Honolulu, herein called the "Grantee".

WHEREAS, the Council of the City and County of Honolulu, State of Hawaii, adopted Resolution No. 88-279 on November 10, 1998, which authorized Grantee, through its Department of Design and Construction, to enter into a lease (the "Lease") with the Government, of approximately 300 acres of federally-owned land located on Waipio Peninsula, situate at Waikole and Waipio, Ewa, Oahu, Hawaii, for the development and operation of a public soccer park and related recreational facilities (the "Soccer Park"); and

WHEREAS, the Government owns that certain real property known and identified as a portion of Waipio Point Access Road, identified as Tax Map Key (1) 9-3-02:1 (portion) required for public access to the Soccer Park; and

WHEREAS, under Section 127 of United States Public Law 105-45, approved on September 30, 1997, the Secretary of the Navy was authorized to grant, without monetary consideration, an easement on, over, under and across said portion of Waipio Point Access Road under the control of the Secretary, pursuant to the terms and conditions described therein; and

WHEREAS, in connection with the Lease, the Grantee desires an easement on, over, under and across said portion of Waipio Point Access Road for operation, use, maintenance, repair, improvement and replacement of a Public Road; and

WHEREAS, the Secretary of the Navy has found that the grant of such easement on the terms and conditions hereinafter stated is not incompatible with the public interest:

DUPLICATE ORIGINAL

NOW, THEREFORE, this indenture witnesseth that in consideration of the covenants and conditions contained herein, the Government, pursuant to the authority of Section 127 of United States Public Law 105-45, hereby grants unto the City and County of Honolulu an easement for a period of 50 years from the date hereof for the operation, use, maintenance, repair, improvement and replacement of a Public Road, such easement to extend through, over, across and along and be confined to the parcels shown and described in Exhibit "A", attached hereto and made a part hereof, herein called the "Premises".

THIS EASEMENT is granted subject to the following terms and conditions:

1. All maintenance, operation, repair, improvement and use of the Premises shall be accomplished without cost or expense to the Department of the Navy and its assigns in accordance with plans previously approved by the Commander, Pacific Division, Naval Facilities Engineering Command, hereinafter referred to as the Commander.
2. The Grantee shall, at its own cost and to the satisfaction of the Government, make any and all improvements to the Premises which the Grantee determines are necessary for safe operation of a Public Road, provided that available parking for naval activities adjacent to the Premises shall not be reduced. It is agreed and understood that any improvements in or upon the Premises made by the Grantee must be in compliance with all applicable environmental laws and regulations including, but without limitation, the National Environmental Policy Act of 1969 (NEPA), as amended (42 U.S.C. 4321, et seq.), and its implementing regulations and be approved in writing by the Government. Any plans submitted by the Grantee to the Government for written approval for such improvements shall be accompanied by complete environmental documentation provided by the Grantee at no cost to the Government. Such environmental documentation is subject to the acceptance and approval of the Government.

3. The Grantee shall, at its own cost and to the satisfaction of the Government, make such drainage improvements as mutually agreed upon by the Grantee and the Government including, but without limitation, minor swale improvements both on the Premises and within the Grantee-owned portions of Waipio Point Access Road and new ditches on the Premises as may be reasonably necessary to prevent flooding of the Premises and naval activities adjacent thereto.

4. All landscaping plans shall be subject to the prior written approval of the Commander.

5. The Grantee shall always keep the Premises reasonably clean and in reasonably good condition and shall promptly make all repairs or improvements to the Premises that it deems desirable or necessary for public safety. It is agreed and understood that the Grantee shall make such improvements as may be necessary in order that traffic conditions at intersections along Waipio Point Access Road do not fall below Level of Service D, as set forth in the *Highway Capacity Manual*, Special Report 209, Transportation Research Board, Third Edition (1984).

6. The Grantee's rights hereunder shall be subject to such reasonable rules and regulations as may be promulgated by the Government to insure that the exercise of such rights shall not interfere with the Government activities including, but not limited to, operations of the Naval Inactive Ship Maintenance Facility.

7. Law enforcement within the Premises shall be the responsibility of the Grantee.

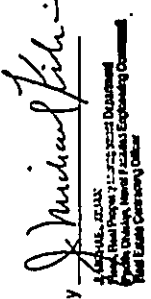
8. The Government reserves to itself and its assigns rights-of-way for all purposes across, over and/or under the right-of-way herein granted. The rights herein reserved shall specifically include (but without limitation) the right of the Government and its assigns to operate vehicles over and across the Premises. Neither the Government nor its assigns shall be responsible for any items of maintenance arising from operation of vehicles of any type along, over and across the Premises.

IN WITNESS WHEREOF, the parties hereto have caused this Instrument to be executed the day and the year first above written.

CITY AND COUNTY OF HONOLULU

UNITED STATES OF AMERICA

By 

By 
U.S. MARSHAL SERVICE
Hon. Paul Pangel 317-223-2022 District
Public District, Head of Public Engineering Council
Hon. Brian Costello, Chief

APPROVED AS TO CONTENTS:

By 
Department of Design and Construction

APPROVED AS TO FORM AND LEGALITY


Deputy Corporation Counsel

9. The Grantee shall be liable and responsible for any damage to any underground pipelines within the premises caused by the Grantee's negligence or otherwise resulting from the exercise by the Grantee of the rights herein granted.

10. Upon termination of the easement granted herein, the Grantee shall either convey, without reimbursement, to the Government, all right, title, and interest of the Grantee in and any and all improvements installed or constructed hereunder or restore the Premises, to the extent practicable, to the same or as reasonably good condition as that which existed prior to the exercise by the Grantee of its rights hereunder, such restoration to be effected to the satisfaction of the Commander.

11. The Grantee assumes and agrees to indemnify and save harmless the Government, its departments, instrumentalities, agencies, officers, agents and employees from and against any and all claims, demands and liability of every sort and whatsoever characterized, whether arising under the Federal Torts Claims Act (28 U.S.C. §§ 1346(b), 2671-2680) or otherwise, including (but without limitation) claims, assessments, damages, actions or suits of any kind or nature for any actual or alleged loss or damage to any resource or property, or noncompliance with laws or regulations for protection of the environment, or injury to or death of any person, which may arise from or be incident to Grantee's use or occupation of the Premises or Grantee's construction, maintenance or use of Waipio Point Access Road.

12. All or any part of the rights-of-way herein granted may be terminated upon failure on the part of the Grantee to reasonably comply with any and all of the terms and conditions contained herein or upon nonuse of the rights granted herein for a period of two (2) consecutive years.

STATE OF HAWAII

CITY AND COUNTY OF HONOLULU

} ss.
}

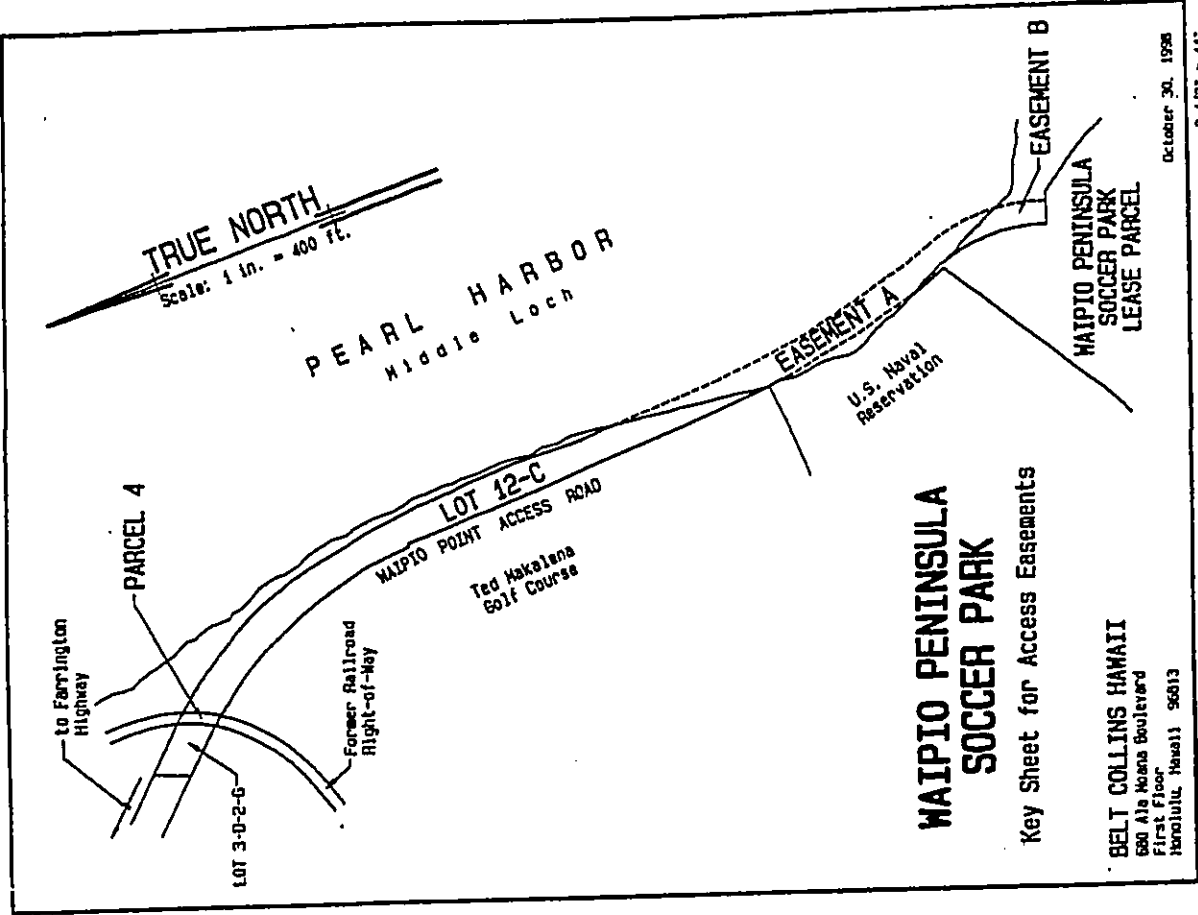
On this 2nd day of December, 1999, before me appeared Tracy Harris, to me personally known, who, being by me duly sworn, did say that he is the

Mayor of the CITY AND COUNTY OF HONOLULU, a municipal corporation and that the seal affixed to said instrument is the corporate seal of said municipal corporation, and that the instrument was signed and sealed in behalf of said municipal corporation by authority of its City Council, and said Mayor acknowledged the instrument to be a free act and deed of said municipal corporation.

L.S.

David A. Orr
Notary Public, First Judicial
Circuit, State of Hawaii

My commission expires 9/20/2001



DESCRIPTION

Parcel 4

Being a portion of Waipio Point Access Road

DESCRIPTION

Lot 3-D-2-G (Portion of Waipio Point Access Road)

Being Lot 3-D-2-G, area 23,915 square feet or 0.549 acre, as shown on Map 146, filed in the office of the Assistant Registrar of the Land Court of the State of Hawaii with Land Court Application 1000 of John H. Estate, Limited and being a portion of the land described in Transfer Certificate of Title No. 44,477 issued to United States of America.

Situated at Waipio, Ewa, Oahu, Hawaii

Situated at Waipio, Ewa, Oahu, Hawaii

Beginning at the North corner of this parcel of land, on the northerly side of Waipio Point Access Road, the coordinates of said point of beginning referred to Government Survey Triangulation Station "EWA CHURCH" being 2,757.78 feet South and 4,245.71 feet West, thence running by azimuths measured clockwise from True South:

1. Along the remainder of Exclusion 23 of Land Court Application 1000 (Former Railroad Right-of-Way), on a curve to the right with a radius of 1,492.40 feet, the chord azimuth and distance being:
316° 57' 27" 46.66 feet;
2. Thence along Lot 12-C as shown on Map 147 of Land Court Application 1000, along the remainder of Waipio Point Access Road, on a curve to the right with a radius of 528.55 feet, the chord azimuth and distance being:
28° 24' 57" 129.78 feet;
3. Thence along the remainder of Exclusion 23 of Land Court Application 1000 (Former Railroad Right-of-Way), on a curve to the left with a radius of 1,372.40 feet, the chord azimuth and distance being:
138° 56' 37" 41.84 feet;

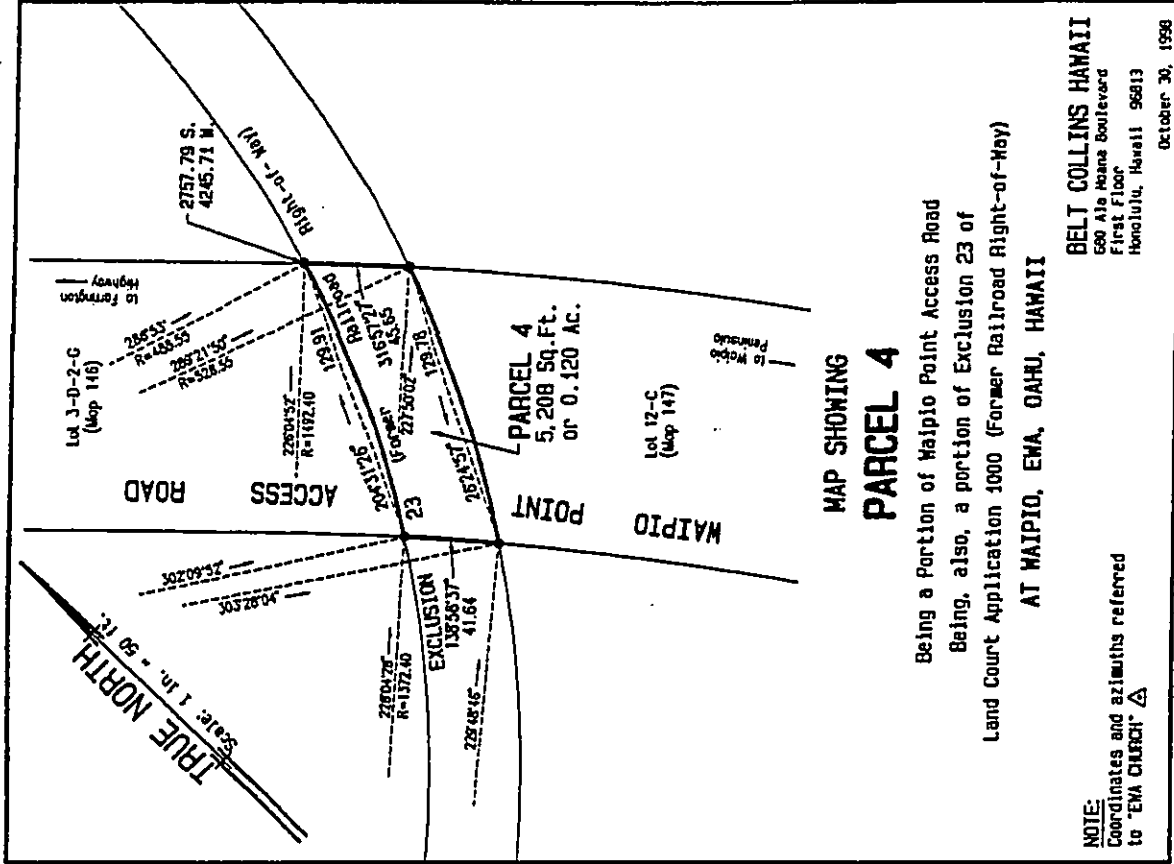
4. Thence along Lot 3 D 2 G as shown on Map 146 of Land Court Application 1000, along the remainder of Waipio Point Access Road, on a curve to the left with a radius of 488.65 feet, the chord azimuth and distance being:
 204° 31' 26" 129.81 feet to the point of beginning and containing an area of 5,208 Square Feet or 0.120 Acre.

880 Ala Moana Blvd.
 First Floor
 Honolulu, Hawaii 96813
 October 30, 1998



BELT COLLINS HAWAII LTD.

Robert W. Cunningham
 Licensed Professional Land Surveyor
 Certificate Number 4188



DESCRIPTION

Lot 12-C (Portion of Waipio Point Access Road)

Being Lot 12-C, area 5.359 acres, as shown on Map 147, filed in the office of the Assistant Registrar of the Land Court of the State of Hawaii with Land Court Application 1000 of John H. Estate, Limited and being a portion of the land described in Transfer Certificate of Title No. 44,178 issued to United States of America

Situated at Waipio, Ewa, Oahu, Hawaii

DESCRIPTION

Easement A
For Road and Utility Purposes

Being a portion of filled land of Middle
Loch of Pearl Harbor

Situated on the easterly boundary of Land Court Application 1000
at Waipio, Ewa, Oahu, Hawaii

Beginning at the North corner of this parcel of land, being, also, the South corner of Lot 12-D as shown on Map 147 of Land Court Application 1000, the coordinates of said point of beginning referred to Government Survey Triangulation Station "EWA CHURCH" being 4,550.79 feet South and 3,804.30 feet West, thence running by azimuths measured clockwise from True South:

1. 357° 32' 378.99 feet along the remainder of filled land of Middle Loch of Pearl Harbor;
2. Thence along the remainder of filled land of Middle Loch of Pearl Harbor, on a curve to the left with a radius of 2,814.79 feet, the chord azimuth and distance being:
348° 34' 50" 878.08 feet;
3. Thence along the remainder of filled land of Middle Loch of Pearl Harbor, on a curve to the right with a radius of 924.56 feet, the chord azimuth and distance being:
352° 07' 48" 270.58 feet;
4. Thence along Lot 12-A-2 as shown on Map 148 of Land Court Application 1000, the direct azimuth and distance between points being:
154° 13' 404.40 feet;
5. Thence along the remainder of filled land of Middle Loch of Pearl Harbor, on a curve to the right with a radius of 2,814.79 feet, the chord azimuth and distance being:
168° 18' 30" 606.14 feet;

Thence along the southerly end of Lot 12-C as shown on Map 147 of Land Court Application 1000 (portion of Waipio Point Access Road) for the next two (2) courses, the direct azimuths and distances between points being:

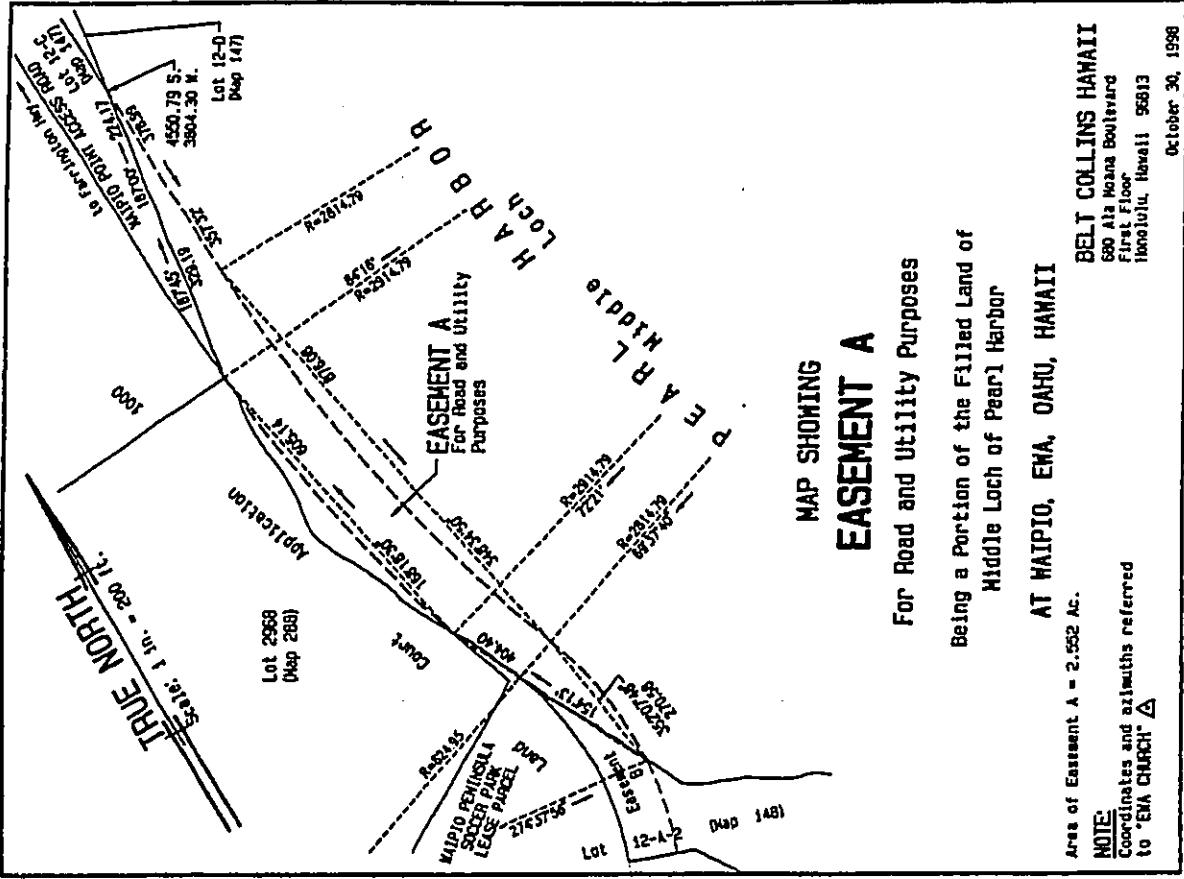
6. 187° 45' 329.19 feet;
7. 187° 00' 224.17 feet to the point of beginning and containing an area of 2.552 acres.



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Robert W. Cunningham
 Licensed Professional Land Surveyor
 Certificate Number 4188

680 Ala Moana Blvd.
 First Floor
 Honolulu, Hawaii 96813
 October 30, 1998



DESCRIPTION

Easement B
For Road and Utility Purposes

Being a portion of Lot 12-A-2 as shown on Map 148 of Land Court Application 1000

Situated at Waipio, Ewa, Oahu, Hawaii

Beginning at the north corner of this parcel of land, the coordinates of said point of beginning referred to Government Survey Triangulation Station "EWA CHURCH" being 5,823.09 feet South and 3,690.09 feet West, thence running by azimuths measured clockwise from True South:

1. Along filled land of Middle Loch of Pearl Harbor, the direct azimuth and distance between points being: 334° 13' 258.89 feet;
2. Thence along the remainder of Lot 12-A-2 as shown on Map 148 of Land Court Application 1000, on a curve to the right with a radius of 624.85 feet, the chord azimuth and distance being: 12° 38' 34" 174.18 feet;
3. 108° 55' 83.00 feet along the remainder of Lot 12-A-2 as shown on Map 148 of Land Court Application 1000;
4. Thence along the remainder of Lot 12-A-2 as shown on Map 148 of Land Court Application 1000, on a curve to the left with a radius of 542.00 feet, the chord azimuth and distance being: 190° 35' 47.6" 376.19 feet to the point of beginning and containing an area of 21,043 square feet.

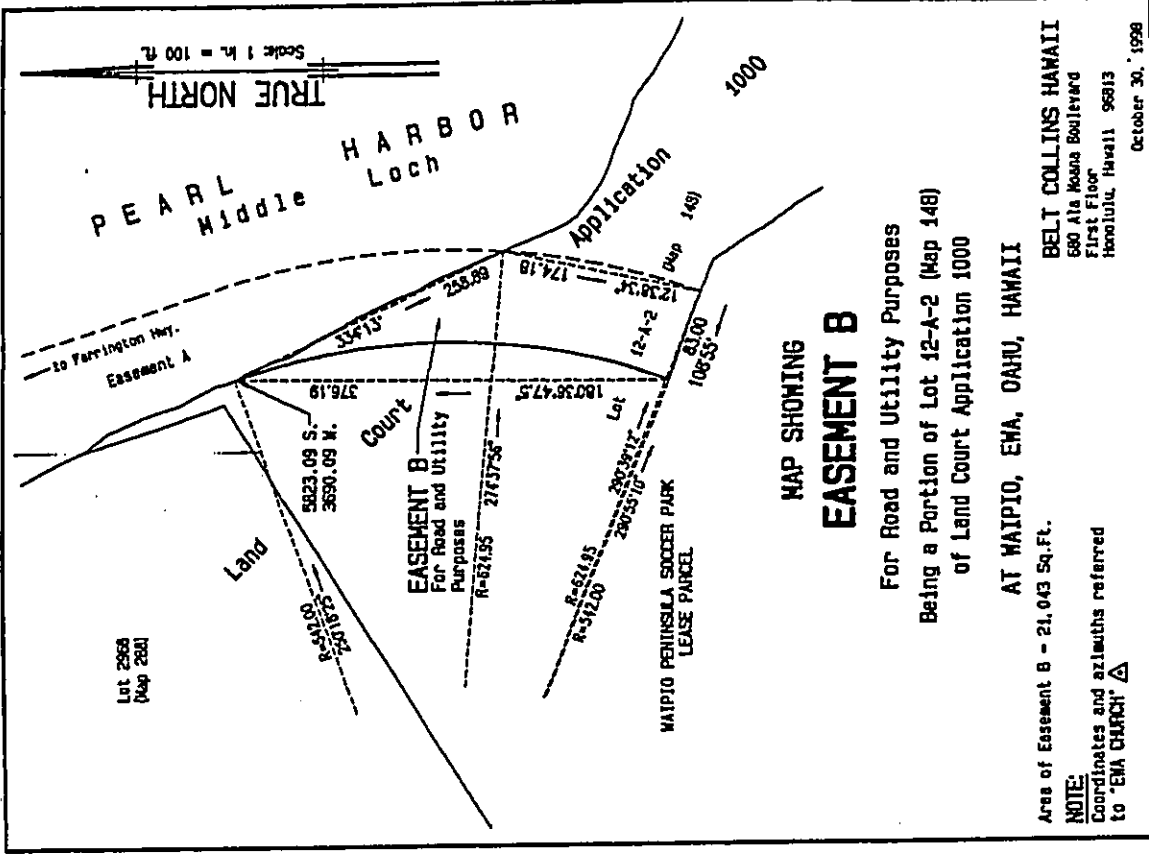
680 Ala Moana Blvd.
First Floor
Honolulu, Hawaii 96813

October 30, 1998



BELT COLLINS HAWAII LTD.

Robert W. Cunningham
Licensed Professional Land Surveyor
Certificate Number 4188



APPENDIX B ■

ROADWAY IMPROVEMENT ALTERNATIVE FIGURES

**WAIPIO POINT ACCESS ROAD
Improvements**

FINAL ENVIRONMENTAL ASSESSMENT

MIDDLE LOCH

NAVSEA Inactive Ships On-Site Maintenance Facility (NISMO)

18th Fairway

18th Green

17th Green

7th Green

8th Tee

Ted Makalena Golf Course
(City & County of Honolulu)

For Details,
See Figure B-2
For Proposed
Roadway Section

For Details,
See Figure B-2
For Proposed
Roadway Section

To Waipahu


OR & L
Bicycle/
Pedestrian
Path

NORTH PROJECT LIMITS

Waipio Point Access Road
Bridge No. 2
over Waialani Drainage Channel

Waialani Drainage Channel

Legend

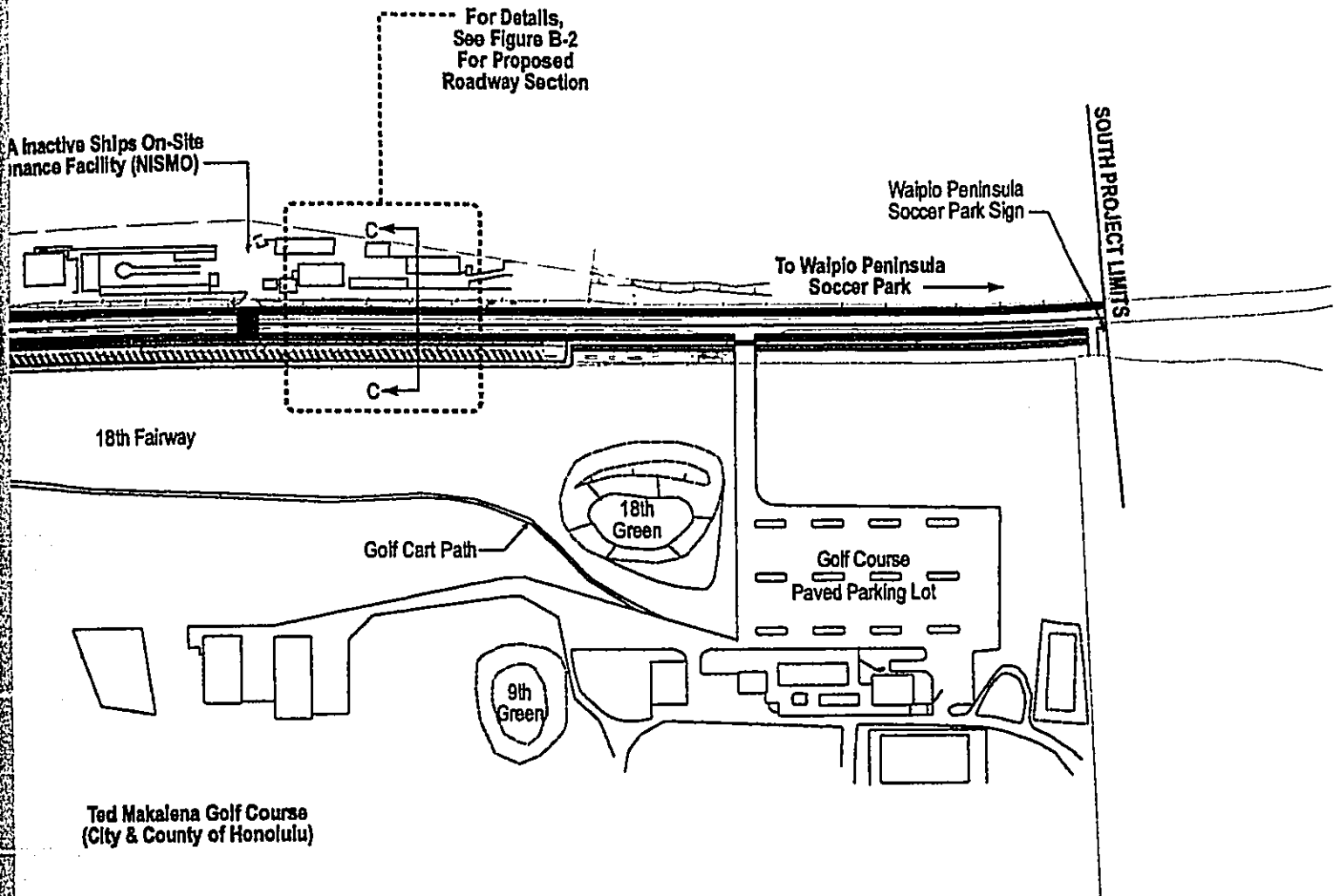
-  Bicycle Lane
-  Roadway Lane
-  Landscaped Area
-  Paved Parking Area
-  Sidewalk
-  Concrete Lined Drainage Channel

Scale:

0 40 80 160 FT.

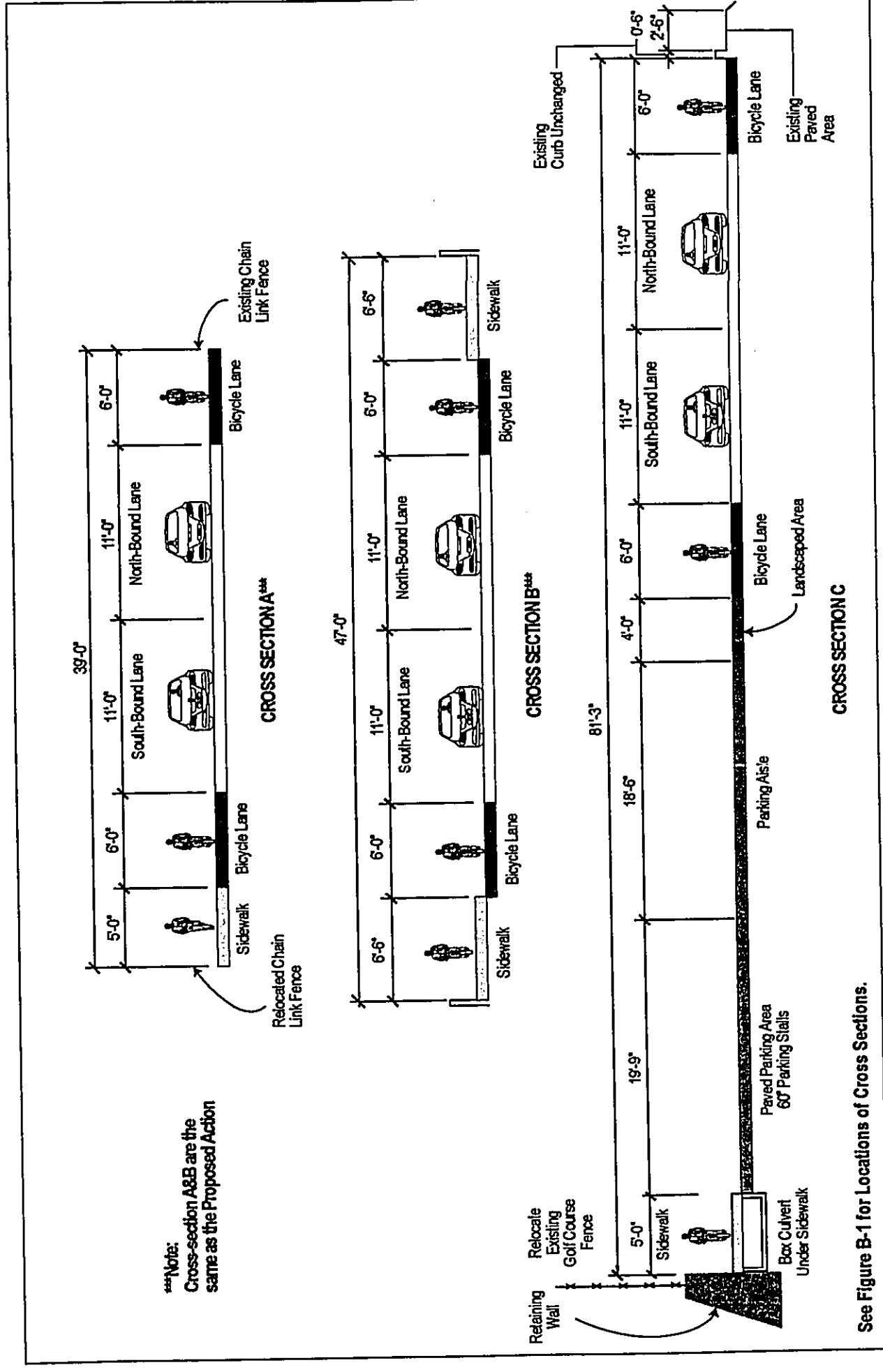


LE LOCH



Alternative 1 - Paved Diagonal Parking
Waipio Point Access Road Improvements
Final Environmental Assessment

Figure B-1



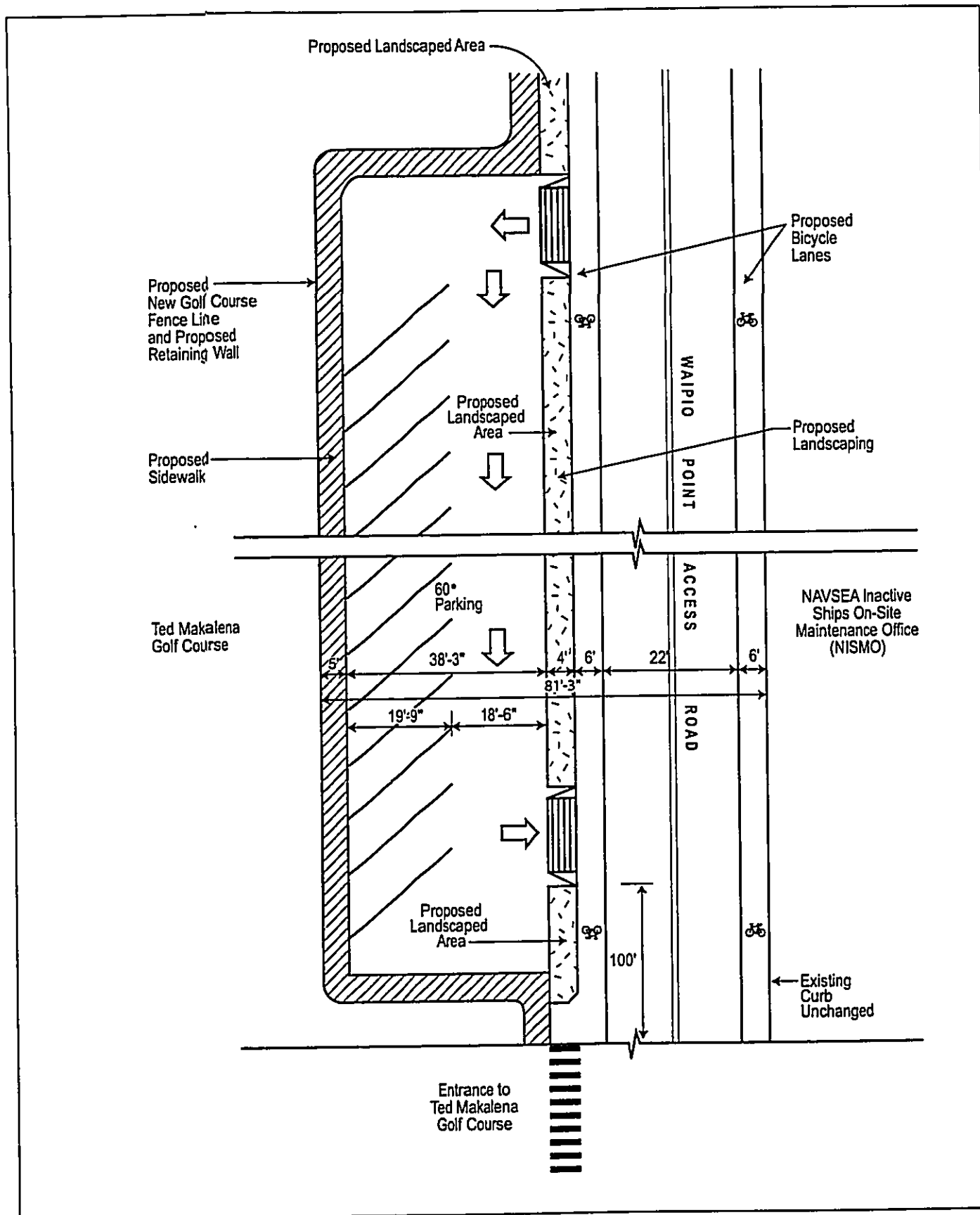
***Note:
 Cross-section A&B are the
 same as the Proposed Action

See Figure B-1 for Locations of Cross Sections.

Not To Scale

Alternative 1 - Paved Diagonal Parking - Sections
Waipio Point Access Road Improvements
Final Environmental Assessment

Figure B-2



Not To Scale

Alternative 1 - Paved Diagonal Parking - Plan
 Waipio Point Access Road Improvements
 Final Environmental Assessment

Figure B-3

B

MIDDLE LOCH

NAVSEA Inactive Ships On-Site Maintenance Facility (NISMO)

18th Fairway

Ted Makalena Golf Course (City & County of Honolulu)

For Details, See Figure B-5 For Proposed Roadway Section

For Details, See Figure B-5 For Proposed Roadway Section

To Waipahu

18th Green

17th Green

7th Green

8th Tee






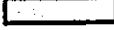
Waiani Drainage Channel

Waipio Point Access Road Bridge No. 2 over Waiani Drainage Channel

OR & L Bicycle/Pedestrian Path

NORTH PROJECT LIMITS

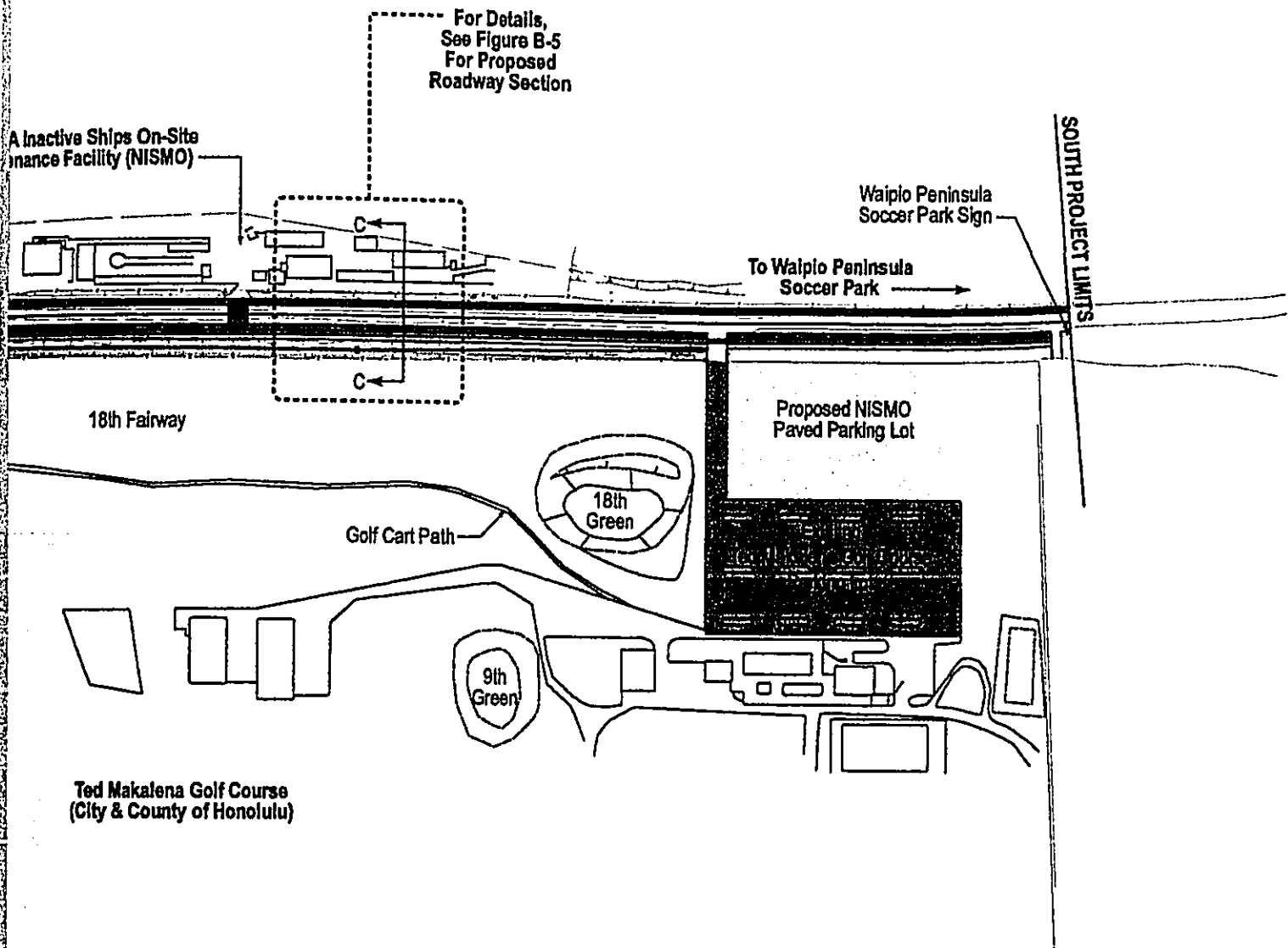
Legend

-  Bicycle Lane
-  Roadway Lane
-  Landscaped Area
-  Sidewalk
-  Concrete Lined Drainage Channel
-  Paved Parking Area

Scale: 0 40 80 160 FT.

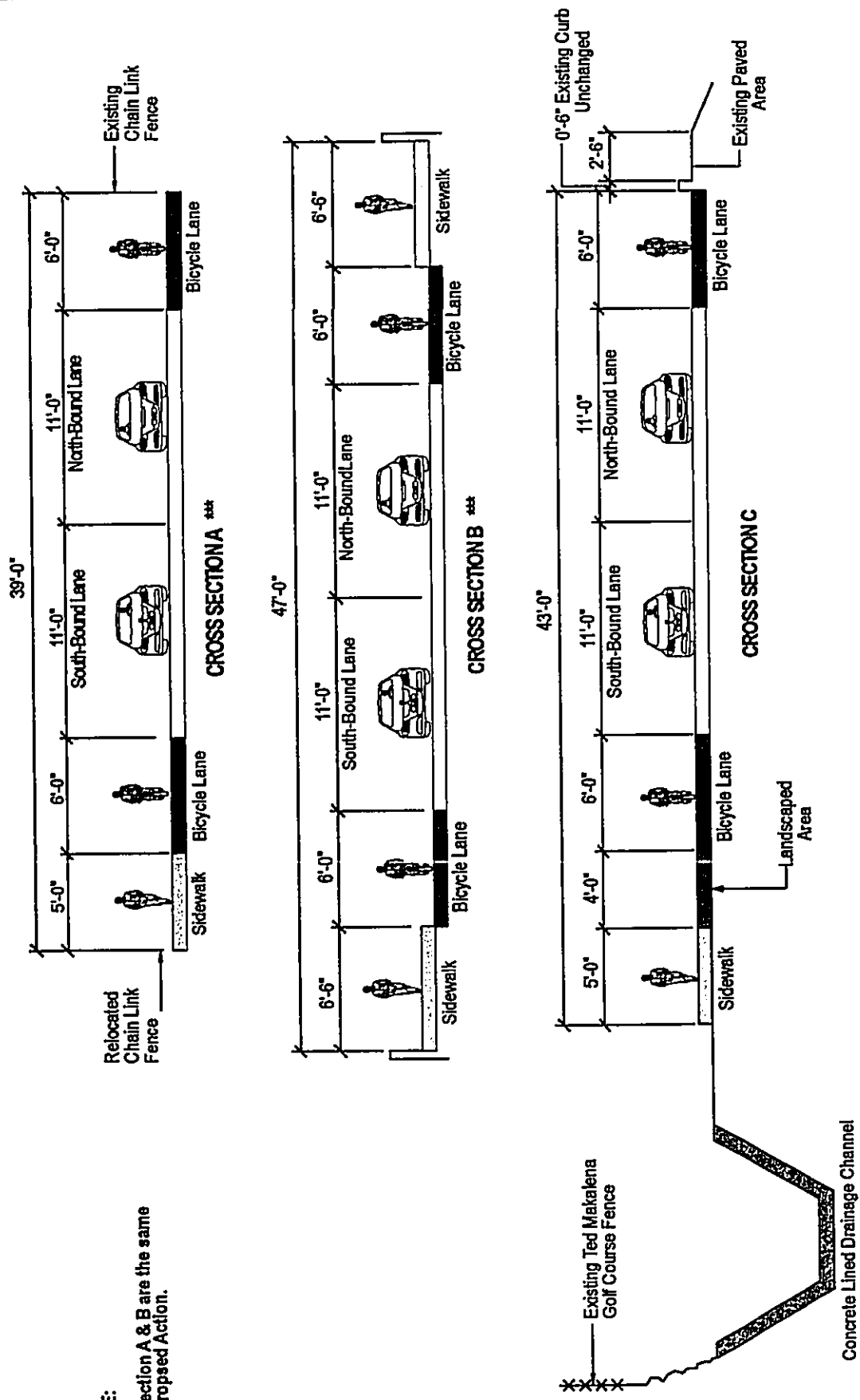


LE LOCH



Alternative 2 - Relocation of Parking Area and Improved Drainage Ditch
Waipio Point Access Road Improvements
Final Environmental Assessment

Figure B-4



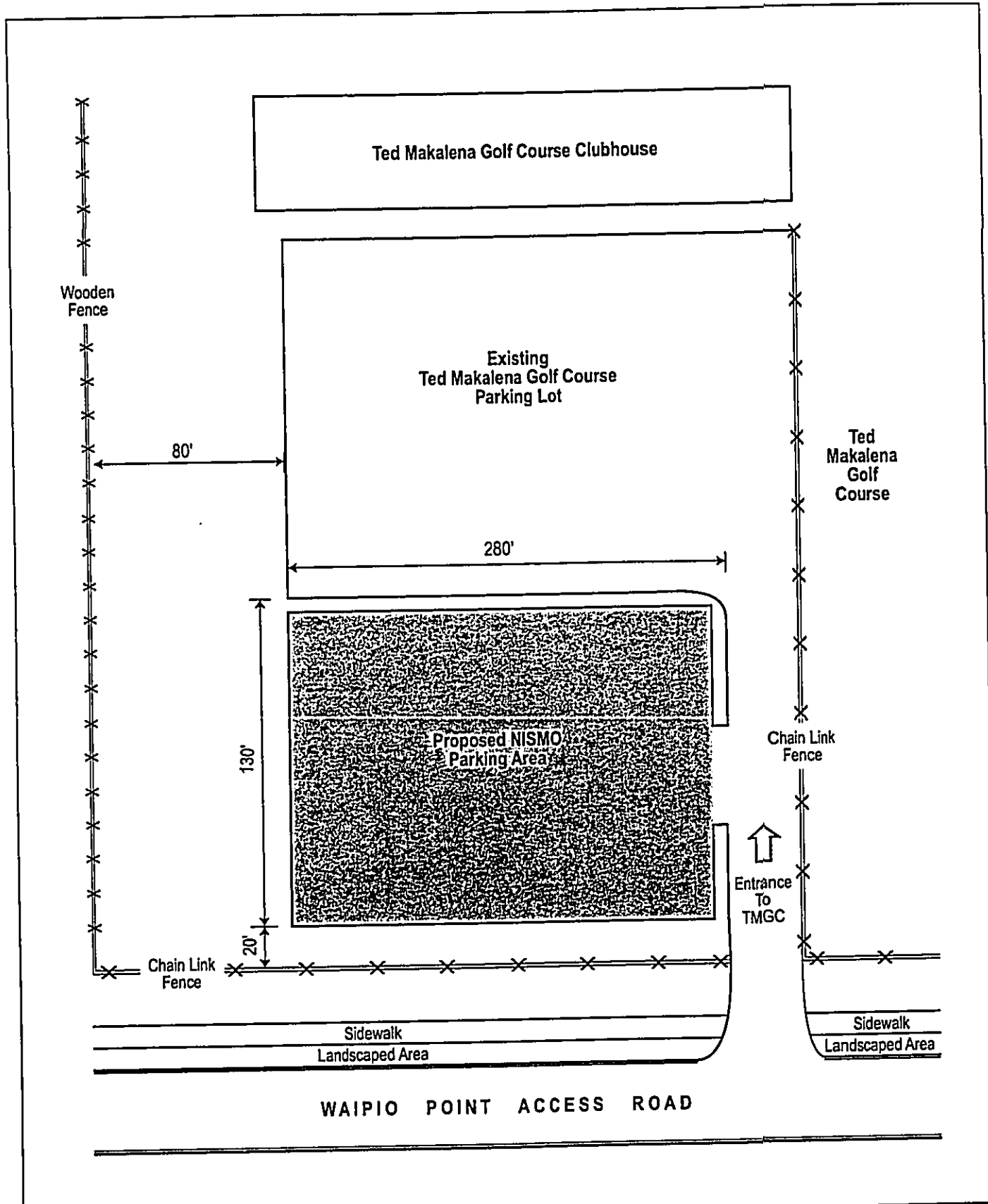
*** NOTE:
Cross Section A & B are the same as the Proposed Action.

See Figure B-4 for Locations of Cross Sections.

Alternative 2 - Relocation of Parking Area and Improved Drainage Ditch - Sections
Waipio Point Access Road Improvements
Final Environmental Assessment

Figure B-5

Not To Scale



Not To Scale

Alternative 2 - Relocation of Parking Area - Plan
 Waipio Point Access Road Improvements
 Final Environmental Assessment



Figure B-6

APPENDIX C ■

AGENCY AND PUBLIC SCOPING COMMENT LETTERS AND RESPONSES

WAIPIO POINT ACCESS ROAD Improvements

FINAL ENVIRONMENTAL ASSESSMENT



May 17, 2004

Lieutenant Colonel David C. Press
District Engineer
U.S. Army Corps of Engineers
Pacific Ocean Division, Building 230
Fort Shafter, Hawaii 96858-5440

Attention: Mr. George P. Young, Chief, Regulatory Branch

Dear Lieutenant Colonel Press:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
Army Corps of Engineers Jurisdiction

The City and County of Honolulu Department of Transportation Services (City) is proposing improvements to the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The project partially involves improving drainage conditions along Waipio Point Access Road, which in the past has caused flooding problems at the naval facility. Among the proposed drainage improvements, we plan to provide a new drainage culvert that would traverse the road, and be used to discharge stormwater directly into Pearl Harbor's Middle Loch. Due to the low elevation of this area, the culvert would be partially submerged during high tide. Also, we plan to concrete line a section of an existing ditch along the road to improve its stormwater flow capacity. This ditch drains into Waialani Drainage Channel, and during high tide the portion of the ditch near the channel is submerged. Therefore, a portion of the concrete lining would also be submerged during high tide. We do not plan to make any changes to the Waialani Drainage Channel.

Due to federal funding requirements, the City might be required to retrofit Waipio Point Access Road Bridge No. 2 over the Waialani Drainage Channel because the project involves substantial road re-construction. At this time, we do not know whether the retrofit will be required, nor do we know what this would involve.

We are planning to prepare an environmental assessment (EA) for this project. The Corps will be provided a copy of the Draft EA for review. However, at this time we would appreciate an assessment of likely Corps jurisdiction of this project based on the information provided above. Please provide your comments by June 17, 2004.

Please call me at 808-566-2246 or e-mail at sato@hbyworld.com if you have questions or require additional information.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato

Larissa Sato
Project Manager

Enclosure: Project vicinity map and proposed roadway improvements plan dated May 17, 2004

cc: Mr. Brian Suzuki, Department of Transportation Services
Over a Century of
Engineering Excellence



SENT TO
ATTENTION OF

Regulatory Branch

Ms. Larissa Sato
Project Manager
Parsons Brinckerhoff Quade & Douglas, Inc.
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Ms. Sato:

This responds to your request for comments to incorporate into a draft Environmental Assessment (IEA) for improvements to Waipio Access Road between the OR&L Bicycle/Pedestrian Path and the Waipio Peninsula Soccer Park. Based on the information provided, I have determined that portions of proposed drainage structures and any bank stabilization improvements which divert surface waters to ocean waters and are located below the Ordinary High Water Mark (OHWM) of the tidal waters of Middle Loch are jurisdictional waters of the U.S. The lateral limits of these waters which extend to the Ordinary High Water Mark (OHWM) of the open water and the adjacent, inland drainageways are therefore subject to Department of the Army (DA) permit requirements under Section 404 of the Clean Water Act and Section 10 of the Rivers and Harbors Act. The location and extent of this jurisdictional water is indicated on the survey map provided by you.

Ground disturbing activities in the upland property area and drainageway which involve activities or structures which constitute excavation of sediments and/or the discharge, or placement of dredged or fill material into the lateral limits of this jurisdictional water may require a DA permit.

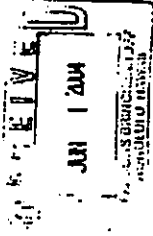
This preliminary jurisdictional determination does not obviate you, or your clients, the City and County of Honolulu, Department of Transportation Services, from addressing and complying with other federal, state, or county permits, certifications or requirements which may be required should you decide to undertake ground-disturbing activities for the proposed project.

In the future, if activities are being considered in or near jurisdictional waters which require excavation, dredging, or the placement of dredged or fill material, consultation should take place with Mr. Farley Wainnabe, of our Regulatory Branch at 438-7701 to determine if a DA permit may be required. Please refer to File Number 200-400335 if you need additional clarification or information.

Sincerely,

William B. Leonard

George P. Young, P.E.
Sgt. Chief, Regulatory Branch



DEPARTMENT OF THE ARMY
U. S. ARMY ENGINEER DISTRICT, HONOLULU
FT. SHAFTER, HAWAII 96858-5440

May 26, 2004



Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-537-7094
Fac 808-538-2368

May 12, 2004

Commander, Naval Base Pearl Harbor
U.S. Navy
Box 110
Pearl Harbor, Hawaii 96860-5020

Attention: Base Civil Engineer

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Commander:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORRL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waiwai Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2246.

Sincerely yours,

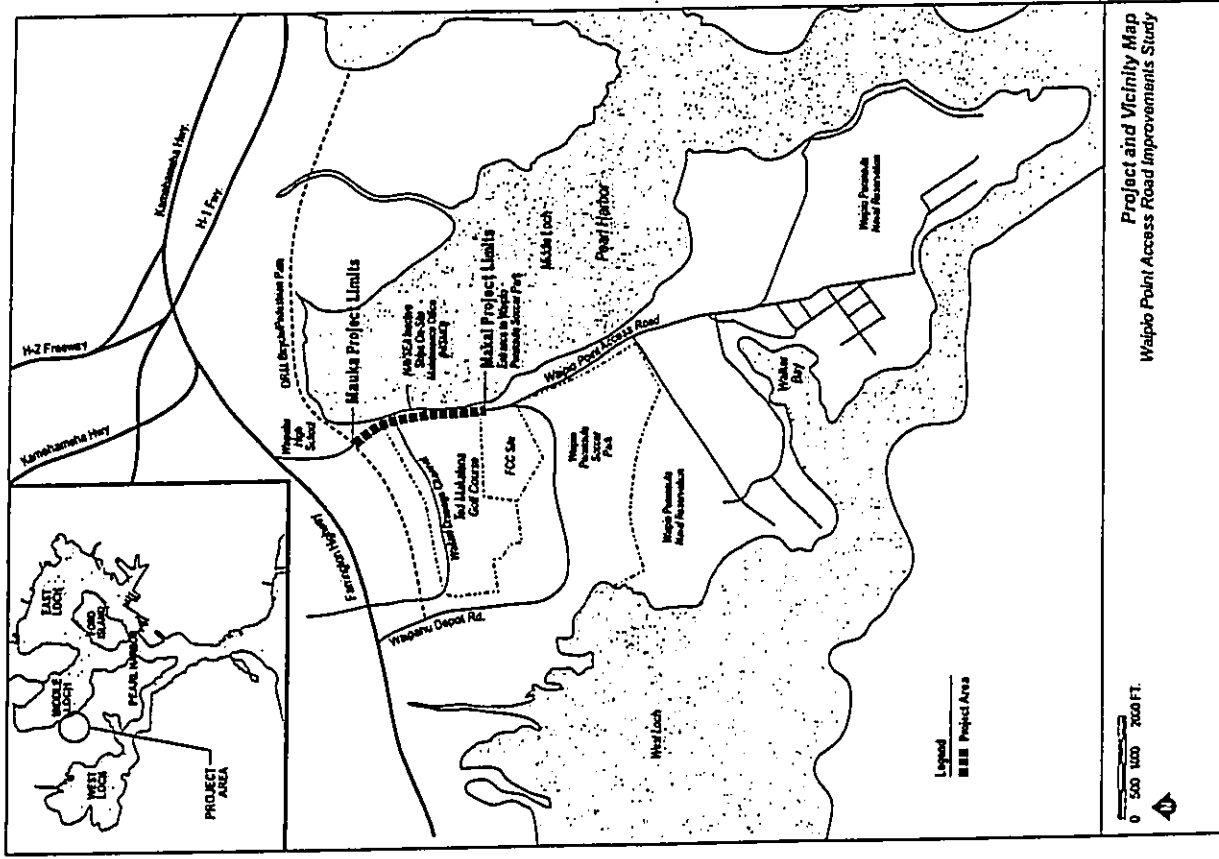
Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato

Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

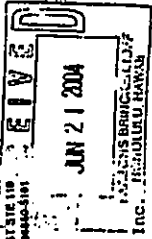
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DEPARTMENT OF THE NAVY

COMMANDER
NAVFAC HONOLULU
1001 BISHOP STREET, SUITE 3000
HONOLULU, HAWAII 96813



Ms. Larissa Sato
Project Manager
Parsons Brinckerhoff Quade & Douglas, Inc.
1001 Bishop Street Suite 3000
Honolulu HI 96813

Dear Ms. Sato:

SUBJECT: WAIPIO POINT ACCESS ROAD IMPROVEMENT PROJECT, WAIPIHU, HAWAII,
CITY & COUNTY OF HONOLULU PROJECT: 2003223

This responds to your letter of May 12, 2004 requesting information that the Navy have regarding environmental or social issues associated with the proposed Waipio Point Access Road Improvement project. The following are provided as detailed in enclosure (1).

1. An abandoned Navy JP-5 fuel line runs along the existing road alignment. Any impact that the project may have on the pipeline will need to be addressed.
2. Two JP-5 plumes from prior release exist on the groundwater table in the vicinity of the project. The City should be aware of groundwater monitoring wells along the roadway and provide adequate notice to the Navy of any plans to disturb those wells.
3. A PCB cleanup is being conducted at former transformer site W-11, to the east of the Waipio Point Access Road and bicycle path intersection. Expected completion date of the cleanup is July 2004.

This letter only addresses environmental and social issues associated with the subject project as requested by your letter. Additional concerns related to the conceptual plans provided for the project shall be addressed via separate correspondence.

Should you have any questions regarding this matter, please contact Mr. Jeffrey Takeuchi at (808) 471-1170, extension 261, fax (808) 471-1160, or email jeffrey.takeuchi@navy.mil.

Sincerely,

D. C. LEWIS
LCDR, CEC, U. S. Navy
Deputy Program Manager for Facilities,
Environmental,
Safety & Passenger Transportation
By direction of
Commander, Navy Region Hawaii

Enclosure: 1. Excerpt of Pearl Harbor Naval Complex Site Summary Report, Waipio Peninsula GSA with site map

August 2000

PHAC Site Summary Report, Waipio Peninsula GSA

Findings

Petroleum-Related Structures. There have been a total of 15 structures (8 existing, 7 demolished) used for petroleum storage, use, and distribution. Table 3-3 presents information about the petroleum-related structures.

Buildings S4, S18, S19, 48, and 81 are demolished structures within the Waipio Peninsula GSA that were used for storage, use, or distribution of petroleum substances. Building S4 was a motor gasoline storage tank situated at the abandoned Degaussing Station Range Facility. Buildings S18 and S19 were a gasoline transfer pump and a 2,500-gallon diesel oil storage tank, respectively, situated at the former Shipyard Annex Storage Area. Building 48 was located at Beckoning Point and was used for lube oil storage. Building 81 was a diesel oil pump/house situated at the former Shipyard Annex Storage Area. Building 48 was a small storage shed and, based on its size and description, it is not likely that the building was connected to a septic tank or the sewer system. Available documentation regarding these structures was reviewed, and no releases were identified. No evidence of release of petroleum substances (e.g., stressed vegetation, stained soil) from these five demolished sources was noted during the VSIs, and they have been assigned an AUP ranking of Category 2.

The Island Garage (no building number), was the property of OSC and was used to store farm equipment and chemicals used in the cane fields (Hart Crowzer 1995). Petroleum substances may also have been stored and utilized in small quantities within this structure. During the VSIs conducted for the Environmental Baseline Survey, Naval Magazine Lualabalet, West Loch Branch Agricultural Outleaves, Island of Oahu (Hart Crowzer 1995), the floor of the building, which was covered with soil, appeared to be heavily oil-stained in some areas. PACDIV real estate personnel confirmed that a concrete slab was present beneath the soil. According to building personnel, the structure was utilized only for minor machinery repairs. There was no evidence of petroleum storage or soil staining associated with the Island Garage at the time of the January 1999 site inspection. A review of available documentation and interviews conducted with building employees did not indicate evidence of past releases at the Island Garage. No evidence of release of petroleum from this source has been identified and the building has been assigned an AUP ranking of Category 2.

Building S40 (UST NS-39) and S43 (UST NS-40) are demolished USTs that contained petroleum substances. Soil samples taken from the former UST sites indicated contamination below regulatory guideline levels, and no further action is required (U.S. Department of the Navy 1997). These two buildings have been assigned an AUP ranking of Category 3.

Building 56 is the location of AST 132, an existing diesel tank. AST 117 was previously located at this site. Available historical documentation regarding these structures was reviewed, and no releases were identified. No evidence of release of petroleum substances (e.g., stressed vegetation, stained soil) from this source was noted during the VSIs, and it has been assigned an AUP ranking of Category 2.

An OWS associated with Building 55 is used to separate machine oil draining from the interior of the building. At one time, the OWS overflowed from the sewer manhole located outside the building; however, according to personnel at the building, only minimal amounts of product were released to adjacent surface soil. The employees could not recall the date of the release or duration. Based on the minimal quantity of product released, no sampling was performed (Earth Tech 1997a). No signs of stressed vegetation or stained soil were noted during the VSIs conducted in January or September 1999. This OWS has been assigned an AUP ranking of Category 3 because a release of a petroleum substance below regulatory guideline levels has been identified.

An inactive Navy JP-5 pipeline underlies the Waipio Peninsula, entering the GSA to the north of Walker Bay, then turning northeast across the northern portion of the peninsula, and paralleling the Waipio Point Access Road to the northern extent of the GSA. The pipeline has been assigned an

AUP ranking of Category 2, with the exception of a portion of the pipeline adjacent to the NISMF GSA. This section of the pipeline and associated plume are discussed below.

A release from the JP-5 pipeline was identified in September 1996 by NISMF personnel, who noted product in an unlined drainage ditch that parallels the western side of the Waipio Point Access Road. Petroleum product was also noted to be seeping to the surface through cracks in the asphalt pavement on the eastern side of the road around fence posts at the airport adjacent to Building 34 within the NISMF GSA. It was determined that an 8-inch FISC pipeline between the road and the ditch had leaked. After investigation of the site, FISC was unable to determine the amount of product released, or the location or duration of the leak. The pipeline was abandoned in place and replaced with a new pipeline in December 1996. Product was flushed from the old pipeline and the pipeline segment was capped at each end. The pipeline was active at the time of the release; however, it was taken out of service in summer 1999. The entire pipeline has been purged of JP-5 product, but has not been filled with inert gas. Groundwater monitoring has been completed, and two product plumes have been identified: one underlying the Waipio Point Access Road, within the Waipio Peninsula GSA, and one within the NISMF GSA. Development of a remedial design is recommended for the site. The portion of the pipeline where the release was identified and the associated plume have been assigned an AUP ranking of Category 5.

An active Air Force JP-8 pipeline underlies the Waipio Peninsula GSA. The subsurface pipeline extends from Pearl City to Hickam AFB. The pipeline enters the Waipio Peninsula area around Beekoning Point, where Valve Pit 33 is located, runs south to Waipio Point, where Valve Pit 34 is located, and exits the peninsula. Although numerous releases from this pipeline have occurred, no releases have been documented for property associated with the Waipio Peninsula GSA. The pipeline has been assigned an AUP ranking of Category 2.

Building 50 is an abandoned Air Force fuel tank. The tank sits upon a concrete slab within a bermed area. Although the bottom of the tank has rusted away, no signs of stained soil or stressed vegetation were noted during a VSI conducted on 15 September 1999. Available documentation did not reveal evidence of a release from this tank. This tank has been assigned an AUP ranking of Category 2.

Spills at Unidentified Locations. In addition, two oil spills have been documented and were noted in documentation reviewed for this SSR (Hart Crowser 1995; Belt Collins 1998). Information regarding location of the spills was not available; therefore, these spills are not shown on Figure 3-1. The spills, which were associated with OSC operations, occurred during 1989 and 1992. This information is based on historical Navy files and interviews with Navy and OSC personnel. Based on an August 1989 spill report, approximately 750 gallons of fuel oil were released on OSC property from an unknown source. The oil was released into the cane wash water stream, pumped into the cane wash water ponds, and overflowed into the main irrigation supply ditch. The spilled material was contained within a settling pond and associated irrigation ditches. Floating fuel was pumped out of the supply ditch and impacted soil was removed and incorporated into road material.

The second release was discovered in 1992. An oily sheen was observed by Navy personnel on the water surface of a supply ditch along an east-west peninsula road. An investigation was conducted, and the findings of the investigation indicated the mosquito larvicidal oil had drifted from a spray application onto the water's surface. A berm and containment ditch were constructed to contain the spill and prevent seepage into the West Loch. The spill was cleaned up by OSC. There is a low potential for residual impacts to soil or sediments surrounding spill areas. Based on available documentation, cleanup of the two spills was adequately performed by OSC. No evidence of the spills was seen during 1994 EBS site inspections (Hart Crowser 1995). Because all required response actions have been completed at these petroleum substance spill sites, these two spill sites have been assigned an AUP ranking of Category 4.

3.8 TRANSFORMERS

Based upon discussions with representatives from the COMNAVREG Environmental Department, review of available documentation, and the field investigation, 20 transformers are or were identified within the Waipio Peninsula GSA. These transformers were screened for potential releases of PCBs. Each transformer has been assigned a PCB category ranking corresponding to the evidence of probable release of PCBs.

3.8.1 Potential Sources of PCB Contamination

Table 3-6 lists and describes the transformers within the Waipio Peninsula GSA; these transformers are discussed and categorized in the paragraphs below.

Pad-Mounted Transformers. A total of six pad-mounted transformers have been identified from electrical system maps, a substation inventory, and the field investigation. Of the six pad-mounted transformers, four (Transformer Stations W-1, W-3, W-4, and W-5) have been assigned a PCB ranking of Category 7. Available information has been reviewed, and the installation date for these transformers could not be determined. Although these transformers either currently contain fluid with less than 50 parts per million (ppm) PCBs or may no longer be within the GSA, it is possible that in the past, these transformers were filled with dielectric fluid containing high concentrations of PCBs. PCBs may have been released as a result of former maintenance activities. Past maintenance practices for pad-mounted transformers included sampling of the transformer oil for dielectric properties and disposing of the samples onto the ground adjacent to the transformer. Although no documentation was identified during the records search indicating releases or spills of transformer fluid or transformer explosions or failures, former transformer maintenance practices could have resulted in PCB contamination of surrounding surfaces.

Transformer Station W-3 serves the Degaussing Facility (Building S17) at Beekoning Point. No soil or concrete wipe sampling data was available for this transformer. Transformer Station W-1 is situated in the southern portion of the Waipio Peninsula GSA. It was identified during the visual site reconnaissance survey, and appears to be abandoned. No information was available regarding this transformer station. Transformer Stations W-4 and W-5, which are adjacent to each other, are associated with operations at the NISMF GSA; however, the transformers are within the Waipio Peninsula GSA. Concrete wipe sampling data indicated that PCBs were below detectable levels at Transformer Station W-5. No soil sampling data was identified for Transformer Station W-5. Concrete wipe and soil sampling data were not available for Transformer Station W-4. These four transformer stations are considered Category 7 because further investigation for potential PCB contamination is recommended. Concrete wipe and soil sampling should be conducted adjacent to the transformer pads due to the former practice of disposing of transformer oil on the adjacent ground.

One pad-mounted transformer, Transformer Station W-11, has been assigned a PCB ranking of Category 6. This transformer, situated north of the NISMF GSA, is associated with NISMF operations, but is within the Waipio Peninsula GSA. Concrete wipe sampling conducted at this transformer identified PCBs at levels requiring a response action. No soil sampling data were available. Soil sampling should be conducted and further response actions are necessary to address identified PCB contamination.

One pad-mounted transformer, Transformer Station W-12 is situated near and serves the sandblast and painting buildings at the Boat Repair Facility (Buildings 62 and 63). According to PWC and building personnel, the transformer was installed in approximately 1994, and has never been filled with fluid containing high PCB concentrations (50 ppm or more). In addition, because the



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Fax: 808-528-2368

May 12, 2004

Ms. Vicki H. Tshako
Manager
U.S. Environmental Protection Agency
300 Ala Moana Boulevard, Rm 5124
Honolulu, Hawaii 96850

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Ms. Tshako:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Fairington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waianai Stream may have to be upgraded to current bikegrade standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-568-2248.

Sincerely yours,

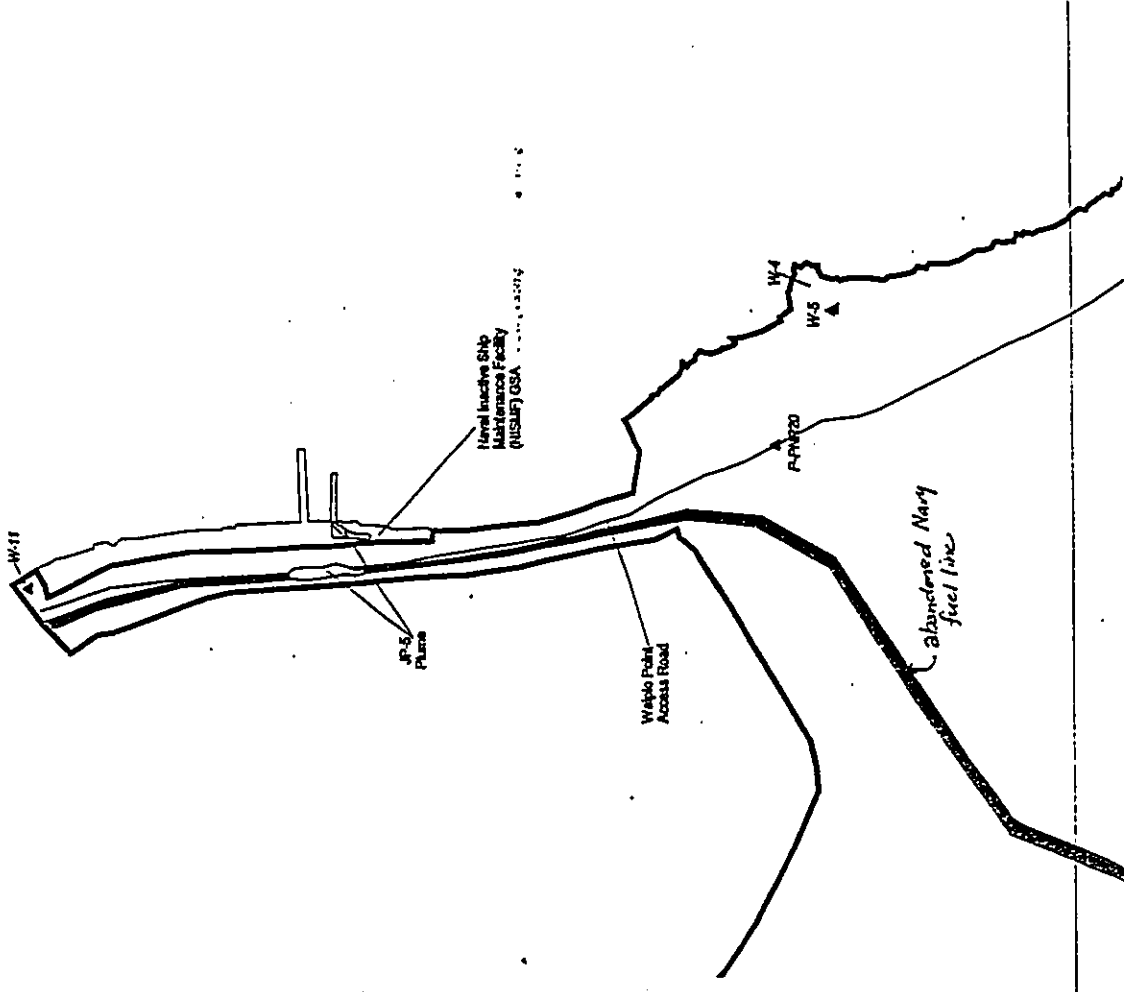
Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato

Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

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American Savings Bank Tower
1001 Bishop Street, Suite 2000
Honolulu, HI 96813
808-531-7054
Fax: 808-538-2368



May 17, 2004

Mr. Kenneth R. Nichols
Chief
Federal Communication Commission
Compliance and Information Bureau, Technology Division
Washington, D.C. 20554

Dear Mr. Nichols:

Subject: Waipio Point Access Road Improvements Study, City and County of Honolulu, Hawaii

The City and County of Honolulu is proposing improvements to Waipio Point Access Road, which is located near a FCC radio monitoring at Waipio Peninsula. The road is owned by the U.S. Navy, and the City entered into an agreement with the Navy to use the road for access to a recently completed City soccer park. As part of this agreement, the City has agreed to provide a paved parking lot along the road for workers of an adjacent naval facility, as well as making other improvements. As part of this project, the City is considering replacing the existing six-foot high chain-link fence that separates the roadway light-of-way from the adjacent Ted Makalana Golf Course with a new vinyl coated fence of about twenty-five feet in height as barrier to prevent errant golf balls from hitting parked vehicles in the new lot.

We would like to know whether the proposed new fence would interfere with the FCC radio monitoring facility. The fence is located approximately 450 feet north of your property line. A map showing the location of the new fence in relation to the facility is enclosed. Please provide any comments you may have by June 17, 2004. I can be reached at 808-566-2246 or e-mail at salod@pbworld.com if you have any questions or require additional information.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa M. V. Sato
Larissa Sato
Project Manager

Enclosures: Proposed roadway improvements plan dated May 17, 2004 showing the location of the proposed fence and FCC radio monitoring facility, and Project Vicinity Map

cc: Ms. Daiba Young, Federal Aviation Administration (with enclosures)
Mr. Brian Suzuki, Department of Transportation Services (w/o enclosures)

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Sato, Larissa M. V.

From: Sieve Davis (Sieve.Davis@fcc.gov)
Sent: Wednesday, June 09, 2004 3:54 AM
To: Sato, Larissa M. V.
Cc: Ryan Hagihara; John Raymond; Robert Crisman; Brian Buller; Sharon Agee; Joseph Casey; Rebecca Dorch; Leo Clibo
Subject: William Hokanson

Larissa,

Please find attached word document outlining our concerns and recommendations for your proposed new fencing along the Waipio Access Road near our property.

Thank you,

Ferrel Bentley
Sieve Davis

NON PUBLIC **** For Internal Use Only ****

10/20/2004

III. fencing along Waipio Point Access Road

Attn:

Larissa Sato, (sato@nbyworld.com) - (808) 531-7094
Parsons, Brincherhoff, Quade & Douglas, Inc

Thank you very much for providing the details and maps of your proposed new fencing. Ken Nichols is no longer with the FCC, please direct any further inquiries to

Equipment Development Group
3600 Hiram-Lithia Springs Rd.
Hiram, GA 30141
Director Ferrel Bentley
Assistant Stevan Davis
(770) 222-4220

Please also copy the local FCC office to keep us all aware.

The 25 foot height of metallic chain link type fencing adjacent to the Ted Makalema Golf Course near our systems would cause significant degradation to our reception capabilities.

The installation of 25 foot fencing around the proposed parking areas could be tolerated due to the increased distance from our antennas. We would however, prefer that the fence height from the end of that parking area, along the road to our property line (approximately 500 feet) be reduced in height to no more than 8 feet.

If the 25 foot height is considered imperative, then a substitute of NON Metallic fencing is requested on our side of the actual parking area. Wooden poles (with NO attached vertically oriented wires) with fiberglass or nylon type netting would be acceptable.

Thank you for your consideration,

Ferrel Bentley (Ferrel.Bentley@fcc.gov)
Director Equipment Development Group
Stevan A. Davis (Stevan.Davis@fcc.gov)
Development Section Leader

Spectrum Enforcement Division
Enforcement Bureau
Federal Communications Commission.
(770) 222-4220

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American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7054
Fax: 808-538-2368

May 12, 2004

Ms. Mary Lou Kobayashi
Planning Program Administrator
Office of Planning
Department of Business, Economic Development and Tourism
P.O. Box 2359
Honolulu, Hawaii 96804

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Ms. Kobayashi:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORRL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waitani Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements
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Fax: 808-538-2368

May 12, 2004

Major General Robert G. S. Lee
Adjutant General and Director of Civil Defense
Department of Defense
3949 Diamond Head Road
Honolulu, Hawaii 96816-4495

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Major General Lee:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORRL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waitani Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements
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May 12, 2004

Ms. Patricia Hamamoto
Superintendent
Department of Education
P.O. Box 2360
Honolulu, Hawaii 96804

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Ms. Hamamoto:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORNL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalea Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalea Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waialae Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-556-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Patricia Sato
Patricia Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

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UNIVERSITY
OF HAWAII
SYSTEMS



STATE OF HAWAII
DEPARTMENT OF EDUCATION
P.O. BOX 2360
HONOLULU, HAWAII 96804

OFFICE OF THE SUPERINTENDENT
May 27, 2004

Ms. Larissa Sato, Project Manager
American Savings Bank Tower
Parsons Brinckerhoff Quade & Douglas, Inc.
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Ms. Sato:

SUBJECT: Pre-Assessment Consultation for
Waipio Point Access Road Improvements
Waipahu (Project # 2003223)

The Department of Education has no comment or concern with the proposed improvements to the Waipio Point Access Road. However, plans are underway for the construction of an eight-classroom building at Waipahu High School. To determine if there might be any potential conflicts with use of the access road near the high school, contact Glenn T. Kinura at Kinura International.

If you have any questions, please call Rae M. Loni, Assistant Superintendent of the Office of Business Services, at 586-3444 or Heidi Mecker of the Facilities and Support Services Branch at 733-4862.

Very truly yours,

Patricia Hamamoto
Patricia Hamamoto
Superintendent

PL:amp

c: Rae M. Loni, OBS
FSSB
Karen Moriyma, CAS/Nanukuli, Pearl City, Waipahu Complex Area
Glenn T. Kinura, Kinura International

AN AFFIRMATIVE ACTION AND EQUAL OPPORTUNITY EMPLOYER

MAY 28 2004
HONOLULU, HAWAII

PATRICIA HAMAMOTO
SUPERINTENDENT



Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3100
Honolulu, HI 96813
808-531-7094
Fax: 808-528-2268

May 12, 2004

Ms. Chiyome Fukino, M.D.
Director
Department of Health
P.O. Box 3378
Honolulu, Hawaii 96801

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Ms. Fukino, M.D.:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalana Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalana Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waianani Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-565-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

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STATE OF HAWAII
DEPARTMENT OF HEALTH
P.O. Box 3178
HONOLULU, HAWAII 96813-0178

June 3, 2004

Ms. Larissa Sato
Parsons, Brinckerhoff, Quade & Douglas, Inc
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Ms. Sato:

SUBJECT: Waipio Point Access Road Improvements Study
Waipahu, Hawaii

Thank you for allowing us to review and comment on the subject document. We have the enclosed standard comments to offer. If there are any questions about these standard comments, please contact Ryan Davenport with the Environmental Planning Office at 586-4346.

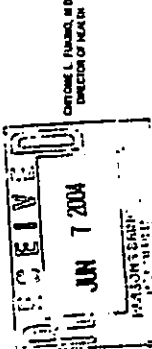
Sincerely,

June F. Harrigan-Lum

JUNE F. HARRIGAN-LUM, MANAGER
Environmental Planning Office

Enclosure

- c. SDWB
- EPO
- SHWB
- NRAIQ
- CWB
- WWB
- CAB
- HEER



STATE OF HAWAII
DEPARTMENT OF HEALTH
P.O. Box 3178
HONOLULU, HAWAII 96813-0178

By: *[Signature]*, Director of Health

EPO-04-124

3. Identify any planning documents, groups, and projects that include specific prescriptions for water quality management at the proposed project site and in the potentially affected waterbodies. Please note those prescriptions that may specifically apply to the proposed project.

Pending water quality management actions

4. Identify all potentially affected water bodies that appear on the current *List of Impaired Waters in Hawaii Prepared under Clean Water Act §303(d)* including the listed waterbody, geographic scope of listing, and pollutant(s) (See Table 7 at <http://www.state.hi.us/doh/ehpnp/wqm/303dpcfinal.pdf>).
5. If the proposed project involves potentially affected water bodies that appear on the current *List of Impaired Waters in Hawaii Prepared under Clean Water Act §303(d)*, identify and quantify expected changes in the following site and watershed conditions and characteristics:
 - surface permeability
 - hydrologic response of surface (timing, magnitude, and pathways)
 - receiving water hydrology
 - runoff and discharge constituents
 - pollutant concentrations and loads in receiving waters
 - aquatic habitat quality and the integrity of aquatic biota

Where TMDLs are already established they include pollutant load allocations for the surrounding lands and point source discharges. In these cases, we suggest that the submittal specify how the proposed project would contribute to achieving the applicable load reductions.

Where TMDLs are yet to be established and implemented, a first step in achieving TMDL objectives is to prevent any project-related increases in pollutant loads. This is generally accomplished through the proper application of suitable best management practices in all phases of the project and adherence to any applicable ordinances, standards, and permit conditions. In these cases we suggest that the submittal specify how the proposed project would contribute to reducing the polluted discharge and runoff entering the receiving waters, including plans for additional pollutant load reduction practices in future management of the surrounding lands and drainage/discharge systems.

Proposed Action and Alternatives Considered

We suggest that each submittal identify and analyze potential project impacts at a watershed scale by considering the potential contribution of the proposed project to cumulative, multi-project watershed effects on hydrology, water quality, and aquatic and riparian ecosystems.

We also suggest that each submittal broadly evaluate project alternatives by identifying more than one engineering solution for proposed projects. In particular, we suggest the

Standard Comments

Environmental Planning Office Dated 3/2/04

The Environmental Planning Office (EPO) is responsible for several surface water quality management programs mandated by the federal Clean Water Act or dictated by State policy. (<http://www.state.hi.us/doh/ehpnp/wqm/wqm.htm>). Among these responsibilities, EPO:

- maintains the *List of Impaired Waters in Hawaii Prepared under Clean Water Act §303(d)* (<http://www.state.hi.us/doh/ehpnp/wqm/303dpcfinal.pdf>);
- develops and establishes Total Maximum Daily Loads (TMDLs) for listed waters (suggesting how much existing pollutant loads should be reduced in order to attain water quality standards, please see <http://www.epa.gov/owow/tmdl/fintro.html>);
- writes TMDL Implementation Plans describing how suggested pollutant load reductions can be achieved; and
- conducts assessments of stream habitat quality and biological integrity.

To facilitate TMDL development and planning, and to assist our assessment of the potential impact of proposed actions upon water quality, pollutant loading, and biological resources in receiving waters, we suggest that environmental review documents, permit applications, and related submittals include the following standard information and analyses:

Waterbody type and class

1. Identify the waterbody type and class, as defined in Hawaii Administrative Rules Chapter 11-54 (<http://www.state.hi.us/doh/rules/11-54.pdf>), of all potentially affected water bodies.

Existing water quality management actions

2. Identify any existing National Pollutant Discharge Elimination System (NPDES) permits and related connection permits (issued by permittees) that will govern the management of water that runs off or is discharged from the proposed project site or facility. Please include NPDES and other permit numbers; names of permittees, permitted facilities, and receiving waters (including waterbody type and class as in 1. above); diagrams showing drainage/discharge pathways and outfall locations; and note any permit conditions that may specifically apply to the proposed project.

consideration of "alternative," "soft," and "green" engineering solutions for channel modifications that would provide a more environmentally friendly and aesthetically pleasing channel environment and minimize the destruction of natural landscapes.

If you have any questions about these comments or EPO programs, please contact Ryan Davenport at 586-4346.

¹"Potentially affected waterbodies" means those in which proposed project activity would take place and any that could receive water discharged by the proposed project activity or water flowing down from the proposed project site. These waterbodies can be presented as a chain of receiving waters whose top link is at the project site upslope and whose bottom link is in the Pacific Ocean, and can be named according to conventions established by Chapter 11-54 and the *List of Impaired Waters in Hawaii Prepared under Clean Water Act §303(d)*. For example, a recent project proposed for Nuhelewai Stream, Oahu might potentially affect Nuhelewai Stream, Kapalama Canal, and Honolulu Harbor and Shore Areas.

[OTHER EXAMPLES OR DIAGRAM??]

Solid and Hazardous Waste Branch Dated 3/2/04

1) The OSWM recommends the development of a solid waste management plan that encompasses all project phases including demolition, construction, and occupation/operation of the completed project.

Specific examples of elements that the plan should address include:

- The recycling of green-waste during clear and grub activities;
- Recycling construction and demolition wastes, if appropriate;
- The use of locally produced compost in landscaping;
- The use of recycled content building materials;
- The provision of recycling facilities in the design of the project.

2) The developer shall ensure that all solid waste generated during project construction is directed to a Department of Health permitted solid waste disposal or recycling facility.

3) The developer should consider providing space in the development for recycling activities. The provision of space for recycling bins for paper, glass, and food/wet waste would help to encourage the recycling of solid waste(s) generated by building occupants.

4) The discussion of solid waste issues contained in the document is restricted to activities within the completed project. The OSWM recommends the development of a solid waste management plan that encompasses all project phases, from construction (and or demolition) to occupation of the project.

Specific examples of plan elements include: the recycling of green-waste during clear and grub activities; maximizing the recycling of construction and demolition wastes; the use of locally produced compost in the landscaping of the project; and the provision of recycling facilities in the design of the project.

5) Hawaii Revised Statutes Chapter 103D-407 stipulates that all highway and road construction and improvement projects funded by the State or a county or roadways that are to be accepted by the State or a county as public roads shall utilize a minimum of ten per cent crushed glass aggregate as specified by the department of transportation in all base-course (treated or untreated) and sub-base when the glass is available to the quarry or contractor at a price no greater than that of the equivalent aggregate.

If you have any questions, please contact the Solid and Hazardous Waste Branch at (808) 586-4240.

Noise, Radiation & Indoor Air Quality Branch Dated 3/2/04

*Project activities shall comply with the Administrative Rules of the Department of Health:

- Chapter 11-39 Air Conditioning and Ventilating.
- Chapter 11-45 Radiation Control.
- Chapter 11-46 Community Noise Control.
- Chapter 11-501 Asbestos Requirements.
- Chapter 11-502 Asbestos-Containing Materials in Schools.
- Chapter 11-503 Fees for Asbestos Removal and Certification
- Chapter 11-504 Asbestos Abatement Certification Program

Should there be any questions, please contact Russell S. Takata, Environmental Health Program Manager, Noise, Radiation and Indoor Air Quality Branch, at 586-4701.*

Clean Water Branch Dated 3/2/04

1. The Army Corps of Engineers should be contacted at (808) 438-9258 to identify whether a Federal license or permit (including a Department of Army permit) is required for this project. Pursuant to Section 401(b)(1) of the Federal Water Pollution Act (commonly known as the "Clean Water Act"), a Section 401 Water Quality Certification is required for "[e]very applicant for Federal license or permit to conduct any activity including, but not limited to, the construction or operation of facilities, which may result in any discharge into the navigable waters...."

2. A National Pollutant Discharge Elimination System (NPDES) general permit coverage is required for the following activities:

Please submit a copy of the request for review by SHPD or SHPD's determination letter for the project.

If you have any questions, please contact the CWB at 586-4309.

Waste Water Branch Dated 3/2/04

All wastewater plans must conform to applicable provisions of the Department of Health's Administrative Rules, Chapter 11-62, "Wastewater Systems". We do reserve the right to review the detailed wastewater plans for conformance to applicable rules.

Should you have any questions, please contact the Planning & Design Section of the Wastewater Branch at 586-4294.

Clean Air Branch Dated 3/2/04

Construction/Demolition Involving Asbestos:

Since the proposed project would entail renovation/demolition activities which may involve asbestos, the applicant should contact the Asbestos Abatement Office in the Noise, Radiation and Indoor Air Quality Branch at 586-5800.

Control of Fugitive Dust:

A significant potential for fugitive dust emissions exists during all phases of construction. Proposed construction activities will occur in proximity to existing residences, businesses, public areas and thoroughfares, thereby exacerbating potential dust problems. It is recommended that a dust control management plan be developed which identifies and addresses all activities that have a potential to generate fugitive dust. Implementation of adequate dust control measures during all phases of development and construction activities is warranted.

Construction activities must comply with the provisions of Hawaii Administrative Rules, §11-60.1-33 on Fugitive Dust.

The contractor should provide adequate measures to control dust from the road areas and during the various phases of construction. These measures include, but are not limited to, the following:

- a) Plan the different phases of construction, focusing on minimizing the amount of dust-generating materials and activities, centralizing on-site vehicular traffic routes, and locating potential dust-generating equipment in areas of the least impact;
- b) Provide an adequate water source at the site prior to start-up of construction activities;
- c) Landscape and provide rapid covering of bare areas, including slopes, starting from the initial grading phase;
- d) Minimize dust from shoulders and access roads;

a. Storm water associated with industrial activities, as defined in Title 40, Code of Federal Regulations, Sections 122.26(b)(14)(i) through 122.26(b)(14)(ix) and 122.26(b)(14)(xi).

b. Construction activities, including clearing, grading, and excavation, that result in the disturbance of equal to or greater than one (1) acre of total land area. The total land area includes a contiguous area where multiple separate and distinct construction activities may be taking place at different times on different schedules under a larger common plan of development or sale. An NPDES permit is required before the commencement of the construction activities.

c. Discharges of treated effluent from leaking underground storage tank remedial activities.

d. Discharges of once through cooling water less than one (1) million gallons per day.

e. Discharges of hydrotesting water.

f. Discharges of construction dewatering effluent.

g. Discharges of treated effluent from petroleum bulk stations and terminals.

h. Discharges of treated effluent from well drilling activities.

i. Discharges of treated effluent from recycled water distribution systems.

j. Discharges of storm water from a small municipal separate storm sewer system.

k. Discharges of circulation water from decorative ponds or tanks.

The CWB requires that a Notice of Intent (NOI) to be covered by a NPDES general permit for any of the above activities be submitted at least 30 days before the commencement of the respective activities. The NOI forms may be picked up at our office or downloaded from our website at <http://www.state.hi.us/health/cwb/forms/geni-index.html>.

3. The applicant may be required to apply for an individual NPDES permit if there is any type of activity in which wastewater is discharged from the project into State waters and/or coverage of the discharge(s) under the NPDES general permit(s) is not permissible (i.e. NPDES general permits do not cover discharges into Class 1 or Class AA receiving waters). An application for the NPDES permit is to be submitted at least 180 days before the commencement of the respective activities. The NPDES application forms may also be picked up at our office or downloaded from our website at <http://www.state.hi.us/health/cwb/forms/indiv-index.html>.

4. Hawaii Administrative Rules, Section 11-55-38, also requires the owner to either submit a copy of the new NOI or NPDES permit application to the State Department of Land and Natural Resources, State Historic Preservation Division (SHPD), or demonstrate to the satisfaction of the DOH that the project, activity, or site covered by the NOI or application has been or is being reviewed by SHPD.

- c) Provide adequate dust control measures during weekends, after hours, and prior to daily start-up of construction activities; and
- f) Control dust from debris being hauled away from the project site.

Hazard Evaluation and Emergency Response (HEER) Dated 3/2/04

1. A phase I Environmental Site Assessment (ESA) should be conducted for developments or redevelopments. If the investigation shows that a release of petroleum, hazardous substance, pollutants or contaminants occurred at the site, the site should be properly characterized through an approved Hawaii State Department of Health (DOH) Hazard Evaluation and Emergency Response Office (HEER) soil and/or groundwater sampling plan. If the site is found to be contaminated, then all removal and remedial actions to clean up hazardous substance or oil releases by past and present owners/tenants must comply with chapter 128D, Environmental Response Law, HRS, and Title 11, Chapter 451, HAR, State Contingency Plan.
2. All lands formerly in the production of sugarcane should be characterized for arsenic contamination. If arsenic is detected above the US EPA Region (preliminary remediation goal (PRG) for non-cancer effects, then a removal and/or remedial plan must be submitted to the Hazard Evaluation and Emergency Response (HEER) Office of the State Department of Health for approval. The plan must comply with Chapter 128D, Environmental Response Law, HRS, and Title 11, Chapter 451, HAR, State Contingency Plan.
3. If the land has a history of previous releases of petroleum, hazardous substances, pollutants, or contaminants, we recommend that the applicant request a "no further action" (NFA) letter from the Hawaii State Department of Health (DOH) Hazard Evaluation and Emergency Response (HEER) Office prior to the approval of the land use change or permit approval.

Safe Drinking Water Branch Dated 3/11/04

The Safe Drinking Water Branch administers programs in the areas of: 1) public water systems; 2) underground injection control; and 3) groundwater protection. Our general comments on projects are as follows.

Public Water Systems

Federal and state regulations define a public water system as a system that serves 25 or more individuals at least 60 days per year or has at least 15 service connections. All public water system owners and operators are required to comply with Hawaii Administrative Rules, Title 11, Chapter 20, titled Rules Relating to Potable Water Systems.

All new public water systems are required to demonstrate and meet minimum capacity requirements prior to their establishment. This requirement involves demonstration that the system will have satisfactory technical, managerial and financial capacity to enable the system to comply with safe drinking water standards and requirements.

Projects that propose development of new sources of potable water serving or proposed to serve a public water system must comply with the terms of Section 11-20-29 of Chapter 20. This section requires that all new public water system sources be approved by the Director of Health prior to its use. Such approval is based primarily upon the submission of a satisfactory engineering report which addresses the requirements set in Section 11-20-29.

The engineering report must identify all potential sources of contamination and evaluate alternative control measures which could be implemented to reduce or eliminate the potential for contamination, including treatment of the water source. In addition, water quality analyses for all regulated contaminants, performed by a laboratory certified by the State Laboratories Division of the state of Hawaii, must be submitted as part of the report to demonstrate compliance with all drinking water standards. Additional parameters may be required by the Director for this submittal or additional tests required upon his or her review of the information submitted.

All sources of public water system sources must undergo a source water assessment which will delineate a source water protection area. This process is preliminary to the creation of a source water protection plan for that source and activities which will take place to protect the source of drinking water.

Projects proposing to develop new public water systems or proposing substantial modifications to existing public water systems must receive approval by the Director of Health prior to construction of the proposed system or modification. These projects include treatment, storage and distribution systems of public water systems. The approval authority for projects owned and operated by a County Board or Department of Water or Water Supply has been delegated to them.

All public water systems must be operated by certified distribution system and water treatment plant operators as defined by Hawaii Administrative Rules, Title 11, Chapter 11-25 titled; Rules Pertaining to Certification of Public Water System Operators.

All projects which propose the use of dual water systems or the use of a non-potable water system in proximity to an existing potable water system to meet irrigation or other needs must be carefully design and operate these systems to prevent the cross-connection of these systems and prevent the possibility of backflow of water from the non-potable system to the potable system. The two systems must be clearly labeled and physically separated by air gaps or reduced pressure principle backflow prevention devices to avoid contaminating the potable water supply. In addition backflow devices must be tested periodically to assure their proper operation. Further, all non-potable spigots and irrigated areas

should be clearly labeled with warning signs to prevent the inadvertent consumption of non-potable water. Compliance with Hawaii Administrative Rules, Title 11, Chapter 11-21 titled: Cross-Connection and Backflow Control is also required.

All projects which propose the establishment of a potentially contaminating activity (as identified in the Hawaii Source Water Assessment Plan) within the source water protection area of an existing source of water for a public water supply should address this potential and activities that will be implemented to prevent or reduce the potential for contamination of the drinking water source.

For further information concerning the application of capacity, new source approval, operator certification, source water assessment, backflow/cross-connection prevention or other public water system programs, please contact the Safe Drinking Water Branch at 586-4258.

Underground Injection Control (UIC)

Injection wells used for the subsurface disposal of wastewater, sewage effluent, or surface runoff are subject to environmental regulation and permitting under Hawaii Administrative Rules, Title 11, Chapter 11-23, titled Underground Injection Control (UIC). The Department of Health's approval must be first obtained before any injection well construction commences. A UIC permit must be issued before any injection well operation occurs.

Authorization to use an injection well is granted when a UIC permit is issued to the injection well facility. The UIC permit contains discharge and operation limitations, monitoring and reporting requirements, and other facility management and operational conditions. A complete UIC permit application form is needed to apply for a UIC permit.

A UIC permit can have a valid duration of up to five years. Permit renewal is needed to keep an expiring permit valid for another term.

For further information about the UIC permit and the Underground Injection Control Program, please contact the UIC staff of the Safe Drinking Water Branch at 586-4258.

Groundwater Protection Program

Projects that propose to develop a golf course are asked to use the Guidelines Applicable to Golf Courses in Hawaii (Version 6) in order to address certain groundwater protection concerns, as well as other environmental concerns



May 12, 2004

Ms. Genevieve Sainonson
Director
Office of Environmental Quality Control
735 S. Beretania Street, Suite 702
Honolulu, Hawaii 96813

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Ms. Sainonson:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORNL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waianai Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Louisa Saito
Project Manager

Louisa Saito
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

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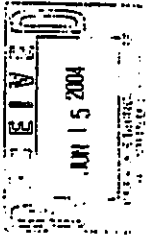
Parsons
Brinckerhoff
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808 531-7094
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LINDA SINGLE
GOVERNOR OF HAWAII



STATE OF HAWAII
OFFICE OF ENVIRONMENTAL QUALITY CONTROL
225 SOUTH WEEHOLE CUNARD STREET
SUITE 202
HONOLULU, HAWAII 96813
PHONE: (808) 551-2100
FAX: (808) 551-2101
E-MAIL: oeq@hawaii.gov

GENEVEVE SALMONSON
DIRECTOR



June 14, 2004

Ms. Larissa Sato
Parsons Brinckerhoff Quade & Douglas, Inc.
1001 Bishop Street, Suite 3000
Honolulu, HI 96813

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii

Dear Ms. Sato:

We are in receipt of your letter dated May 12, 2004 regarding the Waipio Point Access Road Improvements.

We have the following comments:

1. In the future, does the City plan to extend the project to include the Waipio soccer park?
2. Please address the method that will be used to prevent runoff from the bicycle/pedestrian path into the Middle Loch.

We have no further comments to offer at this time, but will reserve further comments when the documents are submitted.

Should you have any questions, please feel free to call our office at 586-4185.

Sincerely,

Genevieve Salmonson
Genevieve Salmonson
Director



May 12, 2004

Mr. Peter Young
Chairperson
Department of Land and Natural Resources
P.O. Box 621
Honolulu, Hawaii 96809

Dear Mr. Young:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OIRL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

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Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-568-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

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Engineering Excellence



May 12, 2004

Ms. Yvonne Izué
Deputy Director
Commission on Water Resource Management
Department of Land and Natural Resources
P.O. Box 621
Honolulu, Hawaii 96809

Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-537-7094
Fax: 808-528-2365

Dear Ms. Izué:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 20032223
Pre-Assessment Consultation

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORRL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

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Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Salo
Larissa Salo
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

Over a Century of
Engineering Excellence

LOCAL OFFICE



STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES
COMMISSION ON WATER RESOURCE MANAGEMENT
P.O. BOX 621
HONOLULU, HAWAII 96809

May 18, 2004

Ms. Larissa Salo
Project Manager
Parsons Brinckerhoff Quade & Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813

Dear Ms. Salo:

Waipio Access Road Improvements

This is in reference to your May 12, 2004 letter requesting whether a stream channel alteration permit (Hawaii Revised Statutes 174C-71) will be required for a bicycle path, landscaping, and bridge improvements at Waipio Access Road, Waipahu.

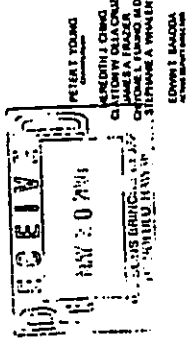
A stream channel alteration permit will be required only if the bed or banks of Waianai Stream are altered.

This project may require other agency approvals regarding water quality, wetlands, grading, grubbing, stockpiling, and floodway and drainage maintenance. This letter shall not constitute or imply compliance with other Federal, state or county rules.

Thank you for your coordination. If you have any questions regarding this letter, please call David Higa at 587-0249.

Sincerely,

Edwin T. Sakoda
EDWIN T. SAKODA
Acting Deputy Director



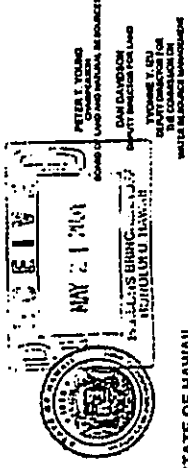


May 12, 2004

Mr. Michael Buck
Administrator
Division of Forestry and Wildlife
Department of Land and Natural Resources
P.O. Box 621
Honolulu, Hawaii 96809

Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7094
Fax: 808-528-2343

LUCIA LEMLE
GOVERNOR OF HAWAII



STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES
DIVISION OF FORESTRY AND WILDLIFE
1151 PUNCHBOWL STREET
HONOLULU, HAWAII 96813
May 18, 2004

PERE L. HUNING
COMMISSIONER
DIVISION OF LAND AND NATURAL RESOURCES
DAN DAVIDSON
DEPUTY DIRECTOR FOR LAND
MANAGEMENT
TYOMME Y. LIU
DEPUTY DIRECTOR FOR
WATER RESOURCE MANAGEMENT
ADJUTANT GENERAL
BRIAN J. HARRIS
DEPUTY DIRECTOR FOR
WILDLIFE
COMMISSIONER
DEPARTMENT OF LAND AND NATURAL RESOURCES
HONOLULU, HAWAII 96813
MAY 21 2004

Dear Mr. Buck:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waianihi Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-506-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

Over a Century of
Engineering Excellence

Ms. Larissa Sato
Parsons Brinckerhoff Quade & Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Ms. Sato:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii by
City and County of Honolulu Project: 2003223, Pre-Assessment
Consultation.

We appreciate the opportunity to comment on your project. DLNR,
Division of Forestry and Wildlife has reviewed your attached subject proposal
dated May 12, 2004 and we do not have any comments to your road improvement
at Waipio Point Access Road, as it will not impact any of DOFAW's management
programs. Thank you for allowing us to comment on your project.

Sincerely yours,

Michael G. Buck

Michael G. Buck
Administrator



May 12, 2004

Ms. Dede Mamiya, Administrator
Department of Land and Natural Resources
Land Division
1151 Punchbowl Street, Room 220
Honolulu, Hawaii 96813

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Ms. Mamiya:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

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Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-565-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

Over a Century of
Engineering Excellence

Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Samoa Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7094
Fax: 808-528-2368

LAND AND NATURAL
RESOURCES
DIVISION



STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES
LAND DIVISION

POST OFFICE BOX 621
HONOLULU, HAWAII 96809

June 1, 2004

WAIPIOPOINT.RCH

Parsons Brinckerhoff Quade & Douglas, Inc.
Larissa Sato, Project Manager
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Ms. Sato:

SUBJECT: Pre-Assessment Consultation for the Waipio Point Access
Road Improvements Study, Waipahu, Oahu, Hawaii
City and County of Honolulu Project 2003223

Thank you for the opportunity to review and comment on the subject matter.

A copy of your letter dated May 14, 2004 pertaining to the proposed project was transmitted or made available to the following Department of Land and Natural Resources' Divisions for their review and comment.

- Division of Aquatic Resources
- Division of Forestry & Wildlife
- Division of State Parks
- Engineering Division
- Commission on Water Resource Management
- Office of Conservation and Coastal Lands
- Land-Oahu District Land Office
- Land-Planning and Development

Enclosed please find a copy of the Engineering Division comment.

Based on the attached responses, the Department of Land and Natural Resources has no other comment to offer on the subject matter at this time.

Should you have any questions, please contact Nicholas A. Vaccaro of the Land Division Support Services Branch at 587-0384.

Very truly yours,

Nicholas A. Vaccaro
NICHOLAS A. VACCARO
Administrator

C: OBILO

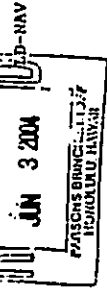
PETER T. YOUNG
DIRECTOR
BOARD OF LAND AND NATURAL RESOURCES
COMMISSION ON WATER RESOURCE MANAGEMENT

DAN DAVIDSON
LAND DIVISION DIRECTOR

YOUNG T. SU
DEPUTY DIRECTOR, WATER

AQUATIC RESOURCES
DIVISION
BOARD OF LAND AND NATURAL RESOURCES
COMMISSION ON WATER RESOURCE MANAGEMENT

LAND DIVISION
BOARD OF LAND AND NATURAL RESOURCES
COMMISSION ON WATER RESOURCE MANAGEMENT



LAND AND NATURAL RESOURCES
ENGINEERING DIVISION

RECEIVED
LAND DIVISION

2004 MAY 25 P 3:35



STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES
LAND DIVISION
POST OFFICE BOX 521
HONOLULU, HAWAII 96809
May 14, 2004

PETER T. YOUNG
BOARD OF LAND AND NATURAL RESOURCES
COMMISSIONER OF LAND AND NATURAL RESOURCES
DAN DAVISON
DEPUTY DIRECTOR - LAND
ERNEST Y. LAU
DEPUTY DIRECTOR - WATER
ADRIATIC RESOURCES
MANAGEMENT CONSULTANTS
COMMISSION ON WATER RESOURCES MANAGEMENT
CONSULTANTS AND MANAGERS
LAW OFFICES
HONOLULU, HAWAII
HONOLULU ISLAND PLANNING COMMISSION
STATE PLANNING

DEPARTMENT OF LAND AND NATURAL RESOURCES
ENGINEERING DIVISION

LARAY

Ref: WAIPIOPOINT.CMT

COMMENTS

- () We confirm that the project site, according to the Flood Insurance Rate Map (FIRM), is located in Flood Zone _____.
 - (X) Please take note that the project site, according to the Flood Insurance Rate Map (FIRM), is located in Zones X & AE.
 - () Please note that the correct Flood Zone Designation for the project site according to the Flood Insurance Rate Map (FIRM) is _____.
 - (X) Please note that the project must comply with the rules and regulations of the National Flood Insurance Program (NFIP) presented in Title 44 of the Code of Federal Regulations (44CFR), whenever development within a Special Flood Hazard Area is undertaken. If there are any questions, please contact the State NFIP Coordinator, Ms. Carol Tyau-Beams, of the Department of Land and Natural Resources, Engineering Division at (808) 587-0767.
- Please be advised that 44CFR indicates the minimum standards set forth by the NFIP. Your Community's local flood ordinance may prove to be more restrictive and thus take precedence over the minimum NFIP standards. If there are questions regarding the local flood ordinance, please contact the applicable County NFIP Coordinators below:
- (X) Mr. Robert Sualimao at (808) 523-4154 or Mr. Mario Sia Li at (808) 523-4247 of the City and County of Honolulu, Department of Planning and Permitting.
 - () Mr. Kelly Gomez at (808) 961-8327 (Hilo) or Mr. Kuran Eiler at (808) 327-3530 (Kona) of the County of Hawaii, Department of Public Works.
 - () Mr. Francis Cruz at (808) 270-7771 of the County of Maui, Department of Planning.
 - () Mr. Mario Antonio at (808) 241-6620 of the County of Kauai, Department of Public Works.

- () The applicant should include project water demands and infrastructure required to meet water demands. Please note that the implementation of any State-sponsored projects requiring water service from the Honolulu Board of Water Supply system must first obtain water allocation credits from the Engineering Division before it can receive a building permit and/or water meter.
- () The applicant should provide the water demands and calculations to the Engineering Division so it can be included in the State Water Projects Plan Update.

Additional Comments: _____

Other: _____

Should you have any questions, please call Mr. Andrew Mooka of the Planning Branch at 587-0229.

Signed: Eric T. Hirano
ERIC T. HIRANO, CHIEF ENGINEER
Date: 5/25/04

LD/NAV
WAIPIOPOINT.CMT

Suspense Date: 5/27/04

MEMORANDUM:

- TO: XXX Division of Aquatic Resources
 XXX Division of Forestry & Wildlife
 XXX Division of State Parks
 XXX Engineering Division
 Division of Boating and Ocean Recreation
 XXX Commission on Water Resource Management
 XXX Office of Conservation and Coastal Lands
 XXX Land-Oahu District Land Office
 XXX Land-Planning and Development

FROM: Dierdre S. Hemiya, Administrator
Land Division

SUBJECT: Waipio Point Access Road Improvement Study, Waipahu, Island of Oahu, Hawaii
City and County of Honolulu Project 2003223
Consultant: Parson Brinckerhoff Quade & Douglas, Inc.
Pre-Assessment Consultation
TMK: 4-3-12-001

Please review the attached letter dated May 12, 2004 pertaining to the subject matter and submit your comment (if any) on Division letterhead signed and dated by the suspense date.

Should you need more time to review the subject matter, please contact Nick Vaccaro at ext.: 7-0384. If this office does not receive your comments by the suspense date, we will assume there are no comments.

() We have no comments. Comments attached.

Signed: Eric T. Hirano Date: 5/25/04

Name: ERIC T. HIRANO, CHIEF ENGINEER Division: Engineering

2802

PETER T. YOUNG
 CHAIRMAN
 BOARD OF LAND AND NATURAL RESOURCES
 COMMISSIONER OF NATURAL RESOURCES MANAGEMENT

DAN DAVIDSON
 DEPUTY DIRECTOR - LAND

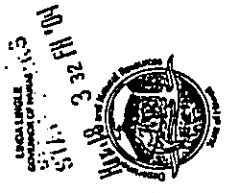
ERNEST Y. K. LAU
 DEPUTY DIRECTOR - WATER

AGRICULTURE
 BOATING AND OCEAN RECREATION
 FORESTRY AND WILDLIFE
 COMMISSIONER OF NATURAL RESOURCES MANAGEMENT
 CONSERVATION AND COASTAL LANDS
 DIVISION OF BOATING AND OCEAN RECREATION
 DIVISION OF FORESTRY AND WILDLIFE
 DIVISION OF STATE PARKS
 ENGINEERING DIVISION
 OFFICE OF WATER RESOURCE MANAGEMENT
 OFFICE OF CONSERVATION AND COASTAL LANDS
 LAND-OAHU DISTRICT LAND OFFICE
 LAND-PLANNING AND DEVELOPMENT

HONOLULU ISLAND RELIANCE COMMISSION
 STATE PARKS



STATE OF HAWAII
 DEPARTMENT OF LAND AND NATURAL RESOURCES
 LAND DIVISION
 POST OFFICE BOX 621
 HONOLULU, HAWAII 96809
 May 14, 2004



PETER T. YOUNG
 CHAIRMAN
 BOARD OF LAND AND NATURAL RESOURCES
 COMMISSIONER OF NATURAL RESOURCES MANAGEMENT

DAN DAVIDSON
 DEPUTY DIRECTOR - LAND

ERNEST Y. K. LAU
 DEPUTY DIRECTOR - WATER

AGRICULTURE
 BOATING AND OCEAN RECREATION
 FORESTRY AND WILDLIFE
 COMMISSIONER OF NATURAL RESOURCES MANAGEMENT
 CONSERVATION AND COASTAL LANDS
 DIVISION OF BOATING AND OCEAN RECREATION
 DIVISION OF FORESTRY AND WILDLIFE
 DIVISION OF STATE PARKS
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 OFFICE OF CONSERVATION AND COASTAL LANDS
 LAND-OAHU DISTRICT LAND OFFICE
 LAND-PLANNING AND DEVELOPMENT

HONOLULU ISLAND RELIANCE COMMISSION
 STATE PARKS



STATE OF HAWAII
 DEPARTMENT OF LAND AND NATURAL RESOURCES
 LAND DIVISION
 POST OFFICE BOX 621
 HONOLULU, HAWAII 96809
 May 14, 2004



LD/NAV
 WAIPIOPOINT.CHT
 Suspension Date: 5/27/04

LD/NAV
 WAIPIOPOINT.CHT
 Suspension Date: 5/27/04

MEMORANDUM:

TO: XXX Division of Aquatic Resources
 XXX Division of Forestry & Wildlife
 XXX Division of State Parks
 XXX Engineering Division
 XXX Division of Boating and Ocean Recreation
 XXX Commission on Water Resource Management
 XXX Office of Conservation and Coastal Lands
 XXX Land-Oahu District Land Office
 XXX Land-Planning and Development

FROM: Dierdre S. Mamiya, Administrator
 Land Division

MEMORANDUM:

TO: XXX Division of Aquatic Resources
 XXX Division of Forestry & Wildlife
 XXX Division of State Parks
 XXX Engineering Division
 XXX Division of Boating and Ocean Recreation
 XXX Commission on Water Resource Management
 XXX Office of Conservation and Coastal Lands
 XXX Land-Oahu District Land Office
 XXX Land-Planning and Development

FROM: Dierdre S. Mamiya, Administrator
 Land Division

SUBJECT: Waipio Point Access Road Improvement Study, Waipahu,
 Island of Oahu, Hawaii
 City and County of Honolulu Project 2003223
 Consultant: Parson Brinckerhoff Quade & Douglas, Inc.
 Pre-Assessment Consultation

SUBJECT: Waipio Point Access Road Improvement Study, Waipahu,
 Island of Oahu, Hawaii
 City and County of Honolulu Project 2003223
 Consultant: Parson Brinckerhoff Quade & Douglas, Inc.
 Pre-Assessment Consultation

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 pertaining to the subject matter and submit your comment (if any)
 on Division letterhead signed and dated by the suspense date.

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 contact Nick Vaccaro at ext.: 7-0384. If this office does not
 receive your comments by the suspense date, we will assume there
 are no comments.

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 are no comments.

We have no comments.
 Comments attached.

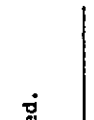
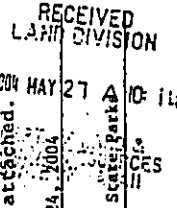
Signed: *[Signature]*
 Date: May 24, 2004

Name: Daniel S. Quinn, Administrator
 Division: State Parks

We have no comments.
 Comments attached.

Signed: *[Signature]*
 Date: MAY 19 2004

Name: MICHAEL G. BUCK, ADMINISTRATOR
 Division: DIVISION OF FORESTRY AND WILDLIFE





Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7084
Fax: 808-528-2268

May 12, 2004

Mr. Rodney Haraga
Director
Department of Transportation
State of Hawaii
869 Punchbowl Street
Honolulu, Hawaii 96813

Dear Mr. Haraga:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side. We have met with Mr. Patrick Tom and Mr. Ron Tsuzuki of your staff regarding the subject project.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waialae Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 608-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements
Over a century of
Engineering Excellence

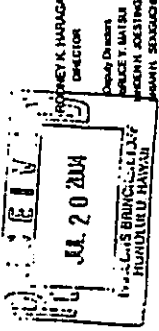
LEI
LIZALIKOLE
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

JUL 19 2004

HWY-PS
2.4725



REPLY REFER TO

Ms. Larissa Sato
Project Manager
Parsons Brinckerhoff Quade & Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Ms. Sato:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
Pre-Assessment Consultation

Thank you for contacting us. We have the following comments:

1. Proposed City improvements to Waipio Point Access Road should not affect planning for the Leeward Community College second access project.
2. Highways Division needs to review and approve plans for any work within the OR&L right-of-way.

Very truly yours,

Rodney K. Haraga
RODNEY K. HARAGA
Director of Transportation



Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7094
Fac: 808-528-2368

June 2, 2004

Mr. Glenn T. Kimura, Principal
Kimura International
Pan Am Building, #1610
Honolulu, Hawaii 96814

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Mr. Kimura:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalela Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

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Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by July 2, 2004.

If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Salo
Larissa Salo
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

Over a Century of
Engineering Excellence



Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7094
Fac: 808-528-2368

May 12, 2004

Mr. Clifford Togo
Director
Leeward Community College
96-045 Ala Ika
Pearl City, Hawaii 96782

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Mr. Togo:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalela Golf Course on the west side and a U.S. naval facility on the east side.

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If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Salo
Larissa Salo
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

Over a Century of
Engineering Excellence



May 12, 2004

Mr. Clyde Nienuo
Administrator
Office of Hawaiian Affairs
711 Kapiolani Boulevard, Suite 500
Honolulu, Hawaii 96813

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Mr. Nienuo:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is linked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waiani Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato

Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

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Engineering Excellence

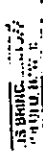
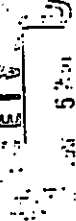
Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7034
Fax: 808-528-2268

PHONE (808) 594-1888



STATE OF HAWAII
OFFICE OF HAWAIIAN AFFAIRS
711 KAPOLANI BOULEVARD, SUITE 500
HONOLULU, HAWAII 96813

FAX (808) 594-1885



HRD04-1397

May 21, 2004

Larissa Sato, Project Manager
Parsons Brinckerhoff Quade & Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813

Subject: Pre-Assessment Consultation for the Waipio Point Access Road
Improvements Study, City and County of Honolulu Project: 2003223, Waipahu,
Hawaii, TMK: (1) 9-3-02; Parcel 1

Dear Ms. Sato:

Thank for your letter dated May 12, 2004 regarding the pre-assessment consultation for the Waipio Point Access Road Improvements Study, City and County of Honolulu Project: 2003223, located at Waipahu, Hawaii, TMK: (1) 9-3-02; Parcel 1. Your letter requests that OHA review and comment on the proposed project.

The early consultation letter requests input on the following issue: knowledge of any environmental or social issues associated with this project.

A Draft Environmental Assessment (DEA), if required by the accepting authority with jurisdiction over the proposed project area, must include a substantive cultural impact statement (CIS) based on consultation with the Native Hawaiian community, as required by Act 50, Session Laws of Hawaii 2000.

The CIS must identify and describe the cultural practices located within the potentially affected area; assess the impact on these practices; examine alternatives to the proposed action; and propose mitigation measures if needed.

If a DEA is not required, attempts should still be made to contact cultural practitioners in the project area.

It is also recommended that the project developers consult with Native Hawaiian practitioners (individuals and organizations) to determine the impact of the proposed project on cultural practices. As a consequence, despite the impacts to the project site and the area, in accordance with the Chapter 343, HRS and HAR §11-200-10, Contents of an environmental assessment, "the proposing agency or approving agency shall prepare any draft or final environmental assessment of each proposed action and determine whether the anticipated effects constitute a significant effect in the context of chapter 343, HRS, and §11-200-12", the project consultants Parsons Brinckerhoff Quade & Douglas, Inc. for the project developers (City and County of Honolulu, Department of Transportation Services) should consult with the following organization, who will advise your organization of the appropriate native Hawaiian community members or practitioners to contact regarding cultural practices (or other native Hawaiian issues) in the area:

- O'ahu Burial Council
c/o of the State Historic Preservation Division
Kakuhikewa Building
601 Kamokila Blvd., Suite 555
Kapolei, Hawaii, 96707

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Steps should also be taken to locate other Native Hawaiians and organizations in the project vicinity.

Please also provide a Tax Map Key(s) for the proposed project.

If you have questions or concerns please contact Matthew Myers, Policy Advocate at 594-1945 or mattmyer@ohia.org.

'O wau iho nō,



Clyde W. Nāmu'o
Administrator



BOARD OF WATER SUPPLY
 CITY AND COUNTY OF HONOLULU
 630 SOUTH BERETANIA STREET
 HONOLULU, HI 96813

June 14, 2004
 CLIFFORD S. JAMILE
 Manager and Chief Engineer
 DONNA FAY KUROSAKI
 Deputy Manager and Chief Engineer
 ROBERT K. HARADA, Esq.
 Attorney at Law
 LARRY A. STEED, Vice Chairman
 JOHN W. LINDO
 ERELY HARRIS, Mayor
 ERIC FLORES, Jr., Chairman
 JOHN A. STEED, Vice Chairman
 JOHN W. LINDO

Parsons
 Brinckerhoff
 Quade &
 Douglas, Inc.
 American Savings Bank Tower
 1001 Bishop Street, Suite 3000
 Honolulu, HI 96813
 808-531-7084
 Fax: 808-528-2368

May 12, 2004

Mr. Clifford Jamile
 Manager & Chief Engineer
 Board of Water Supply
 630 South Beretania Street
 Honolulu, Hawaii 96813

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
 City and County of Honolulu Project: 2003223
 Pre-Assessment Consultation

Dear Mr. Jamile:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waialae Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-565-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
 Larissa Sato
 Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

Over a Century of
 Engineering Excellence

Ms. Larissa Sato
 Parsons Brinckerhoff, Quade & Douglas, Inc.
 1001 Bishop Street, Suite 3000
 Honolulu, Hawaii 96813

Dear Ms. Sato:

Subject: Your Letter of May 12, 2004 on the Environmental Assessment for the Waipio Point Access Road Improvements

Thank you for the opportunity to comment on the proposed project.

The existing water system is presently adequate to accommodate the proposed improvements.

The availability of water will be confirmed when the building permit is approved. When water is made available, the applicant will be required to pay our Water System Facilities Charges for resource development, transmission and daily storage.

The proposed project is subject to Board of Water Supply Cross-Connection Control and Backflow Prevention requirements prior to the issuance of the Building Permit Application.

If you have any questions, please contact Joseph Kaakua at 748-5442.

Very truly yours,

Clifford S. Jamile
 Clifford S. Jamile
 Manager and Chief Engineer

Pure Water... our greatest asset - we'll do it right



Parsons
Brinckerhoff
Quade &
Douglas, Inc.

American Savings Bank Tower
1001 Bishop Street, Suite 2000
Honolulu, HI 96813
808-531-7094
Fax 808-526-2068

May 12, 2004

Mr. Timothy E. Steinberger
Director
Department of Design and Construction
City and County of Honolulu
650 South King Street, 11th Floor
Honolulu, Hawaii 96813

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Mr. Steinberger:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORGL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

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Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements
Over a Century of
Engineering Excellence

DEPARTMENT OF DESIGN AND CONSTRUCTION

CITY AND COUNTY OF HONOLULU

650 SOUTH KING STREET, 11TH FLOOR
HONOLULU, HAWAII 96813
Phone: (808) 527-4544 • Fax: (808) 527-4599
Web Site: www.cc.honolulu.gov



SECRETARY
MAYOR

TIMOTHY E. STEINBERGER, P.E.
DIRECTOR

62576

May 21, 2004

Ms. Larissa Sato, Project Manager
Parsons Brinckerhoff Quade & Douglas, Inc.
1001 Bishop Street, Suite 2000
Honolulu, Hawaii 96813

Dear Ms. Sato:

Subject: Waipio Point Access Road Improvements Study
Waipahu, Hawaii

The Department of Design and Construction (DDC) is in receipt of your letter dated May 12, 2004 regarding the pre-assessment consultation. The proposed improvements to the Waipio Peninsula Soccer Park would be a welcomed addition to all park users and the neighboring community.

DDC suggests that the lighting of the entire road be reviewed and perhaps upgraded to have a consistent lighting level throughout. The lighting of the road will not only provide safe driving conditions but also deter any loitering of people during the nighttime hours.

We look forward to reviewing the draft document once it is published.

Should there be any questions, please contact Dennis Kodama at 527-5611.

Very truly yours,

Timothy E. Steinberger

TIMOTHY E. STEINBERGER, P.E.
Director

TES:ln



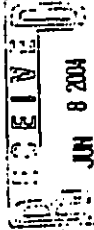
Parsons
Brinckerhoff
Quade &
Douglas, Inc.
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813
Phone: 808-531-7084
Fax: 808-528-2268

American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813
Phone: 808-531-7084
Fax: 808-528-2268

DEPARTMENT OF ENTERPRISE SERVICES
CITY AND COUNTY OF HONOLULU

777 WARD AVENUE, HONOLULU, HAWAII 96813-2116
PHONE: (808) 527-5600 • FAX: (808) 527-5609 • INTERNET: WWW.CITYANDCOUNTY.HI.GOV

GOLF COURSE DIVISION • HONOLULU ZOO • HEALS BLAISDELL CENTER AND WAIKIKI STABLES



JEREMY HARRIS
MAYOR

BARRY FUKUNAGA
DIRECTOR
LYNETTE CHAN
DEPUTY DIRECTOR

May 12, 2004

Mr. Barry Fukunaga
Director
Department of Enterprise Services
City & County of Honolulu
777 Ward Avenue
Honolulu, Hawaii 96814

Dear Mr. Fukunaga:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORSL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half a mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side. We have met with Mr. Wayne Lee and Mr. Glenn Kakutani of your staff regarding the subject project.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waianai Stream may have to be upgraded to current bridge standards.

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If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

Over a Century of
Engineering Excellence

Ms. Larissa Sato, Project Manager
Parsons Brinckerhoff Quade & Douglas, Inc.
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

June 1, 2004

Dear Ms. Sato:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223

In response to your Pre-Assessment Consultation Letter, dated

May 12, 2004 and discussion with members our Golf Division staff, we find no environmental or social issues originating from the Ted Makalena Golf Course that would impact upon this project.

The study's May 17, 2004 preferred alternative design has incorporated recommendations from my staff and we have issued a letter to the Director, Department of Transportation Services indicating our willingness to relinquishing the small parcels of Ted Makalena Golf Course that would be required for the desired routing identified in the study.

Should you have any questions, please contact Wayne Lee, Assistant Golf Course System Administrator at 733-7382.

Sincerely,

Barry Fukunaga
BARRY FUKUNAGA
Director

BF:cz (6/2/04)
Re: Golf Course Division Letter to Sato-Waipio Pt. Access Rd EA 6/2/04
Attachment



May 12, 2004

Mr. Larry Leopardi
Director
Department of Facility Maintenance
City and County of Honolulu
1000 Ulukouia Street, Suite 215
Kapolei, Hawaii 96707

Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7024
Fac: 808-538-2368

Dear Mr. Leopardi:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORAL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalana Golf Course on the west side and a U.S. naval facility on the east side.

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If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements
Over a Century of
Engineering Excellence

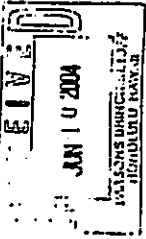
DEPARTMENT OF FACILITY MAINTENANCE
CITY AND COUNTY OF HONOLULU
1000 ULUKOUIA STREET, SUITE 215, KAPOLEI, HAWAII 96707
TELEPHONE: (808) 591-7024 FAX: (808) 591-2367



June 9, 2004

JERRY HARRIS
SALES

LARRY J. LEOPARDI, P.E.
DIRECTOR AND CHIEF ENGINEER
ALVIN K. AU
CITY DIRECTOR
IN REPLY REFER TO:
DRM 04 - 548



Ms. Larissa Sato
Project Manager
Parsons Brinckerhoff Quade & Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Ms. Sato:

Subject: Waipio Point Access Road Improvements Study,
Waipahu, Hawaii, City and County of Honolulu Project
2003223 Pre-Assessment Consultation

Thank you for the opportunity to provide comments and recommendations regarding improvements to Waipio Point Access Road to be addressed in the subject study.

We request that any improvements to the roadway be to City standards and include a drainage system and street lighting. Also, the pavement structure should be designed to support the truck and bus traffic that will be using the roadway. Our experience has shown that a standard 2-12" asphalt surface over 6" of aggregate base for this type of roadway is not adequate and a full depth asphalt pavement should be considered.

Furthermore, we have received concerns regarding the condition and flow within the drainage ditch abutting the roadway. There are low areas along the ditch that pond water and create a stagnant condition. Therefore, we request the study include a survey of the ditch to determine corrective work that should be done to improve storm water flow.

Should you have any questions, please call Mr. Charles Pignataro of our Division of Road Maintenance, at 527-6282.

Very truly yours,

Larry Leopardi
LARRY LEOPARDI, P.E.
Director and Chief Engineer



Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7094
Fax: 808-528-2268

May 12, 2004

Mr. William D. Balfour, Jr.
Director
Department of Parks and Recreation
City & County of Honolulu
650 S. King Street, 10th Floor
Honolulu, Hawaii 96813

Dear Mr. Balfour:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORNL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is limited by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side. We have met with Mr. Brian Carew and Mr. Gibbon Myamoto of your staff regarding the subject project.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

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Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-568-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

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DEPARTMENT OF PARKS AND RECREATION

CITY AND COUNTY OF HONOLULU

1000 ULUKOUHA STREET, SUITE 309 • KAPOLEI, HAWAII 96707
Phone: (808) 662-5665 • FAX: 662-5131 • Internet: www.ci.honolulu.hi.us



JEREMY HARRIS
MAYOR

June 21, 2004

Ms. Larissa Sato, Project Manager
Parsons Brinckerhoff Quade & Douglas, Inc.
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Ms. Sato:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project 2003223
Pre-Assessment Consultation

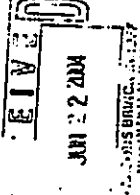
This is in reference to your letter of May 12, 2004, regarding above subject. We know of no environmental or social issues related to this project that would have an impact on an environmental assessment. However, we would like to review the proposed landscape design prior to installation.

Should you have any further questions, please contact Mr. Brian Carew, District V Manager, at 676-8777.

Sincerely,

William D. Balfour, Jr.
WILLIAM D. BALFOUR, JR.
Director

WDB:as
(4/27/04)



WILLIAM D. BALFOUR, JR.
DIRECTOR

EDWARD T. "SKUPPY" DIAZ
DEPUTY DIRECTOR



Parsons
Brinckerhoff
Quade &
Douglas, Inc.

American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7094
Fac: 808-528-2368

May 12, 2004

Mr. Eric Crispin
Director
Department of Planning and Permitting
City and County of Honolulu
650 South King Street, 7th Floor
Honolulu, Hawaii 96813

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Mr. Crispin:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

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If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato

Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements
Over a Century of
Engineering Excellence



Parsons
Brinckerhoff
Quade &
Douglas, Inc.

American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7094
Fac: 808-528-2368

May 12, 2004

Mr. Attilio Leonardi
Fire Chief
Honolulu Fire Department
City and County of Honolulu
3375 Koopaka Street, Suite H425
Honolulu, Hawaii 96819

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Mr. Leonardi:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

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Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

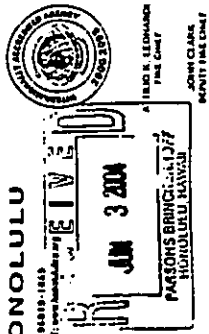
Larissa Sato

Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements
Over a Century of
Engineering Excellence

FIRE DEPARTMENT
CITY AND COUNTY OF HONOLULU

3375 WAIKOLA AVENUE, SUITE 1000, HONOLULU, HAWAII 96819-1643
TELEPHONE: (808) 521-7777 • FAX: (808) 521-7770 • INTERNET: WWW.HONOLULU.FI.HAWAII.GOV



JERRY HARRIS
BATE

May 28, 2004

Ms. Larissa Sato, Project Manager
Parsons Brinckerhoff Quade & Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Ms. Sato:

Subject: Pre-Assessment Consultation
Waipio Point Access Road Improvements Study
City and County of Honolulu Project: 2003223
Waipahu, Oahu, Hawaii

We received your letter dated May 12, 2004, requesting our comments on the above-mentioned project.

The Honolulu Fire Department requires that the following be compiled with for the duration of the project:

1. Maintain fire apparatus access throughout the construction site.
2. Notify the Fire Communication Center at 523-4411 regarding any interruption of the existing fire hydrant system.

Should you have any questions, please call Battalion Chief Lloyd Rogers of our Fire Prevention Bureau at 831-7778.

Sincerely,

Attilio K. Leonard
ATTILIO K. LEONARDI
Fire Chief

AKL/SK:bh



May 12, 2004

Mr. Lee Donohue
Police Chief
Police Department
City and County of Honolulu
801 South Beretania Street
Honolulu, Hawaii 96813

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Chief Donohue:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the Off-Road Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farington Highway (see enclosed map). It is flanked by the Ted Makalela Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalela Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waiani Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

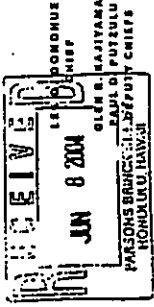
Larissa Sato
Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements
Over a Century of
Engineering Excellence

POLICE DEPARTMENT
CITY AND COUNTY OF HONOLULU

801 SOUTH BERETANIA STREET
HONOLULU, HAWAII 96813 - AREA CODE (808) 929-3111
<http://www.honolulu.gov>
www.co.honolulu.hi.us

JEREMY HARRIS
MAYOR



OUR REFERENCE CS-KP

June 3, 2004

Ms. Larissa Sato, Project Manager
Parsons Brinckerhoff Quade & Douglas, Inc.
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Ms. Sato:

Thank you for the opportunity to review and comment on the Pre-Assessment Consultation for the Waipio Point Access Road Improvements Study.

This proposal should have minimal impact on the services provided by the Honolulu Police Department. However, we would like to reserve our comments until more details are known.

If there are any questions, please call Captain Randal Macadangdang of District 3 at 455-9055 or Ms. Carol Sodeliani of the Support Services Bureau at 529-3658.

Sincerely,

LEE D. DONOHUE
Chief of Police

By *Karl Godsey*
KARL GODSEY
Assistant Chief of Police
Support Services Bureau

Serving and Protecting with Aloha



Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7094
Fax: 808-528-2368

May 12, 2004

Mr. Doug Alton
Acting Administrator
Oahu Civil Defense Agency
City and County of Honolulu
650 South King Street
Honolulu, Hawaii 96813

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Mr. Alton:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

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The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Waiwai Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2246.

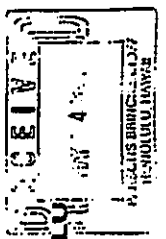
Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements
Over a Century of
Engineering Excellence

OAHU CIVIL DEFENSE AGENCY
CITY AND COUNTY OF HONOLULU
850 SOUTH KING STREET • HONOLULU, HAWAII 96813
PHONE: (808) 523-4121 • FAX: (808) 524-3439



JEREMY HARRIS
OFFICE

R. DOUG ATCH
ACTING ADMINISTRATOR

May 19, 2004

Miss Larissa Sato
Parsons Brinckerhoff Quade & Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Miss Sato:

RE: WAIPIO POINT ACCESS ROAD IMPROVEMENTS STUDY,
WAIPIAHU, HAWAII, CITY AND COUNTY OF HONOLULU
PROJECT: 2003223 - PRE ASSESSMENT CONSULTATION

Thank you for the opportunity to review the above mentioned project. The Oahu Civil Defense Agency does not have any comments at this time.

Sincerely,


R. DOUG ATCH
Acting Administrator

RDA:ms

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0 1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 60 61 62 63 64 65 66 67 68 69 70 71 72 73 74 75 76 77 78 79 80 81 82 83 84 85 86 87 88 89 90 91 92 93 94 95 96 97 98 99 100



May 12, 2004

Mr. Cal Kawamoto
Senator, 18th District
State Capitol 415 S. Beretania St., Rm. 230
Honolulu, Hawaii 96813

Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7034
Fax: 808-538-2388

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 20033223
Pre-Assessment Consultation

Dear Mr. Kawamoto:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORAL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

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If you have any questions, please call me at 808-538-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Latissa Salo
Latissa Salo
Project Manager

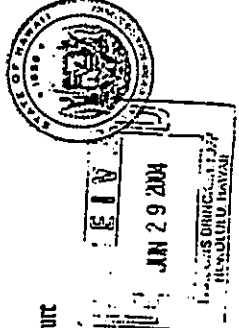
Enclosure: Project vicinity map showing the location of the proposed roadway improvements

Over a Century of
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NORANI BUCKA
PRESIDENT
DOYER BERGACCO DAN
VICE PRESIDENT
COLLEEN HANAHAN
MAJORITY LEADER
CAL KAWAMOTO
MAJORITY FLOOR LEADER
SEAN S. LENTZ
MAJORITY CLERK LEADER
FRED CLEMMENS
MINORITY LEADER
BOB JOHNS
MINORITY FLOOR LEADER
PAUL WALKER
MINORITY POLICY LEADER

FIRST DISTRICT
LAWRENCE A. MOORE
SECOND DISTRICT
KIMBERLY A. HOLLIBAUGH
THIRD DISTRICT
PAUL WALLEN
FOURTH DISTRICT
SHARON HAYES
FIFTH DISTRICT
TERRY W. BISHOP
SIXTH DISTRICT
TERRY W. BISHOP
SEVENTH DISTRICT
DAVID I. WATSON
EIGHTH DISTRICT
DAVID I. WATSON
NINTH DISTRICT
DAVID I. WATSON
TENTH DISTRICT
DAVID I. WATSON
ELEVENTH DISTRICT
DAVID I. WATSON
TWELFTH DISTRICT
DAVID I. WATSON
THIRTEENTH DISTRICT
DAVID I. WATSON
FOURTEENTH DISTRICT
DAVID I. WATSON
FIFTEENTH DISTRICT
DAVID I. WATSON
SIXTEENTH DISTRICT
DAVID I. WATSON
SEVENTEENTH DISTRICT
DAVID I. WATSON
EIGHTEENTH DISTRICT
CAL KAWAMOTO
NINETEENTH DISTRICT
DAVID I. WATSON
TWENTIETH DISTRICT
WILLIE C. LIPINO
TWENTY-FIRST DISTRICT
COLLEEN HANAHAN
TWENTY-SECOND DISTRICT
ROMAN MATA
TWENTY-THIRD DISTRICT
WILLIE C. LIPINO
TWENTY-FOURTH DISTRICT
COLLEEN HANAHAN
TWENTY-FIFTH DISTRICT
WILLIE C. LIPINO
CHIEF CLERK
PAUL I. BERGACCO

The Senate
The Twenty-Second Legislature
of the
State of Hawaii
STATE CAPITOL
HONOLULU, HAWAII 96813
June 28, 2004



Miss Latissa Salo
Parsons Brinckerhoff Quade & Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Miss Salo,

I am writing this letter to express my support for the proposed project to improve the Waipio Point Access Road. This project will not only improve the access to the Waipio Soccer Park but also provide an infrastructure upgrade that this stretch of road so direly needed. The project I feel will also be a compliment to the planned construction of a second access road to the Leeward Community College campus. These two projects when completed will provide the businesses in the Waipahu district with a much-needed economic boost by providing for increased and better traffic flow within the surrounding Waipahu community.

I look forward to the completion of the final study and start of the project. If you need any assistance form my staff or myself please feel free to call my office at 588-6970.

Sincerely,
Cal Kawamoto
Senator Cal Kawamoto, State Senator 18th District



Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7084
Fax: 808-528-2368

May 12, 2004

Ms. Tulsī Gabbard Tamayo
Representative, 42nd District
State Capitol, 415 S. Barretania St., Rm. 313
Honolulu, Hawaii 96813

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Ms. Tamayo:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

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If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Lanissa Sato
Lanissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

Over a Century of
Engineering Excellence



Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7084
Fax: 808-528-2368

May 12, 2004

Mr. Nestor Garcia
City Council Member
Transp. Comm. Chair
530 South King Street, Rm. 202
Honolulu, Hawaii 96813

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Mr. Garcia:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

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Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Lanissa Sato
Lanissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

Over a Century of
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Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7094
Fax: 808-528-2368



May 12, 2004

Mr. T. Michael May, President and CEO
Hawaiian Electric Company, Inc.
P.O. Box 2750
Honolulu, Hawaii 96803

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Mr. May:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORAL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalea Golf Course on the west side and a U.S. naval facility on the east side.

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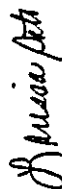
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Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.


Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

Over a Century of
Engineering Excellence



May 12, 2004

Mr. Keith Yoshino
Verizon Hawaii
1177 Bishop Street
Honolulu, Hawaii 96813

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Mr. Yoshino:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORAL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalea Golf Course on the west side and a U.S. naval facility on the east side.

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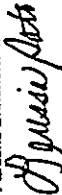
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Sincerely yours,

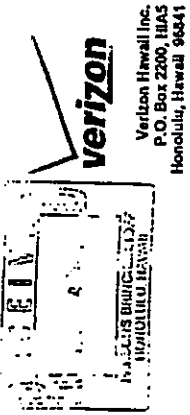
Parsons Brinckerhoff Quade & Douglas, Inc.


Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

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Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7094
Fax: 808-528-2368



May 21, 2004

Ms. Larissa Sato
Parsons Brinckerhoff Quade & Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street
Suite 3000
Honolulu, HI 96813

Dear Ms. Sato:


Subject: **WAIPIO POINT ACCESS ROAD IMPROVEMENTS STUDY, WAIPAHU, HAWAII
CITY AND COUNTY OF HONOLULU PROJECT: 2003223
PRE-ASSESSMENT CONSULTATION**

Thank you for the opportunity to comment on the Pre-Assessment of the Waipio Point Access Road Improvements Study in Waipahu. We are not aware of any environmental or social issues that would be associated with this project.

According to our records, Verizon Hawaii has both aerial and underground telecommunication facilities within the proposed project limits that may be affected by the Waipio Point Access Road Improvement Project. Please continue to work with us to ensure that the appropriate action is taken to relocate our facilities if they are affected. We will require 2 sets of drawings for our review and comment of this project.

Please contact Clay Tang at 548-5381 if you have any questions or require further assistance with this project.

Sincerely,


Stacy Shishido
Section Manager
Network Engineering & Planning
CAF Planning - Hawaii

CT:55

c: J. Lee HIABY3



Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
608.531.7054
Fax: 808-528-2268

May 12, 2004

Mr. Robert Yatchmenoff
President
Hawaiian Railway Society
P.O. Box 60369
Ewa Beach, Hawaii 96706

Subject: **Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation**

Dear Mr. Yatchmenoff:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

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If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.


Larissa Sato
Project Manager

Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

Over a Century of
Engineering Excellence



Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7054
Fax: 808-528-2268

May 12, 2004

Mr. Cal Kawamoto
President
Waipahu Business Association
State Capitol, 415 S. Beretania St., Rm. 230
Honolulu, Hawaii 96813

Subject: Waipaho Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Mr. Kawamoto:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipaho Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipaho Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipaho Peninsula Soccer Park. The Navy owns the affected section of Waipaho Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipaho Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipaho Point Access Road Bridge No. 2 over Wallani Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato

Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

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Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7054
Fax: 808-528-2268

May 12, 2004

Mr. Andy Anderson
President
Waipahu 2000 Community Council
c/o 91-2301 Fort Weaver Road
Ewa Beach, Hawaii 96706

Subject: Waipaho Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Pre-Assessment Consultation

Dear Mr. Anderson:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipaho Point Access Road in Waipahu, Oahu. The project limits are from the OR&L Bicycle/Pedestrian Path to the entrance of the Waipaho Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipaho Peninsula Soccer Park. The Navy owns the affected section of Waipaho Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipaho Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipaho Point Access Road Bridge No. 2 over Wallani Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2246.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato

Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

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1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7694
Fax 808-528-2368

May 12, 2004

Mr. Andy Anderson
Chair
Waipahu Neighborhood Board No. 22
94-114 Hulaha Street
Waipahu, Hawaii 96797

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 20030223
Pre-Assessment Consultation

Dear Mr. Anderson:

The City and County of Honolulu Department of Transportation Services (City) is proposing to improve the existing Waipio Point Access Road in Waipahu, Oahu. The project limits are from the ORNL Bicycle/Pedestrian Path to the entrance of the Waipio Peninsula Soccer Park, a half-a-mile south of Farrington Highway (see enclosed map). It is flanked by the Ted Makalena Golf Course on the west side and a U.S. naval facility on the east side.

The City entered into an agreement with the Navy, to use the road for access to its recently completed Waipio Peninsula Soccer Park. The Navy owns the affected section of Waipio Point Access Road. As part of this agreement, the City is proposing to provide a paved parking lot along the road for workers of the naval facility, and to improve roadway drainage.

The City also wishes to use this opportunity to provide bicycle lanes and sidewalks between the existing bicycle path and Waipio Peninsula Soccer Park, and to improve the visual quality of the area by providing landscaping along the roadway. Right-of-way from Ted Makalena Golf Course may be required to accommodate these improvements, and the Waipio Point Access Road Bridge No. 2 over Wallant Stream may have to be upgraded to current bridge standards.

Parsons Brinckerhoff Quade & Douglas, Inc. was contracted to assist the City in preparing a State Environmental Assessment (EA) for this project. If you have knowledge of any environmental or social issues associated with this project, we would very much appreciate this information. Also, please let us know if you have any concerns regarding this project so we can address them in the EA. Please provide your comments by June 14, 2004.

If you have any questions, please call me at 808-566-2248.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Salo

Larissa Salo
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

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APPENDIX D ■

REGULATORY CORRESPONDENCE

WAIPIO POINT ACCESS ROAD Improvements

FINAL ENVIRONMENTAL ASSESSMENT



May 17, 2004

Ms. Gina Shultz
Acting Field Supervisor
Pacific Islands Office
U.S. Fish and Wildlife Service
300 Ala Moana Boulevard, Room 3-122
Box 50088
Honolulu, Hawaii 96850

Dear Ms. Shultz:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
Request for species list pursuant to Section 7 of the Endangered Species Act

The purpose of this letter is to request a species list from the U.S. Fish and Wildlife Service (Service) pursuant to Section 7 of the Endangered Species Act (ESA) for the subject project. The project site is the existing Waipio Point Access Road from the ORNL Bicycle/Pedestrian Path to the entrance to Waipio Peninsula Soccer Park. The subject portion of Waipio Point Access Road is flanked by the Ted Makalena Golf Course on the west side and the NAVSEA Inactive Ships On-Site Maintenance Facility on the east side. The north project limits starts 0.5 mile south of Farrington Highway (see enclosed map). The project includes the following elements:

- Contiguous bicycle lanes and sidewalks between the ORNL Bicycle/Pedestrian Path and the entrance of Waipio Peninsula Soccer Park;
- A paved parking lot for NAVSEA Inactive Ships On-Site Maintenance Office personnel adjacent to Waipio Point Access Road;
- Improved roadway drainage;
- Upgraded Waipio Point Access Road Bridge No. 2; and
- Roadway landscaping.

If you have questions, please do not hesitate to contact me at (808) 566-2246 or e-mail at sato@pbworld.com.

Sincerely,

Parsons Brinckerhoff Quade & Douglas, Inc.

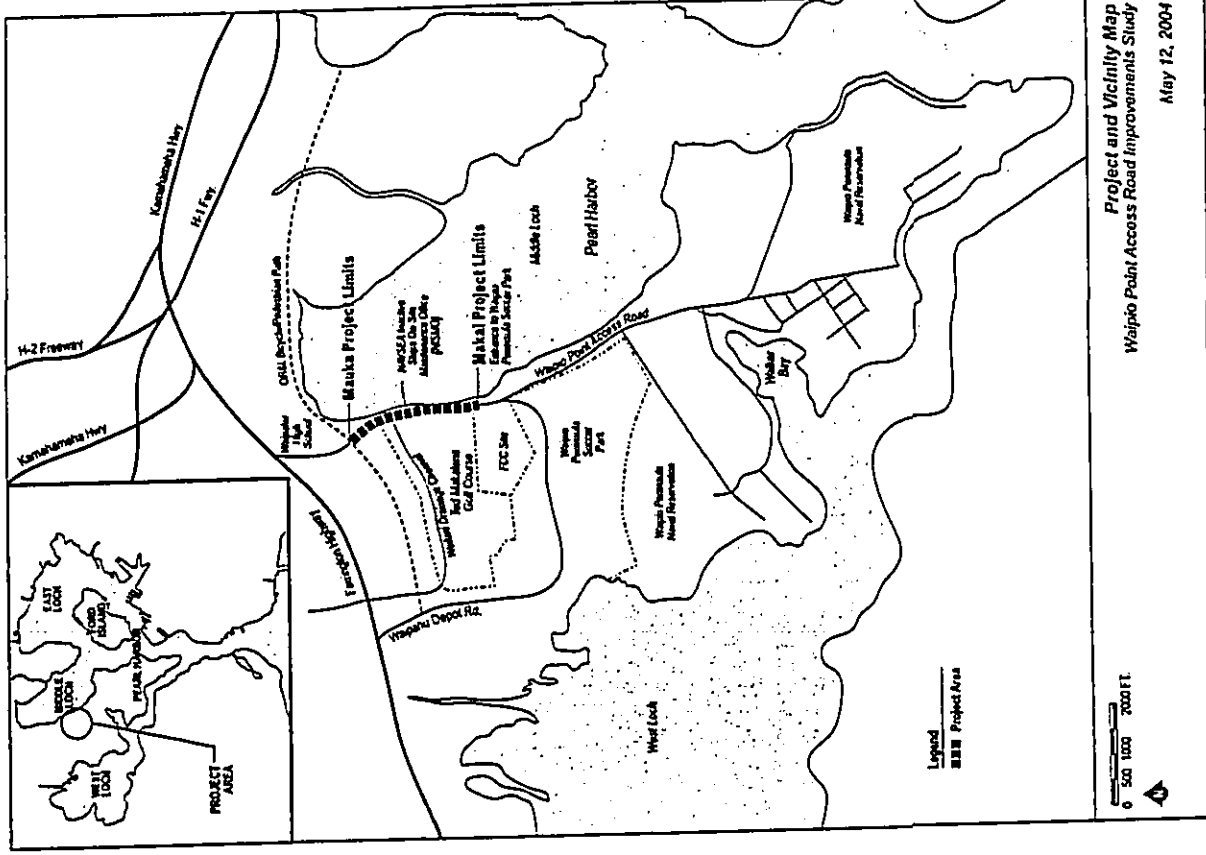
Garice Pats

Larissa Sato
Project Manager

Enclosure: Project vicinity map showing the location of the proposed roadway improvements

cc: Mr. Paul Conroy, Department of Land and Natural Resources, Division of Forestry and Wildlife
Mr. Pat Phung, Federal Highways Administration, Hawaii Division
Mr. Brian Suzuki, City and County of Honolulu, Department of Transportation Services

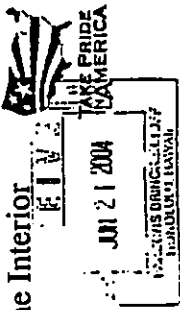
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United States Department of the Interior

FISH AND WILDLIFE SERVICE
Pacific Islands Fish and Wildlife Office
300 Ala Moana Boulevard, Room 3-122
Box 50088
Honolulu, Hawaii 96850



In Reply Refer to:
1-2-2004-SP-172

Ms. Larissa Sato
Parsons Brinckerhoff Quade & Douglas, Inc.
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Ms. Sato:

Thank you for your May 17, 2004, letter in which you request the U. S. Fish and Wildlife Service provide a species list for the proposed Waipio Point Access Road Improvements Study project, in the district of Waipahu, Oahu, Hawaii. Your letter was received in this office on May 21, 2004. The proposed project site is the existing Waipio Point Access Road from the OR&L Bicycle/Pedestrian Path to the entrance to the Waipio Peninsula Soccer Park. The project proposes to construct bicycle lanes, sidewalks and a paved parking lot; improve roadway drainage; upgrade a bridge; and landscape the roadway.

We reviewed the information you provided and pertinent information in our files, including maps prepared by the Hawaii Natural Heritage Program. To the best of our knowledge, no federally listed endangered, threatened, or proposed threatened or endangered species, or proposed or designated critical habitat occur on the proposed project site.

We appreciate your efforts to conserve endangered species. If you have any questions, please contact Marigold Zoll, Botanist (phone: 808/792-9400; fax: 808/792-9380).

Sincerely,

Nicole Alt
Acting Field Supervisor



Parsons Brinckerhoff
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813

April 12, 2004

Shannon Fitzgerald
EPA Groundwater Office
MS WTR-9
75 Hawthorne St.
San Francisco, CA 94105

RE: Waipio Point Access Road, Southern Oahu Basal Aquifer, Section 1424 (c) Review

Dear Ms Fitzgerald:

This letter report has been prepared to meet the coordination requirements of the Section 1424 (c) Review (Safe Drinking Water Act) for the Waipio Point Access Road project, in accordance with the 1984 Sole Source Aquifer Memorandum of Understanding between FIWA and EPA. This information is intended to provide EPA the necessary information to determine the project's impact on the quality of the groundwater in the Southern Oahu Basal Aquifer (SOBA), a sole source aquifer which was designated in November 1987.

The project involves improving the Waipio Point Access Road, between the OR&L Bicycle/Pedestrian Path and the entrance to the Waipio Peninsula Soccer Park, to meet the conditions of the Grant of Easement (December 2, 1999) between the Navy and the City and County of Honolulu (Figure 1).

Although there are several alternatives, the "worst-case" alternative involving the most paving and a box culvert is discussed in this letter and shown in Figure 2. The alternative consists of the following elements:

- eleven-foot wide travel lanes;
- six-foot wide bicycle lanes in both the makai- and mauka-bound directions from the OR&L Bicycle/Pedestrian Path to the entrance of the soccer park;
- a five-foot wide pedestrian walkway on the golf course side of the roadway from the OR&L Bicycle/Pedestrian Path to the entrance of the golf course;
- a paved parking lot with diagonal parking;
- a retaining wall; and
- a box culvert for drainage.

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1001 Bishop Street, Suite 3000
Honolulu, HI 96813

The Waipio Point Access Road is an existing two-lane roadway that originates at Farrington Highway and traverses makai toward the Waipio Peninsula Soccer Park. The Access Road is currently the only public access to the Waipio Peninsula Soccer Park. There are limited pedestrian facilities and no bicycle facilities along Waipio Point Access Road.

The road is underlain directly by coastal and alluvial sediments dominated at the surface by an ancient fishpond, Loko Eo (Ancient Hawaiian Fishponds of Pearl Harbor: Archeological Studies on U.S. Navy Lands, Hawaii, International Archeological Research Institute, Inc., July 2000).

The SOBDA is under artesian pressure in the porous basalt below the caprock, a wedge of alluvial sediments and limestone. The caprock layer thins with distance from the shoreline, and at varying distances inland, the caprock layer ends, and the basalt layer, containing the SOBDA, underlies surficial materials. Water quality in the SOBDA is excellent and the SOBDA is the primary source of drinking water on Oahu. Water in the caprock aquifers is too saline to be potable.

The SOBDA in the project area is in the Pearl Harbor Aquifer Sector and the Waipahu Aquifer System. Based on Hawaii status codes to protect drinking water, it is rated as a currently used, ecologically important, low salinity, irreplaceable and vulnerable to contamination (Aquifer Identification and Classification for Oahu: Groundwater Protection Strategy for Hawaii, John Mink and L. Stephen Lau, 1990).

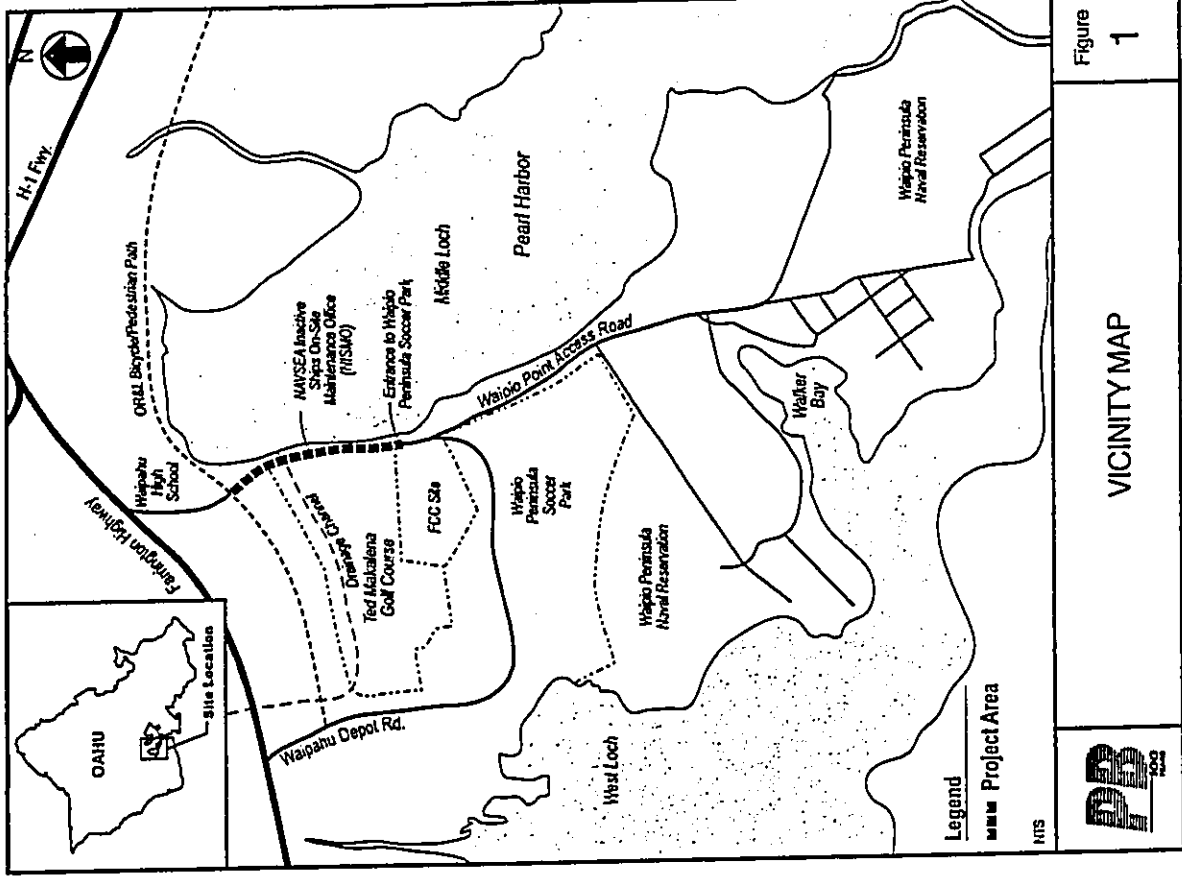
The project is located on recent alluvium underlain by caprock. The project should not have an impact on the quality of water in the SOBDA. There would be an increase in paved area for parking and the drainage swale would be lined as a box culvert. These changes would slightly reduce the area available for rainfall infiltration into the surficial sediments overlying the SOBDA. No increases in traffic, parking or vehicle use in the area would result from this project. Only facility upgrades, such as a box culvert, paved parking and a designated pedestrian path and bikeway, to satisfy the Easement Grant are proposed.

Please contact Larissa Sato at 808-531-7094 with any questions about the project. We would appreciate an approval letter to include in our Draft EA.

Sincerely,

Jan Reichelderfer
Geologist/Lead Environmental Planner

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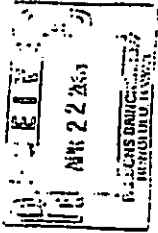


VICINITY MAP
Figure 1

Parsons
Brinckerhoff
Waiopu Point Access Road
Section 1424 (4) IAWA
April 2004



UNITED STATES ENVIRONMENTAL PROTECTION AGENCY
REGION IX
75 Hawthorne Street
San Francisco, CA 94105-3901



Memorandum



April 19, 2004

Jan Reichelderfer
Geologist/Lead Environmental Planner
Parsons Brinckerhoff
1001 Bishop Street, Suite 3000
Honolulu, HI 96813

Re: Waipio Point Access Road, Southern Oahu Basal Aquifer (SOBA),
Safe Drinking Water Act Amendments §1424(c) Review

Mail Code: WTR-9

MEETING SUMMARY

To: Fik
From: Larissa Salo
Date: March 17, 2004
Subject: Meeting with Ms. Sarah Collins and Ms. Muffett Jourdane of the State Historic Preservation Division regarding the Waipio Point Access Road Improvements Study

Meeting Date, Time, Location: March 17, 2004 at 8:45 a.m. at the State Historic Preservation Office

Meeting Attendees: Ms. Sarah Collins, SHPD
Ms. Muffett Jourdane, SHPD
Ms. Larissa Salo, PB

A meeting with Ms. Sarah Collins and Ms. Muffett Jourdane of the State Historic Preservation Division was held to discuss the historic fishpond "Loko Eo". Larissa indicated that roadway improvement Alternative B requires that the existing Ted Makalena Golf Course fence be relocated up to approximately 20 feet Ewa and the construction of a box culvert and retaining wall to accommodate a paved parking lot. Construction would require excavation of up to 6 feet below existing golf course grades.

Sarah and Muffett referred to the "Ancient Hawaiian Fishponds of Peaia Harbor. Archaeological Studies on U.S. Navy Lands, Hawaii" report dated July 2000. The study indicates that Ted Makalena Golf Course is built on top of this fish pond. As part of the study, exploratory cores were recovered. Analysis of the core reflects that intact sediments begin at a depth of approximately 7.17 meters (21 feet) below the surface. Therefore, Sarah indicated that unless the proposed construction requires an excavation as deep or deeper than 21 feet, the fishpond will not be impacted. She indicated that "No historic properties affected" would be appropriated for this project.

Sarah suggested I contact Eric West or Emily Donaldson of Annie Griffin's (Navy) staff for a list of contacts to consult with regarding this area.

Dear Ms. Reichelderfer:

Thank you for submitting the Waipio Point Access Road project for review under the Sole Source Aquifer program. Based on the information that was provided, it appears highly unlikely that there will be significant impacts to the SOBA from this project.

If you have any questions, please contact me at (415) 972-3525.

Sincerely,

Shannon FitzGerald
Shannon FitzGerald
Environmental Scientist
Ground Water Office



Parsons
Brinckerhoff
Quade &
Douglas, Inc.
American Springs Park Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7094
FAC 808-526-2369

May 12, 2004

Mr. Jonathan Scheuer
Policy Analyst
Office of Hawaiian Affairs
711 Kapiolani Boulevard, Suite 500
Honolulu, Hawaii 96813

Dear Mr. Scheuer:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
National Historic Preservation Act Section 106 Consultation

We are assisting the City and County of Honolulu Department of Transportation Services and the U.S. Department of Transportation Federal Highway Administration in a project to improve Waipio Point Access Road in Waipahu, Oahu. The project site is about a half-a-mile north of Farrington Highway (see enclosed map). The purpose of this letter is to request your consultation pursuant to Section 106 of the National Historic Preservation Act.

The project includes the following elements:

- Contiguous bicycle lanes and sidewalks between the CR&L Bicycle/Pedestrian Path and the entrance of Waipio Peninsula Soccer Park;
- A paved parking lot for NAVSEA Inactive Ships On-Site Maintenance Office personnel adjacent to Waipio Point Access Road;
- Improved roadway drainage;
- Upgraded Waipio Point Access Road Bridge No. 2; and
- Roadway landscaping.

Based on information provided by the Navy, we are aware of a large buried fishpond, "Loko Eo", that underlies much of the project and surrounding area, including the Ted Makalena Golf Course and Naval facilities along the shoreline (see enclosed map). We are not aware of know of any other historic property within our project limits.

We have consulted with Dr. Sara Collins of the State Historic Preservation Division, who informed us that archaeological studies done by the Navy indicate that intact sediments of the fishpond occur approximately 21 feet below the existing surface (Ancient Hawaiian Fishponds of Pearl Harbor: Archaeological Studies of U.S. Navy Lands, Hawaii, July 2000). The proposed project requires excavation of up to 13 feet below the surface. Therefore, Dr. Collins believes that a "no historic properties affected" determination per Section 106 would be appropriate for the project because the required excavation would be far less than the approximately 21 feet where one could expect to uncover fishpond sediment. At this time, we are inclined to agree with Dr. Collins' assessment.

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Mr. Jonathan Scheuer
May 12, 2004
Page 2

Please let me know if you have any comments or concerns regarding our current assessment of the potential impacts to Loko Eo. In addition to letting me know if you know of any other historic property that could be affected by the project. We would appreciate receiving your comments or other information by June 14, 2004.

If you have questions or require additional information, please do not hesitate to call me at (808) 566-2246.

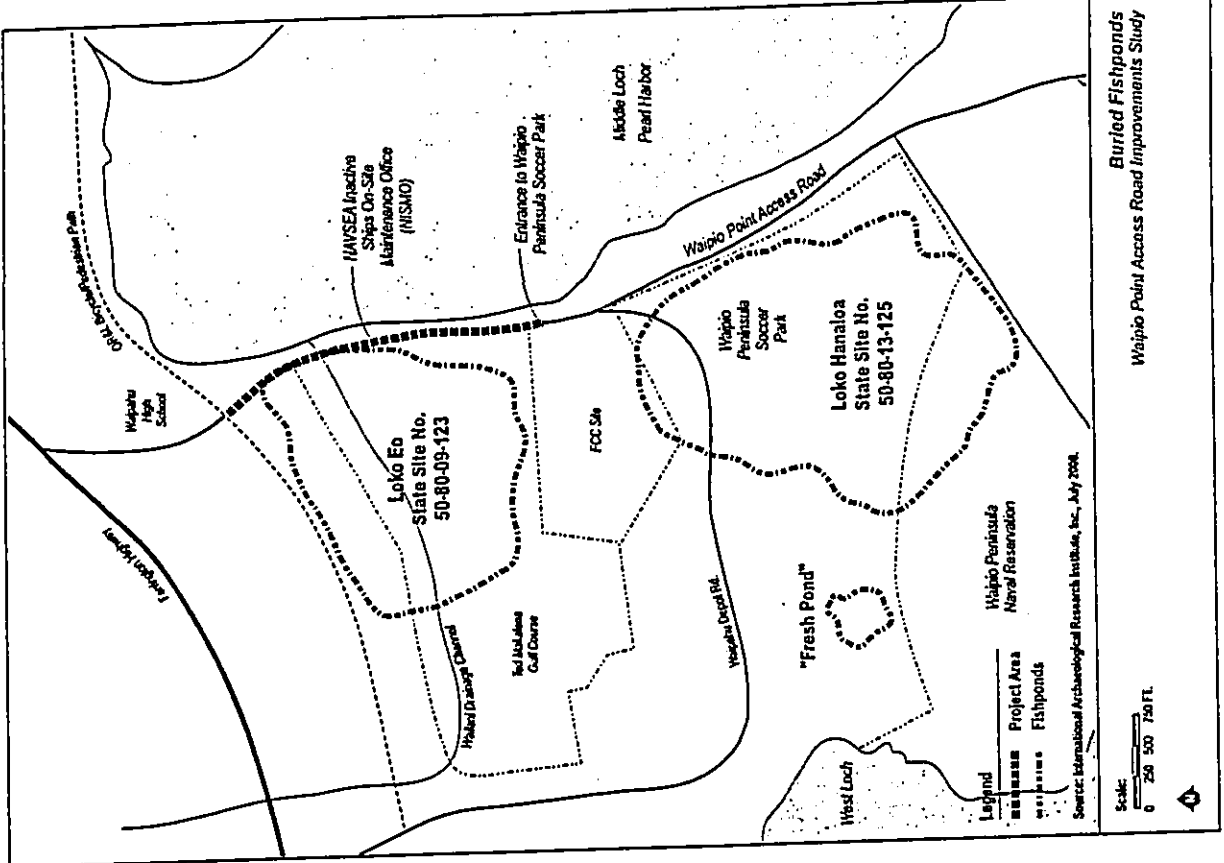
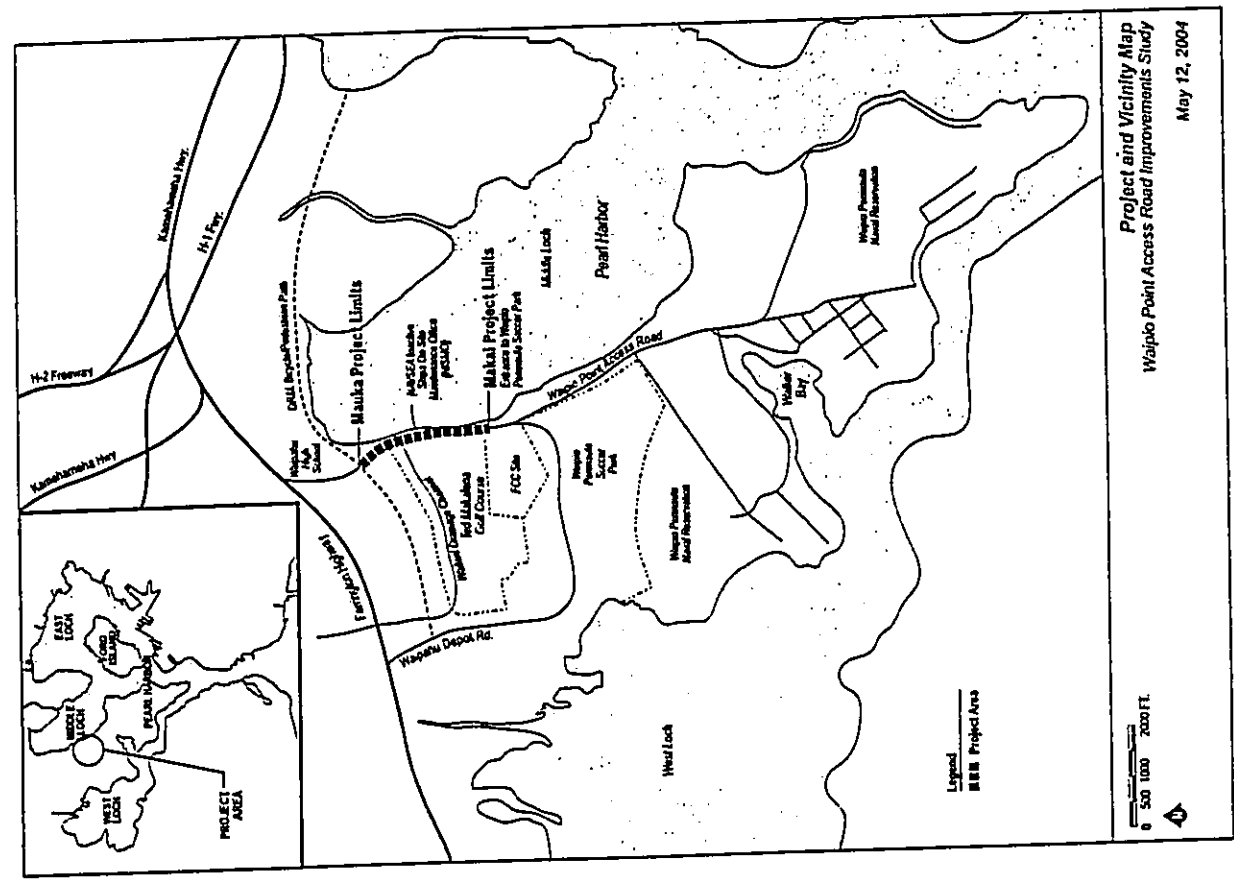
Sincerely yours,
Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato
Larissa Sato
Project Manager

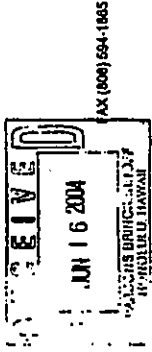
Enclosure: Project vicinity map and Buried Fishpond Map

cc: Mr. Brian Suzuki, City and County of Honolulu, Department of Transportation Services
Mr. Pal Phung, Federal Highway Administration

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PHONE (808) 594-1800



STATE OF HAWAII
OFFICE OF HAWAIIAN AFFAIRS
711 KAPOLANI BOULEVARD, SUITE 500
HONOLULU, HAWAII 96813

We will further rely on your assurances that should Iwi or Native Hawaiian cultural or traditional deposits be found during project activities, work will cease, and the appropriate agencies will be contacted pursuant to applicable law.

Thank you for the opportunity to comment. If you have further questions or require more information regarding OHA's comments, please contact Heidi Cluth by phone at 594-1962 or by e-mail at heidi@oha.org.

Sincerely,

Clyde W. Namu'o
Clyde W. Namu'o
Administrator

June 10, 2004

Larissa Sato
Parsons Brinckerhoff Quade & Douglas, Inc.
American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

HIRD04/1397 B

RE: Request for comments on Section 106 consultation about Waipio Point Access Road Improvements Study, Waipahu, Oahu: City and County of Honolulu Project: 2003223

Dear Larissa Sato:

The Office of Hawaiian Affairs (OHA) is in receipt of your May 12, 2004, request for comment on the above project, which would include contiguous bicycle lanes and sidewalks, a paved parking lot, improved roadway drainage, upgrades to Bridge No. 2, and landscaping. We offer the following comments.

Although the State Historic Preservation Division finds that the described activities planned for the project area do not appear to impact on the buried Loko Eo fishpond, OHA hopes that this cultural, historic site will continue to be monitored and preserved.

OHA also suggests that the Navy's selection of vegetation used for the proposed landscaping should include native plants as much as possible. This will enable the area to absorb as much water as is locally and naturally possible, while also ensuring fewer introductions of alien species to our fragile ecosystems.



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1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7084
Fax: 808-528-2268

May 12, 2004

Mr. Shad Kane
Oahu Council of Hawaiian Civic Clubs
92-1309 Uluhauai Street
Kapolei, Hawaii 96707

Dear Mr. Kane:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 20032223
National Historic Preservation Act Section 106 Consultation

We are assisting the City and County of Honolulu Department of Transportation Services and the U.S. Department of Transportation Federal Highway Administration in a project to improve Waipio Point Access Road in Waipahu, Oahu. The project site is about a half-a-mile makai of Fairington Highway (see enclosed map). The purpose of this letter is to request your consultation pursuant to Section 106 of the National Historic Preservation Act.

The project includes the following elements:

- Contiguous bicycle lanes and sidewalks between the O'ahu Bicycle/Pedestrian Path and the entrance of Waipio Peninsula Soccer Park;
- A paved parking lot for NAVSEA Inactive Ships On-Site Maintenance Office personnel adjacent to Waipio Point Access Road;
- Improved roadway drainage;
- Upgraded Waipio Point Access Road Bridge No. 2; and
- Roadway landscaping.

Based on information provided by the Navy, we are aware of a large buried fishpond, "Loko Eo", that underlies much of the project and surrounding area, including the Ted Makalana Golf Course and Naval facilities along the shoreline (see enclosed map). We are not aware of know of any other historic property within our project limits.

We have consulted with Dr. Sara Collins of the State Historic Preservation Division, who informed us that archaeological studies done by the Navy indicate that intact sediments of the fishpond occur approximately 21 feet below the existing surface (Ancient Hawaiian Fishponds of Pearl Harbor: Archaeological Studies of U.S. Navy Lands, Hawaii, July 2000). The proposed project requires excavation of up to 73 feet below the surface. Therefore, Dr. Collins believes that a "no historic properties affected" determination per Section 106 would be appropriate for the project because the required excavation would be far less than the approximately 21 feet where one could expect to uncover fishpond sediment. At this time, we are inclined to agree with Dr. Collins' assessment.

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Mr. Shad Kane
May 12, 2004
Page 2

Please let me know if you have any comments or concerns regarding our current assessment of the potential impacts to Loko Eo, in addition to letting me know if you know of any other historic property that could be affected by the project. We would appreciate receiving your comments or other information by June 14, 2004.

If you have questions or require additional information, please do not hesitate to call me at (808) 566-2246.

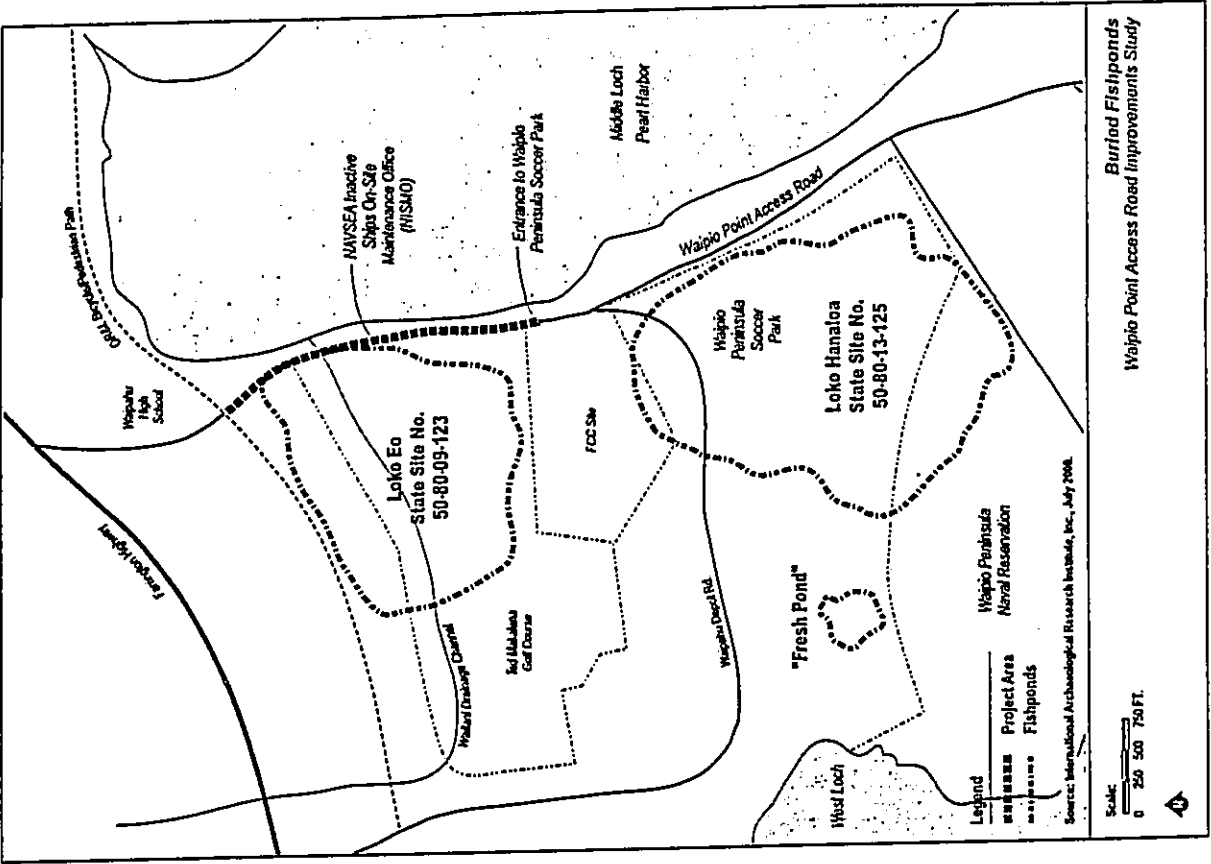
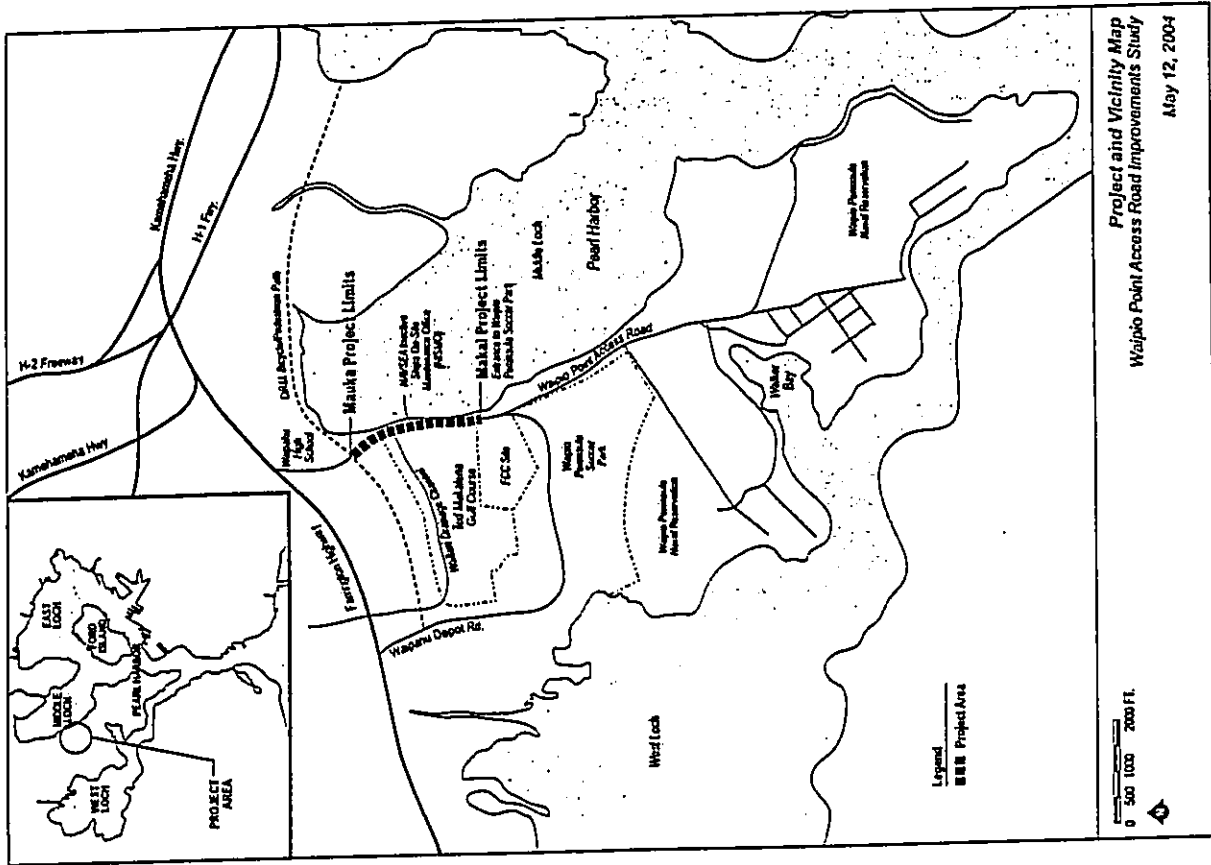
Sincerely yours,
Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Salo
Project Manager

Enclosure: Project vicinity map and Buried Fishponds map

cc: Mr. Bijan Suzuki, City and County of Honolulu, Department of Transportation Services
Mr. Pat Phung, Federal Highway Administration

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Parsons
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Quade &
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American Savings Bank Tower
1001 Bishop Street, Suite 3000
Honolulu, HI 96813
808-531-7094
Fax: 808-528-2368

May 12, 2004

Mr. Kunal Nihipali
Hui Malama I Na Kupuna O Hawaii Nei
P.O. Box 190
Halekwa, Hawaii 96712-0190

Dear Mr. Nihipali:

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
National Historic Preservation Act Section 106 Consultation

We are assisting the City and County of Honolulu Department of Transportation Services and the U.S. Department of Transportation Federal Highway Administration in a project to improve Waipio Point Access Road in Waipahu, Oahu. The project site is about a half-mile north of Farrington Highway (see enclosed map). The purpose of this letter is to request your consultation pursuant to Section 106 of the National Historic Preservation Act.

The project includes the following elements:

- Contiguous bicycle lanes and sidewalks between the ORAL Bicycle/Pedestrian Path and the entrance of Waipio Peninsula Soccer Park;
- A paved parking lot for NAVSEA inactive Ships On-Site Maintenance Office personnel adjacent to Waipio Point Access Road;
- Improved roadway drainage;
- Upgraded Waipio Point Access Road Bridge No. 2; and
- Roadway landscaping.

Based on information provided by the Navy, we are aware of a large buried fishpond, "Loko Eo", that underlies much of the project and surrounding area, including the Ted Malama Golf Course and Naval facilities along the shoreline (see enclosed map). We are not aware or know of any other historic property within our project limits.

We have consulted with Dr. Sara Collins of the State Historic Preservation Division, who informed us that archaeological studies done by the Navy indicate that intact sediments of the fishpond occur approximately 21 feet below the existing surface (Ancient Hawaiian Fishponds of Pearl Harbor: Archaeological Studies of U.S. Navy Lands, Hawaii, July 2000). The proposed project requires excavation of up to 13 feet below the surface. Therefore, Dr. Collins believes that a "no historic properties affected" determination per Section 106 would be appropriate for the project because the required excavation would be far less than the approximately 21 feet where one could expect to uncover fishpond sediment. At this time, we are inclined to agree with Dr. Collins' assessment.

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Mr. Kunal Nihipali
May 12, 2004
Page 2

Please let me know if you have any comments or concerns regarding our current assessment of the potential impacts to Loko Eo, in addition to letting me know if you know of any other historic property that could be affected by the project. We would appreciate receiving your comments or other information by June 14, 2004.

If you have questions or require additional information, please do not hesitate to call me at (808) 566-2246.

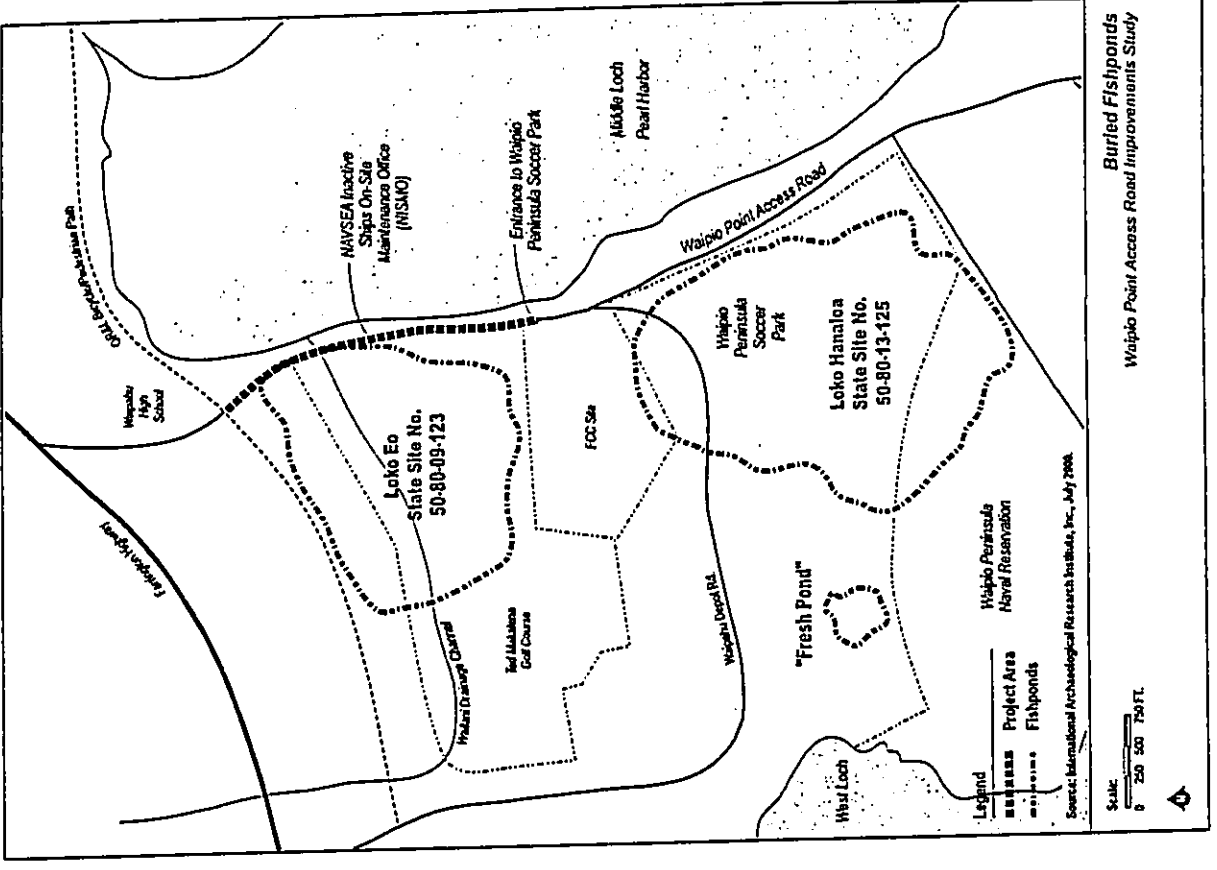
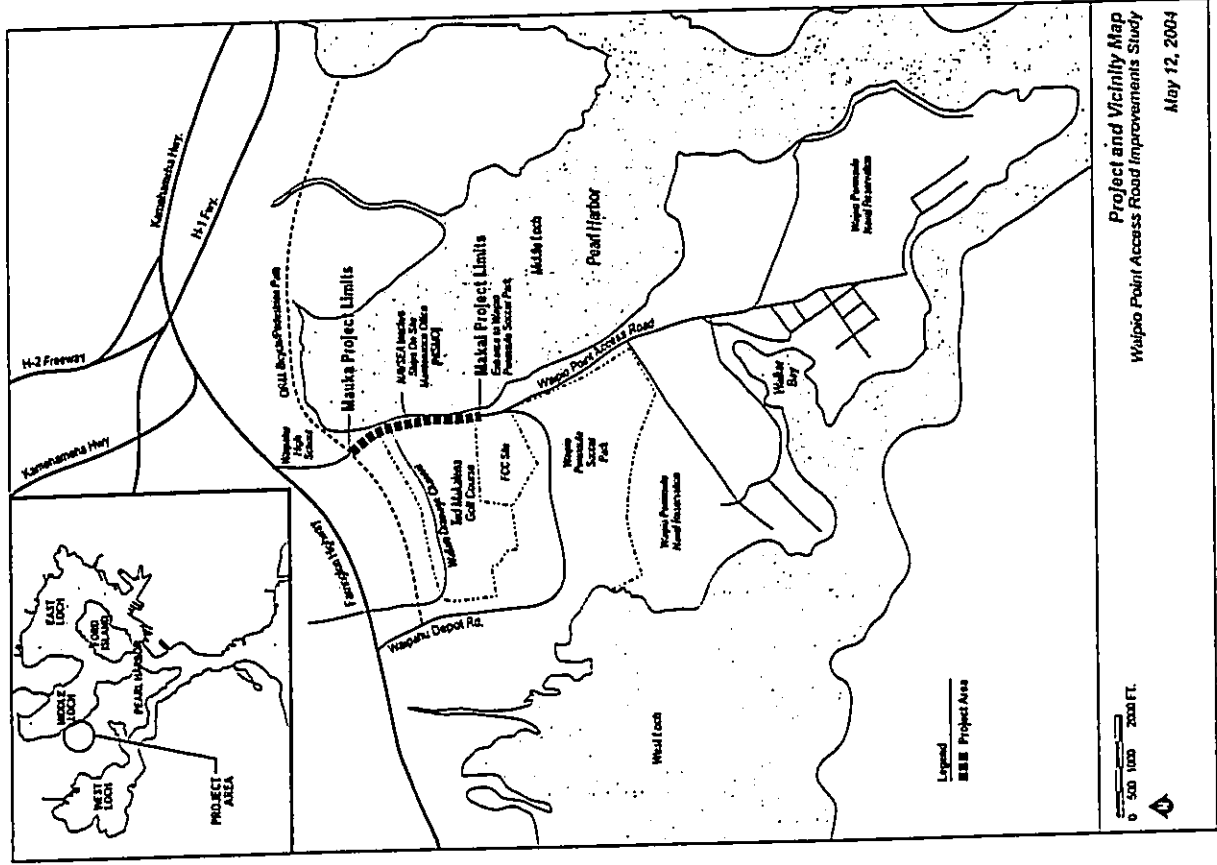
Sincerely yours,
Parsons Brinckerhoff Quade & Douglas, Inc.

Luana Salo
Luana Salo
Project Manager

Enclosure: Project vicinity map and Buried Fishponds map

cc: Mr. Brian Suzuki, City and County of Honolulu, Department of Transportation Services
Mr. Pat Phung, Federal Highway Administration

Over a Century of
Engineering Excellence





June 2, 2004

Mr. Van Diamond, Chair
Oahu Burial Council
c/o the State Historic Preservation Division
601 Kamohala Boulevard, Suite 555
Kapolei, Hawaii 96707

Subject: Waipio Point Access Road Improvements Study, Waipahu, Hawaii
City and County of Honolulu Project: 2003223
National Historic Preservation Act Section 106 Consultation

Dear Mr. Diamond:

We are assisting the City and County of Honolulu Department of Transportation Services and the U.S. Department of Transportation Federal Highway Administration in a project to improve Waipio Point Access Road TMK (1) 9-3-02:1 (portion) in Waipahu, Oahu. The project site is about a half-a-mile makai of Farrington Highway (see enclosed map). The purpose of this letter is to request your consultation pursuant to Section 106 of the National Historic Preservation Act.

The project includes the following elements:

- Contiguous bicycle lanes and sidewalks between the OR&L Bicycle/Pedestrian Path and the entrance of Waipio Peninsula Soccer Park;
- A paved parking lot for NAVSEA Inactive Ships On-Site Maintenance Office personnel adjacent to Waipio Point Access Road;
- Improved roadway drainage;
- Upgraded Waipio Point Access Road Bridge No. 2; and
- Roadway landscaping.

Based on information provided by the Navy, we are aware of a large buried fishpond, "Loko Eo", that underlies much of the project and surrounding area, including the Ted Makalena Golf Course and Naval facilities along the shoreline (see enclosed map). We are not aware or know of any other historic property within our project limits.

We have consulted with Dr. Sara Collins of the State Historic Preservation Division, who informed us that archaeological studies done by the Navy indicate that intact sediments of the fishpond occur approximately 21 feet below the existing surface (Ancient Hawaiian Fishponds of Pearl Harbor: Archaeological Studies of U.S. Navy Lands, Hawaii, July 2000). The proposed project requires excavation of up to 13 feet below the surface. Therefore, Dr. Collins believes that a "no historic properties affected" determination per Section 106 would be appropriate for

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Engineering Excellence



Mr. Van Diamond, Chair
June 2, 2004
Page 2

the project because the required excavation would be far less than the approximately 21 feet where one could expect to uncover fishpond sediment. At this time, we are inclined to agree with Dr. Collins' assessment.

Please let me know if you have any comments or concerns regarding our current assessment of the potential impacts to Loko Eo, in addition to telling me know if you know of any other historic property that could be affected by the project. We would appreciate receiving your comments or other information by July 2, 2004.

If you have questions or require additional information, please do not hesitate to call me at (808) 566-2235.

Sincerely yours,

Parsons Brinckerhoff Quade & Douglas, Inc.

Larissa Sato

Larissa Sato
Project Manager

Enclosure: Project vicinity map and Buried Fishponds map

cc: Mr. Brian Suzuki, City and County of Honolulu, Department of Transportation Services
Mr. Pal Phung, Federal Highway Administration

Over a Century of
Engineering Excellence

APPENDIX E ■

DRAFT ENVIRONMENTAL ASSESSMENT COMMENT LETTERS AND RESPONSES

**WAIPIO POINT ACCESS ROAD
Improvements**

FINAL ENVIRONMENTAL ASSESSMENT



DEPARTMENT OF THE NAVY
NAVAL FACILITIES ENGINEERING COMMAND, PACIFIC
238 MAHALA DRIVE, STE. 110
PEARL HARBOR, HAWAII 96813-3154

11011

Ser OPHREQW/182

11 FEB 2005

FEB 15 10:25

RECEIVED

OPERATIONAL PLANNING
OPERATIONAL PLANNING
OPERATIONAL PLANNING

Mr. George "Keoki" Miyamoto
Director
Department of Transportation Services
City and County of Honolulu
650 South King Street, 3rd Floor
Honolulu, HI 96813

Dear Mr. Miyamoto:

The Draft Environmental Assessment for the Waipio Point Access Road Improvements forwarded by your letter of December 21, 2004 has been reviewed and the following comments are provided:

1. Page S-11, Utilities - Relocation of Utilities could involve excavation into groundwater. Should dewatering be required and the dewatered effluent require discharge to the storm drain or Waters of the United States, such as Pearl Harbor, the contractor will be required to obtain a National Pollutant Discharge Elimination System (NPDES) Permit for the Discharges Associated with Construction Activity Dewatering.
2. Page S-11, Utilities - Relocation of Utilities such as water and sewer lines may require hydrotesting of the relocated lines. Should it be required to discharge hydrotest waters to the storm drain or Waters of the United States, such as Pearl Harbor, the contractor will be required to obtain a National Pollutant Discharge Elimination System (NPDES) Permit authorizing the discharge of hydrotest waters.
3. Page S-12, S.6/Page 2-27, 2.6 -- Approvals and Permits - A National Pollutant Discharge Elimination System Permit for Discharges Associated with Construction Activity Dewatering and a National Pollutant Discharge Elimination system Permit Authorizing the Discharge of Hydrotest Waters are other permits that could be required.
4. Page 1-7, 1.2.2. - Second paragraph on this page states that accidents have occurred due to speeding on the straight stretch of roadway. Consider use of traffic calming device (possibly a crosswalk ramp) on this stretch to slow traffic if it will not impact level of service.
5. General Comments: There is an on going monitoring and remediation of JP-5 in the area within and surrounding area NISMO facility. In that area there are monitoring wells located in the general improvement area (see enclosed figure 1). Any work should take account the location of the monitoring wells. In addition, any excavated soil from the proximity of the roadway, may have potential for petroleum contamination.

Enclosed Figure 2 shows the JP-5 plume area in 1999. The plume has reduced in size and is currently be evaluated under the monitoring program. Any work taking place in the general area should be aware of plume.

If you have any questions regarding the above comments, please contact me at 471-9337 or email address genie.wery@navy.mil.

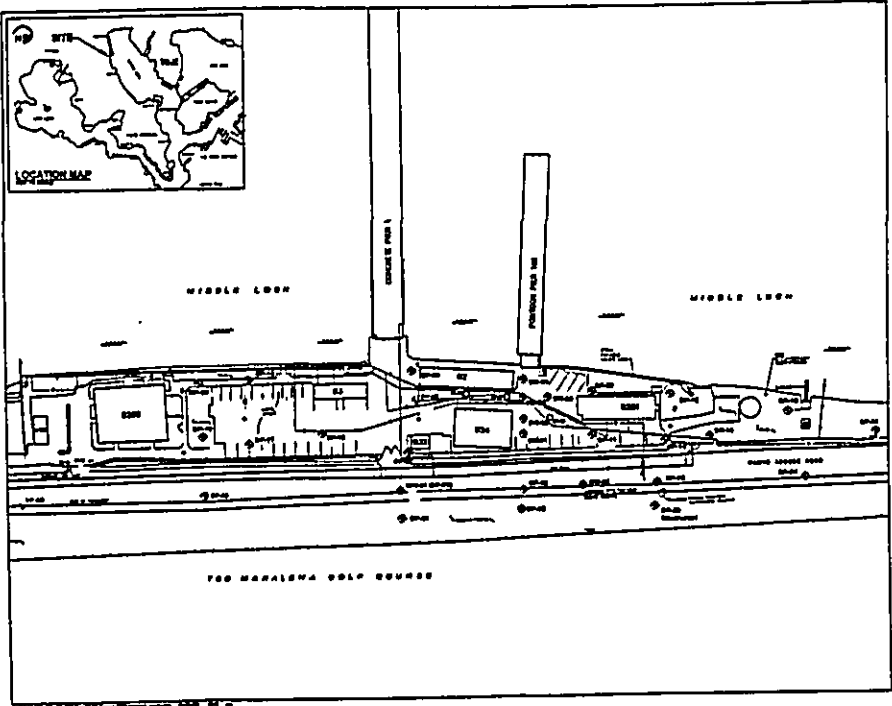
Sincerely,

GEMIE WERY
Realty Specialist
Integrated Product Team Hawaii
Real Estate

Encl:

- (1) Figure 1
- (2) Figure 2

ENCL 10



LEGEND

- SP-01 Above Ground Location with Identifier
- SP-02 Closed Point and Survey Location with Identifier
- Check Line Point
- Water Line
- Fuel Line
- Sewer Line
- Storm Drain Line

NOTES

1. All utility line locations are approximate and should be field verified prior to any release work.
2. SP-01 is ground level.
3. SP-02 is no longer present, aerial observed at 1983 location.

SOURCE

Department of the Army, Petroleum Engineering Command
 PEG (DAG) HQ, 780271
 Closed Point Survey and Engineering Inc. May 15, 1987.

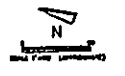
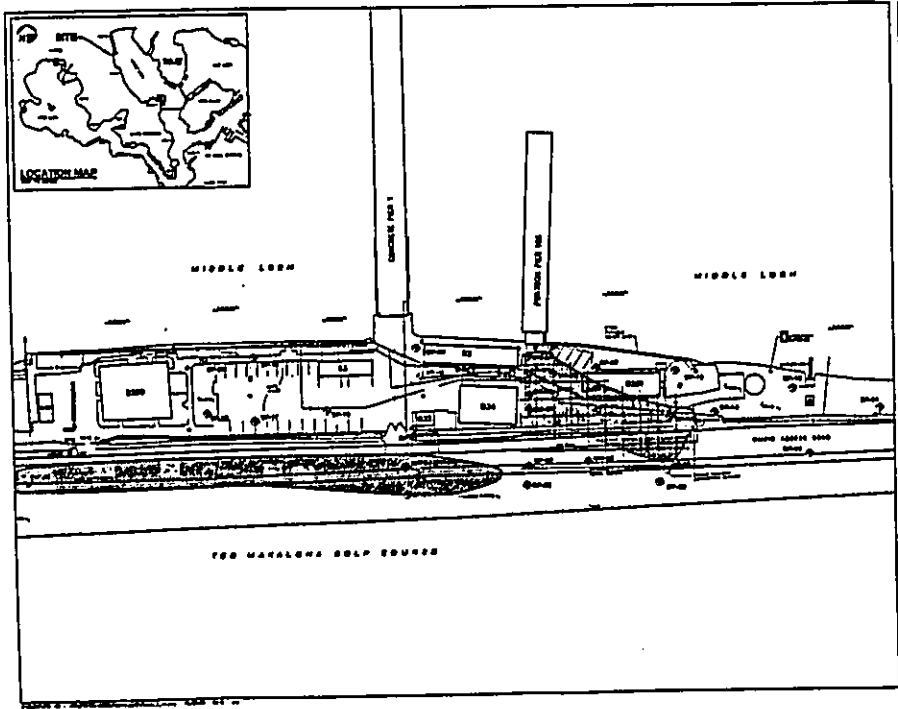


Figure 1
 Possible Location Map with
 Monitoring Point Locations
 1982/83, 1984
 Ocala, Florida

ENCL 11



LEGEND

- SP-01 Above Ground Location with Identifier
- SP-02 Closed Point and Survey Location with Identifier
- Check Line Point
- Water Line
- Fuel Line
- Sewer Line
- Storm Drain Line
- Plume Area

NOTES

1. All utility line locations are approximate and should be field verified prior to any release work.
2. SP-01 is ground level.
3. SP-02 is no longer present, aerial observed at 1983 location.

SOURCE

Department of the Army, Petroleum Engineering Command
 PEG (DAG) HQ, 780271
 Closed Point Survey and Engineering Inc. May 15, 1987.



Figure 2
 Observed Petroleum Product
 Plumes in 1999
 1982/83, 1984
 Ocala, Florida

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU
650 SOUTH KING STREET, 3RD FLOOR • HONOLULU, HAWAII 96813
TELEPHONE: (808) 523-4233 • FAX: (808) 523-4730 • INTERNET: www.cc.hawaii.gov



MULTI-MEDIA
DIRECTOR

EDWARD Y. HIRATA
DIRECTOR

TPD05-00239R

May 5, 2005

Ms. Genie Wery
Realty Specialist
Department of the Navy
Naval Facilities Engineering Command, Hawaii
400 Marshall Road, Bldg 223
Pearl Harbor, Hawaii 96860-3139

Dear Ms. Wery:

Subject: Waipio Point Access Road Improvements Draft Environmental Assessment

Thank you for providing your comments in your letter dated February 11, 2005 on the Waipio Point Access Road Draft Environmental Assessment (EA). We would like to provide the following responses by number provided in your letter. A copy of your letter is provided.

Comments 1, 2 and 3: Sections S.5, 2.4.5 and, 2.6 will be revised in the Final EA to disclose that National Pollutant Discharge Elimination System permits will be required if the contractor discharges water from dewatering or hydrotesting during utility relocation work.

Comment 4: The Waipio Point Access Road is classified as a major collector and as such the use of traffic calming devices on major collectors is inappropriate.

Comment 5: Thank you. We are aware of the plume, and as stated in Section 2.4.2 of the Draft EA, testing will be conducted prior to construction to determine the extent of contamination that may be uncovered during construction. Section 2.4.2 will be revised in the Final EA to disclose the locations of the JP-5 monitoring wells. We will try to avoid displacing the wells during construction. Any well displaced by construction will be replaced.

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

Sincerely,

EDWARD Y. HIRATA
Director



DEPARTMENT OF THE NAVY
COMMANDER
NAVY REGION HAWAII
650 TCOMBERGCA ST STE 118
PEARL HARBOR HI 96860-3101

Mr. George Miyamoto
Department of Transportation Services
City and County of Honolulu
650 South King Street 3rd Floor
Honolulu HI 96813

Dear Mr. Miyamoto:

SUBJECT: WAIPIO POINT ACCESS ROAD IMPROVEMENTS
WAIPIAHU, OAHU, HAWAII
DRAFT ENVIRONMENTAL ASSESSMENT

This responds to your letter TPD04-00616 of December 21, 2004 requesting comment on the subject Draft Environmental Assessment. The following comments are provided:

1. There is a serious safety concern with vehicles crossing the path of bicyclists when entering and leaving the parking stalls provided for NAVSEA Inactive Ships Maintenance Office use. The safety concern needs to be addressed prior to obtaining Navy approval to proceed.

2. An acronym list should be inserted after the Table of Contents.

3. Page 9-8, section 2.3 project description, approval must be obtained from the Navy to add the 0.05 acres of Navy land northeast of Bridge No. 2 to the City's roadway easement. The City must formally request use of the land from the Navy.

4. Page 2-6, section 2.1.1.2.2 only addresses the roadway-related pollutants impact to Middle Loch. The summary on page 9-9, section 5.5 water resources should be revised to clearly reflect "roadway-related" pollutants because analysis was not provided for other potential sources such as the new paved parking area and drainage improvements.

Should you have any questions regarding this matter, please contact Mr. Jeffrey Takeuchi at (808) 471-1170, extension 261, fax (808) 471-1160, or e-mail jeffrey.takeuchi@navy.mil.

Sincerely,

D. C. LEWIS
LCDR, CBC, USN
Deputy Program Manager for Facilities,
Environmental, Safety & Passenger
Transportation
By direction of
Commander, Navy Region Hawaii

11011
Ser H464/00004
21 JAN 2005

RECEIVED
35 JAN 25 10:14
NAVY REGION HAWAII
TRANSPORTATION

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU
650 SOUTH KING STREET, 3RD FLOOR • HONOLULU, HAWAII 96813
TELEPHONE: (808) 532-4519 • FAX: (808) 532-4790 • INTERNET: www.ci.honolulu.hi.us



LEI HANAUWALE
MAYOR

EDWARD Y. HIRATA
DIRECTOR

TPD05-00238R

May 5, 2005

Commander
Navy Region Hawaii
850 Ticonderoga Street, Suite 110
Pearl Harbor, Hawaii 96860-5102

Attention: N484 Mr. Jeffrey Takeuchi

Dear Commander:

Subject: Waipio Point Access Road Improvements Draft Environmental Assessment

Thank you for providing your comments in your letter dated January 21, 2005 on the Waipio Point Access Road Draft Environmental Assessment (EA). We would like to provide the following responses by number provided in your letter. A copy of your letter is provided.

Comment 1: We had previously met with your staff prior to the preparation of the Draft EA document. At the time, we presented the proposed alternative and received minor comments on the alternative. However, in light of your concerns, we will re-examine the bicycle safety issue in preparing the Final EA.

Comment 2: We will comply with this comment.

Comment 3: Thank you for this information.

Comment 4: Section S.5 will be revised to clarify that the pollutants referred to are from "roadway-related" sources.

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

Sincerely,

EDWARD Y. HIRATA
Director



FEDERAL COMMUNICATIONS COMMISSION
Enforcement Bureau
Spectrum Enforcement Division
445 12th Street, S.W.
Washington, D.C. 20554

RECEIVED
15 MAR 17 10:14
U.S. DEPARTMENT OF JUSTICE
FEDERAL BUREAU OF INVESTIGATION
WASHINGTON, D.C. 20535

March 11, 2005

Brian Suzuki
Department of Transportation Services
City and County of Honolulu
650 South King Street, 3rd Floor
Honolulu, Hawaii 96813

Dear Mr. Suzuki:

Pursuant to our telephone conversation of February 7, 2005, I am sending this letter in response to the invitation to submit comments to the Waipio Point Access Road Improvements Draft Environmental Assessment. Our Hawaii Resident Agent Office has raised concerns about the placement of the Waipio Peninsula Soccer Park sign and accompanying gate. As we discussed, the sign and gate are currently located at the entrance to the driveway leading to the FCC's Resident Agent Office on Waipio Point Access Road. Because of the placement of the sign, people frequently turn into the FCC's driveway, assuming that they are turning into the entrance to the soccer park, when the park entrance is actually approximately one-half mile past the FCC's property. Moving the sign would eliminate the unnecessary, potential safety hazard of drivers having to re-enter the roadway either by turning around on the FCC's property or backing out onto the roadway. It also appears that it may be better for park security and maintenance purposes to have the sign and gate located at the park entrance. Moreover, the gate, when locked, blocks the frontage of the FCC's property.

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU

630 SOUTH KING STREET, 2ND FLOOR - HONOLULU, HAWAII 96813
TELEPHONE: (808) 525-5151 • FAX: (808) 523-1720 • INTERNET: www.cc.honolulu.hi



MULTIMEDIA
DIVISION

EDWARD Y. HIRATA
DIRECTOR

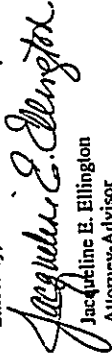
TPD05-00237R

May 5, 2005

To remedy this situation, we propose moving the soccer park sign and gate 700 feet past the FCC's driveway towards the actual entrance to the soccer park. We believe this placement of the sign and gate would be better for the FCC, park staff, and drivers looking for the soccer park.

Your time and attention to this matter is greatly appreciated.

Sincerely,


Jacqueline E. Ellington
Attorney-Advisor
Enforcement Bureau
Spectrum Enforcement Division

Ms. Jacqueline E. Ellington
Attorney-Advisor
Enforcement Bureau
Spectrum Enforcement Division
Federal Communications Commission
445 12th Street, S.W.
Washington, D.C. 20554

cc: Joseph P. Casey
Chief, Spectrum Enforcement Division
FCC, Enforcement Bureau

Ryan Hagiwara
Resident Agent, Honolulu Office
FCC, Enforcement Bureau

Dear Ms. Ellington:

Subject: Waipio Point Access Road Improvements Draft Environmental Assessment
Thank you for providing your comments in your letter dated March 11, 2005 on the Waipio Point Access Road Draft Environmental Assessment.

Your letter expressed concern with the existing location of the Waipio Peninsula Soccer Park sign and gate, and the confusion it causes for soccer park users as they often turn into the Federal Communications Commissions driveway thinking that the road leads to the soccer park.

Our project primarily involves the improvement of the Waipio Point Access roadway. We will refer your concern to the City's Department of Parks and Recreation (DPR) and suggest that the matter be investigated by their staff.

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

Sincerely,


EDWARD Y. HIRATA
Director

EDWARD Y. HIRATA
DIRECTOR



PATRICIA HAMAMOTO
SUPERINTENDENT

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU
650 SOUTH KING STREET, 3RD FLOOR • HONOLULU, HAWAII 96813
TELEPHONE: (808) 533-4229 • FAX: (808) 533-4230 • INTERNET: www.cc.honolulu.gov



MAITUMARE MANA
MAYOR

STATE OF HAWAII
DEPARTMENT OF EDUCATION
P.O. BOX 2360
HONOLULU, HAWAII 96810

OFFICE OF THE SUPERINTENDENT

January 11, 2005

RECEIVED
35 JAN 13 12:07
LINEUP UNIT - FULL
DEPT. OF
TRANSPORTATION SERVICES

EDWARD Y. HIRATA
DIRECTOR

TP-105-90402R

May 5, 2005

Mr. George "Keoki" Miyamoto, Director
Department of Transportation Services
City and County of Honolulu
650 South King Street
Honolulu, Hawaii 96813

Ms. Patricia Hamamoto
Superintendent
Department of Education
State of Hawaii
P.O. Box 2360
Honolulu, Hawaii 96804

Dear Mr. Miyamoto:

SUBJECT: Waipio Point Access Road Improvements
Draft Environmental Assessment
Waipahu (Project # 2003223), Oahu

Dear Ms. Hamamoto:

Subject: Waipio Point Access Road Improvements Draft Environmental Assessment
Thank you for providing your comments in your letter dated January 11, 2005 on the Waipio Point Access Road Draft Environmental Assessment (EA).

The Department of Education (DOE) has reviewed the Draft Environmental Assessment (DEA) for the proposed improvements to the Waipio Point Access Road. Although the DEA states that Waipahu High School has no "social influence" on the project site, the school relies on the north end of Waipio Point Access Road as a school access. The DOE requests that contact be made with the principal of Waipahu High School whenever there will be a road closure, disruptions to utility service, or other conditions that may impact the school including relocation of pedestrian passageways.

Your letter expressed concern that construction may adversely affect access to Waipahu High School. We do not anticipate that construction activities, including utility disruptions, would affect the high school because the project limits start makai of the OR&L Right-of-Way. The most noticeable impact would be construction-related vehicles using Waipio Point Access Road and Farrington Highway to access the construction site. Nevertheless, we will inform the principal if a construction activity disrupts access to Waipahu High School or utility service to the school is temporarily halted.

If you should have any questions, please call Rae Loui, Assistant Superintendent of the Office of Business Services, at 586-3444 or Heidi Mecker of the Facilities and Support Services Branch at 733-4862.

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

Very truly yours,

Patricia Hamamoto

Patricia Hamamoto
Superintendent

PH:j

Sincerely,

Edward Y. Hirata

EDWARD Y. HIRATA
Director

c: Rae Loui, OBS
Karen Moriyama, CAS, Nanakuli/Pearl City/Waipahu Complex Area
Patricia Pedersen, Principal, Waipahu High School

AN AFFIRMATIVE ACTION AND EQUAL OPPORTUNITY EMPLOYER

1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 60 61 62 63 64 65 66 67 68 69 70 71 72 73 74 75 76 77 78 79 80 81 82 83 84 85 86 87 88 89 90 91 92 93 94 95 96 97 98 99 100

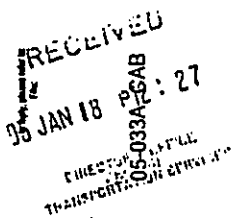
LINDA LINGLE
COMMISSIONER OF HEALTH



STATE OF HAWAII
DEPARTMENT OF HEALTH
P.O. BOX 3378
HONOLULU, HAWAII 96813

January 13, 2005

CHRISTOPHER L. FURUKAWA, M.D.
DIRECTOR OF PUBLIC HEALTH



Mr. George "Keoki" Miyamoto
January 13, 2005
Page 2

- c) Landscape and provide rapid covering of bare areas, including slopes, starting from the initial grading phase;
- d) Minimize dust from shoulders and access roads;
- e) Provide adequate dust control measures during weekends, after hours, and prior to daily start-up of construction activities; and
- f) Control dust from debris being hauled away from the project site.

If you have any questions, please contact Mr. Barry Ching of my staff at 586-4200.

Sincerely,

WILFRED K. NAGAMINE
Manager, Clean Air Branch

BC:jhm

Mr. George "Keoki" Miyamoto
Department of Transportation Services
City and County of Honolulu
650 South King Street, 3rd Floor
Honolulu, Hawaii 96813

Dear Mr. Miyamoto:

SUBJECT: Draft Environmental Assessment for the Waipio Point Access Road
Improvements Project, Waipio, Oahu (Ref: 1PD04-00616)

This letter is to transmit the following comments on the subject document:

Control of Fugitive Dust:

There is significant potential for fugitive dust emissions during all phases of construction. Proposed construction activities will occur in proximity to existing residences, public areas and major thoroughfares, thereby exacerbating potential dust problems. It is recommended that a dust control management plan be developed which identifies and addresses all activities that have a potential to generate fugitive dust. Implementation of adequate dust control measures during all phases of development and construction activities is warranted.

Construction activities must comply with the provisions of Hawaii Administrative Rules, §11-60.1-33 on Fugitive Dust. The contractor should provide adequate measures to control dust from the road areas and during the various phases of construction. These measures include, but are not limited to, the following:

- a) Plan the different phases of construction, focusing on minimizing the amount of dust-generating materials and activities, centralizing on-site vehicular traffic routes, and localizing potential dust-generating equipment in areas of the least impact;
- b) Provide an adequate water source at the site prior to start-up of construction activities;

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU

450 SOUTH KING STREET, 3RD FLOOR - HONOLULU, HAWAII 96813
TELEPHONE: (808) 533-4333 • FAX: (808) 533-4130 • INTERNET: www.ctd.hawaii.gov



MULTIMEDIA
UNIT

EDWARD Y. HIRATA
DIRECTOR

TP-1/05-90696R

May 5, 2005

Mr. Wilfred K. Nagamine
Manager
Clean Air Branch
Department of Health
State of Hawaii
P.O. Box 3378
Honolulu, Hawaii 96801

Dear Mr. Nagamine:

Subject: Waipio Point Access Road Improvements Draft Environmental Assessment

Thank you for providing your comments in your letter dated January 13, 2005 on the Waipio Point Access Road Draft Environmental Assessment (EA).

Your letter asked that we control dust emissions during construction. Section 2.4.4 of the Draft EA identifies measures that may be implemented to control fugitive dust during construction. In accordance with State law, we will instruct the contractor to control dust emissions from the construction site.

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

Sincerely,

EDWARD Y. HIRATA
Director



STATE OF HAWAII
OFFICE OF ENVIRONMENTAL QUALITY CONTROL

228 SOUTH KULIKANI STREET
HONOLULU, HAWAII 96813
TELEPHONE: (808) 536-1100
FAX: (808) 536-1100

January 20, 2005

Mr. Edward Hirata, Director
Department of Transportation Services
City and County of Honolulu
711 Kapiolani Boulevard, Suite 1200
Honolulu, Hawaii 96813

Dear Mr. Hirata:

Subject: Draft EA for the Waipio Point Access Road Improvements, Oahu

Thank you for the opportunity to review this document. We have the following comments.

1. This project should comply with sections 103D-407 and 408 of Hawaii Revised Statutes concerning the use of indigenous plants and recycled glass.
2. Please disclose whether any cultural practices associated with Loko Eo will be affected by this project.
3. Please consult with the Hawaii Soccer Association and other major park users.
4. Please send response letters to the pre-consultation commenters.

Should you have any questions please call Jeyan Thirugnanam at 586-4185.

Sincerely,

Genevieve Salomonson
Director

c: Parsons Brinckerhoff

GENEVIEVE SALOMONSON
DIRECTOR

RECEIVED
35 JAN 24 P2:07

ENVIRONMENTAL QUALITY CONTROL
TRANSPORTATION SERVICES

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU
450 SOUTH KING STREET, 3RD FLOOR • HONOLULU, HAWAII 96813
TELEPHONE: (808) 523-4339 • FAX: (808) 523-4730 • INTERNET: www.cc.hawaii.gov



METROPLANNING
MANAGER

May 5, 2005

TP1005-91463R

Ms. Genevieve Salmonson
Director

Office of Environmental Quality Control
235 South Beretania Street, Suite 702
Honolulu, Hawaii 96813

Dear Ms. Salmonson:

Subject: Waipio Point Access Road Improvements Draft Environmental Assessment

Thank you for providing your comments in your letter dated January 20, 2005 on the Waipio Point Access Road Draft Environmental Assessment (EA). We would like to provide the following responses by number provided in your letter. A copy of your letter is provided.

Comment 1: We will consider using indigenous plants for landscaping, and recycled glass material for roadway pavements.


Comment 2: Because the Loko Eo is buried 21 feet below the surface, it is not in use as an active fishpond, nor can it be restored as an active fishpond.

Comment 3: We consulted with the American Youth Soccer Organization and other major users of the soccer park. Chapter 3 will be revised to include reference to this coordination.

Comment 4: The agencies and organizations that assisted us in scoping for the EA received copies of the Draft EA.

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

Sincerely,


EDWARD Y. HIRATA
Director



STATE OF HAWAII
DEPARTMENT OF HEALTH
P.O. BOX 3378
HONOLULU, HAWAII 96831-3378

February 14, 2005

LANA LUCILE
DIRECTOR OF HEALTH

CYRUS L. JENSEN, M.D.
DIRECTOR OF HEALTH

RECEIVED
35 FEB 22 P 2: 41

DIRECTOR'S OFFICE
DEPARTMENT OF HEALTH
TRANSPORTATION SERVICES

Mr. Edward Y. Hirata, Acting Director
Department of Transportation Services
City and County of Honolulu
650 South King Street, 3rd Floor
Honolulu, Hawaii 96813

Dear Mr. Hirata:

SUBJECT: Waipio Point Access Road Improvements
Draft Environmental Assessment

Thank you for the opportunity to review and comment on the above cited document. The Solid Waste, Hazardous Waste, and Underground Storage Tank programs within the Solid and Hazardous Waste Branch have reviewed it.

We have no comments to offer at this time.

Sincerely,


STEVEN Y. K. CHANG, P. E., CHIEF
Solid and Hazardous Waste Branch

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU

650 SOUTH KING STREET, 2ND FLOOR • HONOLULU, HAWAII 96813
TELEPHONE (808) 523-5222 • FAX (808) 523-4730 • INTERNET: www.ctd.hawaii.gov



EDWARD Y. HIRATA
DIRECTOR

May 5, 2005

TP2005-95143R

Mr. Steven Y.K. Chang, P.E.
Chief
Solid and Hazardous Waste Branch
Department of Health
State of Hawaii
P.O. Box 3378
Honolulu, Hawaii 96801

Dear Mr. Chang:

Subject: Waipio Point Access Road Improvements Draft Environmental Assessment

Thank you for reviewing our Waipio Point Access Road Draft Environmental Assessment (EA).

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

Sincerely,

EDWARD Y. HIRATA
Director

LINDA WICKS
DIRECTOR OF TOURISM



STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES
LAND DIVISION

POST OFFICE BOX 621
HONOLULU, HAWAII 96809

February 1, 2005

WAIPIOPOINTDEA.RCH

Parsons Brinckerhoff Quade & Douglas, Inc.
Larissa Sato, Project Manager
1001 Bishop Street, Suite 3000
Honolulu, Hawaii 96813

Dear Ms. Sato:

SUBJECT: Draft Environmental Assessment (DEA) for the Waipio Point
Access Road Improvements Study, Waipahu, Oahu, Hawaii
City and County of Honolulu Project 2003223

Thank you for the opportunity to review and comment on the subject matter.

A copy of DEA pertaining to the subject project was transmitted or made available to the following Department of Land and Natural Resources' Divisions for their review and comment.

- Engineering Division
- Commission on Water Resource Management
- Office of Conservation and Coastal Lands
- Land-Oahu District Land Office

Enclosed please find a copy of the Engineering Division comment.

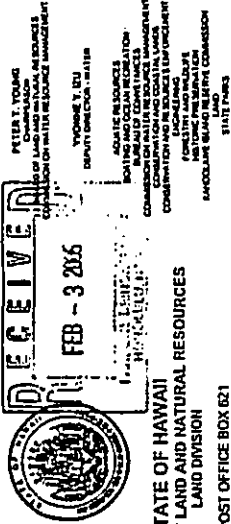
Based on the attached responses, the Department of Land and Natural Resources has no other comment to offer on the subject matter.

Should you have any questions, please contact Nicholas A. Vaccaro of the Land Division Support Services Branch at 587-0384.

Very truly yours,

WARREN WEGEEND
Administrator

C: ODLIO



ID-NAV

PETER T. YOUNG
 CHIEF OF LAND AND NATURAL RESOURCES
 COMMISSION ON WATER RESOURCE MANAGEMENT

DAVID VANDER
 DEPUTY DIRECTOR - LAND

YOUNG, T. DU
 DEPUTY CHIEF OF STAFF

COMMISSION ON WATER RESOURCE MANAGEMENT
 DEPARTMENT OF LAND AND NATURAL RESOURCES
 LAND DIVISION
 POST OFFICE BOX 621
 HONOLULU, HAWAII 96809

January 4, 2005

STATE OF HAWAII
 DEPARTMENT OF LAND AND NATURAL RESOURCES
 LAND DIVISION
 POST OFFICE BOX 621
 HONOLULU, HAWAII 96809

January 4, 2005

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January 4, 2005

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 LAND DIVISION
 POST OFFICE BOX 621
 HONOLULU, HAWAII 96809

January 4, 2005

LD/NAV
 WAIPIOPOINTDEA.CMT

Suspense Date: 1/18/05

MEMORANDUM:

TO: *HW* *XXX Engineering Division
 *XXX Commission on Water Resource Management
 *XXX Office of Conservation and Coastal Lands
 *XXX Land-Oahu District Land Office

FROM: Dierdre S. Hamiya, Administrator
 Land Division

SUBJECT: Waipio Point Access Road Improvement Study, Waipahu,
 Island of Oahu, Hawaii
 City and County of Honolulu Project 2003223
 Consultant: Parson Brinckerhoff Quade & Douglas, Inc.
 Draft Environmental Assessment

Please review the DEA pertaining to the subject matter and submit your comment (if any) on Division letterhead signed and dated by the suspense date.

NOTE: One copy of the document is available for your review in the Land Division Office, Room 220.

Should you need more time to review the subject matter, please contact Nicholas A. Vaccaro at 587-0384. If this office does not receive your comments by the suspense date, we will assume there are no comments.

(V) We have no ^{additional} comments. () Comments attached.

Signed: *Edwin T. Sakoda* Date: *1/1/05*

Name: *Edwin T. Sakoda* Division: *CLM/NA*

LD/NAV
 WAIPIOPOINTDEA.CMT

Suspense Date: 1/18/05

MEMORANDUM:

TO: *XXX Engineering Division
 *XXX Commission on Water Resource Management
 *XXX Office of Conservation and Coastal Lands
 *XXX Land-Oahu District Land Office

FROM: Dierdre S. Hamiya, Administrator
 Land Division

SUBJECT: Waipio Point Access Road Improvement Study, Waipahu,
 Island of Oahu, Hawaii
 City and County of Honolulu Project 2003223
 Consultant: Parson Brinckerhoff Quade & Douglas, Inc.
 Draft Environmental Assessment

Please review the DEA pertaining to the subject matter and submit your comment (if any) on Division letterhead signed and dated by the suspense date.

NOTE: One copy of the document is available for your review in the Land Division Office, Room 220.

Should you need more time to review the subject matter, please contact Nicholas A. Vaccaro at 587-0384. If this office does not receive your comments by the suspense date, we will assume there are no comments.

(V) We have no comments. () Comments attached.

Signed: *Cecil Santos* Date: *1/5/05*

Name: *Cecil Santos* Division: *LAND*

PETRA T. YOUNG
Commissioner
DEPARTMENT OF LAND AND NATURAL RESOURCES
COMMISSION ON WATER RESOURCES MANAGEMENT
STATE OFFICE BOX 231
HONOLULU, HAWAII 96809
TELEPHONE: 521-2100
FAX: 521-2101
WWW.DLN.HAWAII.GOV

STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES
LAND DIVISION
POST OFFICE BOX 231
HONOLULU, HAWAII 96809
January 4, 2005



PETRA T. YOUNG
Commissioner
DEPARTMENT OF LAND AND NATURAL RESOURCES
COMMISSION ON WATER RESOURCES MANAGEMENT
STATE OFFICE BOX 231
HONOLULU, HAWAII 96809
TELEPHONE: 521-2100
FAX: 521-2101
WWW.DLN.HAWAII.GOV

STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES
LAND DIVISION
POST OFFICE BOX 231
HONOLULU, HAWAII 96809
January 4, 2005



LD/NAV
WAIPIOPOINTDEA.CHT
MEMORANDUM
Suspense Date: 1/18/05

TO: *XXX Engineering Division
*XXX Commission on Water Resource Management
*XXX Office of Conservation and Coastal Lands
*XXX Land-Oahu District Land Office

FROM: Dierdre S. Mamiya, Administrator
Land Division

SUBJECT: Waipio Point Access Road Improvement Study, Waipahu,
Island of Oahu, Hawaii
City and County of Honolulu Project 2003223
Consultant: Parson Brinckerhoff Quade & Douglas, Inc.
Draft Environmental Assessment

Please review the DEA pertaining to the subject matter and submit your comment (if any) on Division letterhead signed and dated by the suspense date.

NOTE: One copy of the document is available for your review in the Land Division Office, Room 220.

Should you need more time to review the subject matter, please contact Nicholas A. Vaccaro at 587-0384. If this office does not receive your comments by the suspense date, we will assume there are no comments.

() We have no comments. Comments attached.
Signed: Eric Hirano Date: 1/6/05
Name: ERIC T. HIRANO, CHIEF ENGINEER Division: Engineering

LD/NAV
WAIPIOPOINTDEA.CHT
MEMORANDUM
Suspense Date: 1/18/05

TO: *XXX Engineering Division
*XXX Commission on Water Resource Management
*XXX Office of Conservation and Coastal Lands
*XXX Land-Oahu District Land Office

FROM: Dierdre S. Mamiya, Administrator
Land Division

SUBJECT: Waipio Point Access Road Improvement Study, Waipahu,
Island of Oahu, Hawaii
City and County of Honolulu Project 2003223
Consultant: Parson Brinckerhoff Quade & Douglas, Inc.
Draft Environmental Assessment

Please review the DEA pertaining to the subject matter and submit your comment (if any) on Division letterhead signed and dated by the suspense date.

NOTE: One copy of the document is available for your review in the Land Division Office, Room 220.

Should you need more time to review the subject matter, please contact Nicholas A. Vaccaro at 587-0384. If this office does not receive your comments by the suspense date, we will assume there are no comments.

() We have no comments. () Comments attached.
Signed: Cecil Santus Date: 1/5/05
Name: CECIL SANTUS Division: LAND

05 JAN 04 PM 04:54 ENGINEERING



DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU
 540 SOUTH KING STREET, 2ND FLOOR - HONOLULU, HAWAII 96813
 TELEPHONE: (808) 533-4531 - FAX: (808) 533-4230 - INTERNET: www.honolulu.gov



EDWARD Y. HIRATA
 DIRECTOR

TPD05-00240

May 5, 2005

MARTIN HANAUSSON
 MANAGER

DEPARTMENT OF LAND AND NATURAL RESOURCES
 ENGINEERING DIVISION

LJNAV

Re: WAIPIOPOINTDEA.CMT
 OAIU-478

COMMENTS

- () We confirm that the project site, according to the Flood Insurance Rate Map (FIRM), is located in Flood Zone .
 - () Please take note that the project site according to the Flood Insurance Rate Map (FIRM), is located in Zone .
 - () Please note that the correct Flood Zone Designation for the project site according to the Flood Insurance Rate Map (FIRM) is .
 - () Please note that the project must comply with the rules and regulations of the National Flood Insurance Program (NFIP) presented in Title 44 of the Code of Federal Regulations (44CFR), whenever development within a Special Flood Hazard Area is undertaken. If there are any questions, please contact the State NFIP Coordinator, Ms. Carol Tyeu-Beam, of the Department of Land and Natural Resources, Engineering Division at (808) 587-0267.
- Please be advised that 44CFR indicates the minimum standards set forth by the NFIP. Your Community's local flood ordinance may prove to be more restrictive and thus take precedence over the minimum NFIP standards. If there are questions regarding the local flood ordinances, please contact the applicable County NFIP Coordinators below:
- () Mr. Robert Sumamoto at (808) 523-4254 or Mr. Mario Sui Li at (808) 523-4247 of the City and County of Honolulu, Department of Planning and Permitting.
 - () Mr. Kelly Gomez at (808) 961-8327 (Hilo) or Mr. Kiran Enler at (808) 327-3530 (Kona) of the County of Hawaii, Department of Public Works.
 - () Mr. Francis Cerizo at (808) 210-7771 of the County of Maui, Department of Planning.
 - () Mr. Mario Antonio at (808) 241-6620 of the County of Kauai, Department of Public Works.

- () The applicant should include project water demands and infrastructure required to meet water demands. Please note that the implementation of any State-sponsored projects requiring water service from the Honolulu Board of Water Supply system must first obtain water allocation credits from the Engineering Division before it can receive a building permit and/or water meter.
- () The applicant should provide the water demands and calculations to the Engineering Division so it can be included in the State Water Projects Plan Update.
- () Additional Comments: _____
- (X) Other: Our comments dated May 25, 2004, which was attached to the Draft Environmental Assessment document still apply except, Flood Zone X should be changed to Flood Zone D.

Should you have any questions, please call Mr. Andrew Monion of the Planning Branch at 587-0229.

Signed:
 ERIC T. HIRANO, CHIEF ENGINEER
 Date: 1/6/05

Mr. Warren Wegesend
 Administrator
 Land Division
 Department of Land and Natural Resources
 State of Hawaii
 P.O. Box 621
 Honolulu, Hawaii 96809

Dear Mr. Wegesend:

Subject: Waipio Point Access Road Improvements Draft Environmental Assessment

Thank you for providing your comments in your letter dated February 1, 2005 on the Waipio Point Access Road Draft Environmental Assessment (EA).

Your letter informed us that the flood plain designation of the project site is Zone D. This information was disclosed properly in the Draft EA.

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

Sincerely,

EDWARD Y. HIRATA
 Director

LINDA LINDALE
DEPARTMENT OF HAWAII



STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES
HISTORIC PRESERVATION DIVISION
KAKUHIHEWA BUILDING, ROOM 555
601 KAMOKILA BOULEVARD
HONOLULU, HAWAII 96707

PETER L. YOUNG
DIRECTOR
DEPARTMENT OF LAND AND NATURAL RESOURCES
HISTORIC PRESERVATION DIVISION
KAKUHIHEWA BUILDING, ROOM 555
601 KAMOKILA BOULEVARD
HONOLULU, HAWAII 96707



MARK HANAUSS
MANAGER

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU
650 SOUTH KING STREET, 3RD FLOOR - HONOLULU, HAWAII 96813
TELEPHONE: (808) 932-4328 • FAX: (808) 932-4730 • INTERNET: www.honolulu.gov

EDWARD Y. HIRATA
DIRECTOR

TP205-93246R

May 5, 2005

January 27, 2005

George Miyamoto, Director
Department of Transportation Services
City and County of Hawaii
650 South King Street, 3rd Floor
Honolulu, Hawaii 96813

Ms. Melanie A. Chinen
Administrator
State Historic Preservation Division
Department of Land and Natural Resources
State of Hawaii
Kakuhihewa Building, Room 555
601 Kamokila Boulevard
Honolulu, Hawaii 96707

Dear Mr. Miyamoto:

SUBJECT: Chapter 6E-8 Historic Preservation Review - Draft Environmental Assessment (DEA)
For the Waipio Point Access Road Improvements
Waipio, Ewa, Oahu
TMK: (1)S-3-002

Thank you for the opportunity to comment on the DEA prepared for the proposed Waipio Point Access Road Improvement project. The proposed improvements include a paved parking area for NAVSEA Inactive Ships on Site Maintenance Office (NISMO) personnel and contractors; addressing drainage and flooding problems in the area; and providing bicycle and pedestrian facilities between the Oritia I. bicycle/pedestrian path and the entrance to the Waipio soccer park. Our review is based on historic reports, maps, and aerial photographs maintained at the State Historic Preservation Division; no field inspection was made of the project areas.

The DEA, in section 2.3.3 (Isotopic Properties), provides an adequate summary of the regulatory requirements, existing conditions and identification of historic sites within the project area. The DEA identifies that Loko Eo, a traditional Hawaiian fishpond is located beneath fill soils within the Ted Makalena golf course. Previous archaeological investigations of the fishpond has identified that the upper 21 feet of the existing ground has been disturbed and is comprised of fill soils.

We concur with the findings that fishpond sediments would not be affected by the proposed improvements. Ground disturbance required for the project will not extend beyond the depth of fill soils. Therefore we believe that no historic properties will be affected by this undertaking.

Should you have any questions, please feel free to call Mary Carney at 692-8027.

Aloha,
Melanie A. Chinen
Melanie A. Chinen, Administrator
State Historic Preservation Division

EJjen

Dear Ms. Chinen:

Subject: Waipio Point Access Road Improvements Draft Environmental Assessment

Thank you for your agency's review pursuant to Section 6E-8 of the Hawaii Revised Statutes as transmitted in your letter dated January 27, 2005. Your letter provided concurrence that the proposed project will not adversely affect historic properties, in particular fishpond sediments of Loko Eo. Because federal funding may be used for construction, we will soon coordinate with your office pursuant to Section 106 of the National Historic Preservation Act.

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final Environmental Assessment.

Sincerely,

Edward Y. Hirata
EDWARD Y. HIRATA
Director

LEO LINGKE
GOVERNOR



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

January 20, 2005

Mr. Edward Hirata
Director
Department of Transportation Services
City and County of Honolulu
650 South King Street, 3rd Floor
Honolulu, Hawaii 96813

Dear Mr. Hirata:

Subject: Waipio Point Access Road Improvements
Draft Environmental Assessment (DEA)

Thank you for your transmittal requesting our comments on the subject project.

Most of the proposed roadway improvements come under the jurisdiction of the City and County of Honolulu. The portion connecting to the OR & L Bicycle/Pedestrian Path is under our jurisdiction. Use and occupancy agreements will be required for all project utilities within the OR & L railroad right-of-way.

In addition to our departmental review and approval, we understand that work within the OR & L railroad right-of-way will need prior review by the Railroad Society and Historic Preservation Office.

We appreciate the opportunity to provide comments.

Very truly yours,

RODNEY K. HARAGA
Director of Transportation

RODNEY K. HARAGA
DIRECTOR

RODNEY K. HARAGA
DIRECTOR
DEPARTMENT OF TRANSPORTATION
869 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

IN REPLY REFER TO:

STP 8.1544

RECEIVED
35 JAN 25 12:09

DIRECTOR OF TRANSPORTATION
DEPARTMENT OF TRANSPORTATION
HONOLULU, HAWAII 96813-5097

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU
650 SOUTH KING STREET, 3RD FLOOR - HONOLULU, HAWAII 96813
TELEPHONE: (808) 533-4339 • FAX: (808) 533-4730 • INTERNET: www.dts.hawaii.gov



May 5, 2005

TP105-91634R

EDWARD Y. HIRATA
DIRECTOR

Mr. Rodney K. Haraga
Director
Department of Transportation
State of Hawaii
869 Punchbowl Street
Honolulu, Hawaii 96813

Dear Mr. Haraga:

Subject: Waipio Point Access Road Improvements Draft Environmental Assessment

Thank you for providing your comments in your letter dated January 20, 2005 on the Waipio Point Access Road Draft Environmental Assessment (DEA). We would like to provide the following responses to your comments, which have been numbered. A copy of your letter with the numbering is provided.

Comment 1: Thank you for this information.

Comment 2: We will coordinate use and occupancy agreements with the State Department of Transportation for all work within the OR&L Right-of-Way.

Comment 3: The Hawaiian Railway Society and the State Historic Preservation Division (SHPD) were provided copies of the Draft EA. SHPD completed its review pursuant to Section 6E-8 of the Hawaii Revised Statutes. Although the Railway Society did not provide comments, we will coordinate with them during the design phase so that the project does not preclude use of the right-of-way for planned railroad operations.

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

Sincerely,

EDWARD Y. HIRATA
Director

LINDA LINGGIE
Governor



STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
889 PUNCHBOWL STREET
HONOLULU, HAWAII 96813-5097

January 20, 2005

Mr. Edward Hirata
Director
Department of Transportation Services
City and County of Honolulu
650 South King Street, 3rd Floor
Honolulu, Hawaii 96813

Dear Mr. Hirata:

Subject: Waipio Point Access Road Improvements
Draft Environmental Assessment (DEA)

Thank you for your transmittal requesting our comments on the subject project.

1 Most of the proposed roadway improvements come under the jurisdiction of the City and County
2 of Honolulu. The portion connecting to the OR & L Bicycle/Pedestrian Path is under our
jurisdiction. Use and occupancy agreements will be required for all project utilities within the
3 OR & L railroad right-of-way.

In addition to our departmental review and approval, we understand that work within the
OR & L railroad right-of-way will need prior review by the Railroad Society and Historic
Preservation Office.

We appreciate the opportunity to provide comments.

Very truly yours,

RODNEY K. HARAGA
Director of Transportation

RODNEY K. HARAGA
DIRECTOR
Deputy Directors
BRUCE Y. MATSUDA
BARRY FUCHIMAGA
BRUNN H. BERGBOUCH

IN REPLY REFER TO:

STP 8.1544

RECEIVED
35 JAN 25 12:09

DIRECTOR'S OFFICE
DEPARTMENT OF TRANSPORTATION
TRANSPORTATION SERVICES

BOARD OF WATER SUPPLY

CITY AND COUNTY OF HONOLULU
630 SOUTH BERETANA STREET
HONOLULU, HI 96813



January 13, 2005

TO: EDWARD Y. HIRATA, DIRECTOR
DEPARTMENT OF TRANSPORTATION SERVICES
FROM:
CLIFFORD S. JAMILE, MANAGER AND CHIEF ENGINEER
BOARD OF WATER SUPPLY
ATTENTION: BRIAN SUZUKI
SUBJECT: Your Letter of December 21, 2004 on the Draft Environmental
Assessment for Waipio Point Access Road Improvements, TPD04-00616

Our comments of June 14, 2004, which are included in the Draft Environmental Assessment, are
still applicable.

If you have any questions, please contact Joseph Kaakua at 748-5442.

RECEIVED
35 JAN 14 11:38
DIRECTOR'S OFFICE
DEPARTMENT OF TRANSPORTATION
TRANSPORTATION SERVICES

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU
630 SOUTH KING STREET, 2ND FLOOR - HONOLULU, HAWAII 96813
TELEPHONE: (808) 525-5319 • FAX: (808) 525-4730 • INTERNET: www.cc.honolulu.hi.us



MUTI HANNANON
MAYOR

EDWARD Y. HIRATA
DIRECTOR

TP105-90527R

May 5, 2005

MEMORANDUM

TO: CLIFFORD S. JAMILE, MANAGER AND CHIEF ENGINEER
BOARD OF WATER SUPPLY

FROM: EDWARD Y. HIRATA, DIRECTOR

SUBJECT: WAIPIO POINT ACCESS ROAD IMPROVEMENTS
DRAFT ENVIRONMENTAL ASSESSMENT

Thank you for reviewing our Waipio Point Access Road Draft Environmental Assessment (EA).

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

EDWARD Y. HIRATA

DEPARTMENT OF DESIGN AND CONSTRUCTION
CITY AND COUNTY OF HONOLULU
630 SOUTH KING STREET, 1ST FLOOR
HONOLULU, HAWAII 96813
PHONE: (808) 525-4344 • FAX: (808) 525-4547
WEB SITE ADDRESS: www.cc.honolulu.hi.us



MUTI HANNANON
MAYOR

TIMOTHY E. STEINBERGER, P. E.
ACTING DIRECTOR

WALTER HIRATA, P. E.
CHIEF ENGINEER

TIMOTHY E. STEINBERGER, P. E.
ACTING DIRECTOR

WALTER HIRATA, P. E.
CHIEF ENGINEER

RECEIVED
JAN 24 8:23
DEPARTMENT OF DESIGN AND CONSTRUCTION

January 21, 2005

MEMORANDUM

TO: MR. EDWARD Y. HIRATA, ACTING DIRECTOR
DEPARTMENT OF TRANSPORTATION SERVICES

FROM:
TIMOTHY E. STEINBERGER, P. E., ACTING DIRECTOR
DEPARTMENT OF DESIGN AND CONSTRUCTION

SUBJECT: DRAFT ENVIRONMENTAL ASSESSMENT
WAIPIO POINT ACCESS ROAD IMPROVEMENTS

We have reviewed the above Draft EA and have no objections or comments to offer regarding the project.

Thank you for the opportunity to comment.

Should you have any questions, please contact Gregory Sue at extension 6304.

GS:dk

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU
434 SOUTH KING STREET, 3RD FLOOR • HONOLULU, HAWAII 96813
TELEPHONE: (808) 533-4333 • FAX: (808) 523-4750 • INTERNET: www.honolulu.gov



HALEIUA MAIYOR

EDWARD Y. HIRATA
DIRECTOR

TP105-91467R

May 5, 2005

MEMORANDUM

TO: WAYNE HASHIRO, DIRECTOR
DEPARTMENT OF DESIGN AND CONSTRUCTION

FROM: EDWARD Y. HIRATA, DIRECTOR

SUBJECT: WAIPIO POINT ACCESS ROAD IMPROVEMENTS
DRAFT ENVIRONMENTAL ASSESSMENT

Thank you for your agency's review of our Waipio Point Access Road Draft Environmental Assessment (EA).

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

EDWARD Y. HIRATA

DEPARTMENT OF FACILITY MAINTENANCE
CITY AND COUNTY OF HONOLULU
1000 ULUOHA STREET, SUITE 215, KUALA IHA, HI 96707
PHONE: (808) 533-4333 • FAX: (808) 582-3857
Website: www.honolulu.gov



HALEIUA MAIYOR

LAVERNE HIGA, P.E.
ACTING DIRECTOR AND CHIEF ENGINEER

BY REPLY REFER TO: DFM 05 008

RECEIVED
05 JAN 11 P 2: 27

DIRECTOR GENERAL
DEPARTMENT OF FACILITY MAINTENANCE

January 7, 2005

MEMORANDUM

TO: ED HIRATA, ACTING DIRECTOR
DEPARTMENT OF TRANSPORTATION SERVICES

FROM:
LAVERNE HIGA, P.E., ACTING DIRECTOR AND CHIEF ENGINEER
DEPARTMENT OF FACILITY MAINTENANCE

SUBJECT: WAIPIO POINT ACCESS ROAD IMPROVEMENTS
DRAFT ENVIRONMENTAL ASSESSMENT

We have the following comments regarding the above subject:

- Page S-3
Suggest that the drainage facilities constructed be designed to accommodate the 100-year storm event, not the 50-year storm event.
- Page S-5, Cross Section A
Suggest that the sidewalk be elevated above the roadway grade to deter vehicular traffic on the sidewalk and as a pedestrian safety feature.
- Page S-8
The proposed re-designation of property from the golf course to the roadway right-of-way is not necessary. The drainage primarily serves the golf course and should remain with the golf course so as to have clear maintenance responsibility.
- Page S-9
The Navy's abandoned JP-S pipeline should be evacuated of all oils and contaminants so as to lessen the possible future oil contamination of the environment. Suggest that the new drainage structure and systems be designed to 100-year event, not the proposed 50-year event.

If you require further clarification of our comments, please contact Larry Leopardi at 484-7605.

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU
 150 SOUTH KING STREET, 2ND FLOOR • HONOLULU, HAWAII 96813
 TELEPHONE: (808) 522-4529 • FAX: (808) 523-4720 • INTERNET: www.honolulu.gov



MURTI MANIKAVANAN
 MAYOR

EDWARD Y. HIRATA
 DIRECTOR

TP-105-90074R

May 5, 2005

MEMORANDUM

TO: LAYERNE HIGA, P.E., DIRECTOR AND CHIEF ENGINEER
 DEPARTMENT OF FACILITY MAINTENANCE

FROM: EDWARD Y. HIRATA, DIRECTOR

SUBJECT: WAIPLO POINT ACCESS ROAD IMPROVEMENTS
 DRAFT ENVIRONMENTAL ASSESSMENT

Thank you for providing your comments in your memorandum dated January 7, 2005 on the Waiplo Point Access Road Draft Environmental Assessment (EA). We would like to provide the following responses to your comments, which have been numbered. A copy of your letter with the numbering is provided.

- Comment 1: We will be following the City and County of Honolulu Drainage Standards, which require drainage structures to be designed to accommodate a 50-year storm event.
 - Comment 2: We will address your concerns regarding the safety of pedestrians along Waiplo Point Access Road in the Final EA.
 - Comment 3: Thank you for your comment. This issue will be discussed and addressed in the Final EA.
 - Comment 4: The Navy is currently monitoring two plumes of petroleum contamination through use of groundwater monitoring wells. The petroleum contamination is being remediated through use of sorbent socks with monitored natural attenuation.
 - Comment 5: Please see response to Comment 1.
- We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

EDWARD Y. HIRATA

DEPARTMENT OF FACILITY MAINTENANCE
CITY AND COUNTY OF HONOLULU
 1000 Uluohia Street, Suite 215, Honolulu, Hawaii 96707
 Phone: (808) 582-5054 • Fax: (808) 582-5857
 Website: www.honolulu.gov



MURTI MANIKAVANAN
 MAYOR

LAYERNE HIGA, P.E.
 ACTING DIRECTOR AND CHIEF ENGINEER

IN REPLY, REFER TO: DMM 05-008

RECEIVED
 05 JAN 11 P2:27
 DIRECTOR'S OFFICE
 DEPARTMENT OF
 TRANSPORTATION SERVICES

January 7, 2005

MEMORANDUM

TO: ED HIRATA, ACTING DIRECTOR
 DEPARTMENT OF TRANSPORTATION SERVICES

FROM: LAYERNE HIGA, P.E., ACTING DIRECTOR AND CHIEF ENGINEER
 DEPARTMENT OF FACILITY MAINTENANCE

SUBJECT: WAIPLO POINT ACCESS ROAD IMPROVEMENTS
 DRAFT ENVIRONMENTAL ASSESSMENT

We have the following comments regarding the above subject:

- 1 | Page S-3
 Suggest that the drainage facilities constructed be designed to accommodate the 100-year storm event, not the 50-year storm event.
- 2 | Page S-5, Cross Section A
 Suggest that the sidewalk be elevated above the roadway grade to deter vehicular traffic on the sidewalk and as a pedestrian safety feature.
- 3 | Page S-8
 The proposed re-designation of property from the golf course to the roadway right-of-way is not necessary. The drainage primarily serves the golf course and should remain with the golf course so as to have clear maintenance responsibility.
- 4 | Page S-9
 The Navy's abandoned JP-S pipeline should be evacuated of all oils and contaminants so as to lessen the possible future oil contamination of the environment. Suggest that the new drainage structure and systems be designed to 100-year event, not the proposed 50-year event.
- 5 |

If you require further clarification of our comments, please contact Larry Leopardi at 484-7605.

DEPARTMENT OF PARKS AND RECREATION
CITY AND COUNTY OF HONOLULU

1000 ULUKOUA STREET, SUITE 309 - KAPOLEI, HAWAII 96707
TELEPHONE: (808) 432-5561 • FAX: (808) 432-5151 • INTERNET: www.cc.honolulu.hi.us



RUZL HANDELMAN
MANAGER

January 6, 2005

TO: EDWARD Y. HIRATA, ACTING DIRECTOR
DEPARTMENT OF TRANSPORTATION SERVICES

FROM: WILLIAM D. BALFOUR, JR., DIRECTOR

SUBJECT: DRAFT ENVIRONMENTAL ASSESSMENT
WAIPIO POINT ACCESS ROAD IMPROVEMENTS

Thank you for the opportunity to review and comment on the Draft Environmental Assessment relating to the Waipio Point Access Road Improvements.

The Department of Parks and Recreation has no comment on this project. Should you have any questions, please contact Mr. John Reid, Planner, at 692-5454.


WILLIAM D. BALFOUR, JR.
Director

WDB:mk
(4/26)

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU

430 SOUTH KING STREET, 3RD FLOOR - HONOLULU, HAWAII 96813
TELEPHONE: (808) 523-6227 • FAX: (808) 523-4730 • INTERNET: www.cc.honolulu.hi.us



MARU HANDELMAN
MANAGER

May 5, 2005

MEMORANDUM

TO: LESTER K.C. CHANG, DIRECTOR
DEPARTMENT OF PARKS AND RECREATION

FROM: EDWARD Y. HIRATA, DIRECTOR

SUBJECT: WAIPIO POINT ACCESS ROAD IMPROVEMENTS
DRAFT ENVIRONMENTAL ASSESSMENT

Thank you for your agency's review of our Waipio Point Access Road Draft Environmental Assessment (EA).

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.


EDWARD Y. HIRATA

EDWARD Y. HIRATA
DIRECTOR

TP1/05-89720R

WILLIAM D. BALFOUR, JR.
DIRECTOR

DEPUTY DIRECTOR

RECEIVED

05 JAN 10 8:06

DIRECTOR OF THE
DEPARTMENT OF
TRANSPORTATION SERVICES

DEPARTMENT OF PLANNING AND PERMITTING
CITY AND COUNTY OF HONOLULU
650 SOUTH KING STREET, 7TH FLOOR • HONOLULU, HAWAII 96813
PHONE: (808) 523-4432 • FAX: (808) 527-8143 • CITY WEB SITE: www.honolulu.gov



LEA HIRATA
MAYOR

RECEIVED
DEPARTMENT OF PLANNING AND PERMITTING
CITY AND COUNTY OF HONOLULU
JAN 28 2005
2004/ELOG-2874(BA)32

January 28, 2005

TO: EDWARD Y. HIRATA, ACTING DIRECTOR
DEPARTMENT OF TRANSPORTATION SERVICES

FROM: HENRY ENG, FAICP, ACTING DIRECTOR
DEPARTMENT OF PLANNING AND PERMITTING

SUBJECT: WAIPIO POINT ACCESS ROAD IMPROVEMENTS - WAIPIHU
DRAFT ENVIRONMENTAL ASSESSMENT REVIEW

Thank you for the opportunity to review the Draft Environmental Assessment for the proposed improvements to Waipio Point Access Road. We offer the following comments:

Special Management Area
The DEA correctly states that the project is within the Special Management Area. Please provide information as to whether the project is also located within the Shoreline Seaback area.

General Plan
Page 2-25: The second bullet statement should indicate that the project is consistent with the priority the General Plan places on improving existing roads and utilities, especially correcting substandard or unsafe conditions. (Transportation and Utilities policies D-1 and A-5)

Central Oahu Sustainable Communities Plan
Page 2-25: The DEA adequately covers the Sustainable Communities Plan's (SCP) planning principle of community integration for parks (section 3-2.2, 3rd bullet). The EA should mention that the project is already shown on the Central Oahu Public Infrastructure Map, as an element of the Waipio Peninsula Recreation Complex PIM project.

Waipahu Livable Communities Initiative (May 1998)
The Initiative recommends the provision of pedestrian/bikeway access and facilities along Waipio Point Access Road, linking the community to the recreational activities of Waipio Peninsula.

Edward Y. Hirata, Acting Director
Department of Transportation Services
Page 2

Pearl Harbor Historic Trail Master Plan (May 2001)
The project provides an opportunity to enhance the Trail's goal of becoming a world-class heritage and recreational facility. The Pearl Harbor Historic Trail (PHHT) Master Plan identifies various trail connections as being priority projects, including this connection from the Waipio Point Access Road to the soccer park.

SHARED-USE PATH. The project should be designed to enhance the accessibility of the PHHT, Makalena Golf Course, and the soccer park. The PHHT Master Plan recommends an off-road shared-use path that is acceptable for walking, jogging and bicycling (10-12 feet in paved width). The proposal should consolidate the bike lanes and sidewalk widths to create a single shared-use path along one side of the road way for the length of the project (providing a consistent cross-section). Redirecting bike travel to a shared path (away from the travel lane and maneuvering area of the parking lot) would provide a safer route for cyclists of all ages and increase usage traveling to and from the soccer park. The shared-use path concept would substantially increase the existing narrow 30-inch pathway on the Loch-side of Cross Section C.

LANDSCAPE AND IRRIGATION. The EA should discuss general landscape material, including the provision of street trees, and the irrigation system.

OTHER

PARKING. The Department of Transportation Services (DTS) should consider a combination of the two alternatives. A portion of the parking could be provided along Waipio Point Access Road, which would include a landscaped separation between the parking area and travel way with diagonal parking, and the balance of the parking provided in the area adjacent to the existing Ted Makalena Golf Course parking lot. Due to increase in activities associated with the Waipio Point Soccer Complex and the increase in traffic along the Access Road, we recommend not permitting or discouraging motorists from reversing onto the vehicular travel way. The Cross Section diagrams should include location of property lines.

The project descriptions on pages S-3 and 1-8 should include the planned increase in parking spaces from 75 to about 90.

The DTS may want to clarify the extent of Waipio Point Access Road. Sections of the DEA indicate the road dead-ends just past the entrance to the soccer park (bottom of page 1-1, page 3 of Appendix E), however pages 36-39 of Appendix E indicate that the road extends further.

Two other land uses located along this stretch of road (the FAA site and Hokuhihi Landscaping) should be disclosed on the bottom of page 1-1.

Edward Y. Hirata, Acting Director
Department of Transportation Services
Page 3

We have enclosed a copy of the PHHT Master Plan (compact disc) for your reference. Should you have any questions, please do not hesitate to contact Bonnie Arakawa of our staff at 527-5837.

HE:lh
Doc: 3/7710
Enclosure

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU

650 SOUTH KING STREET, 3RD FLOOR - HONOLULU, HAWAII 96813
TELEPHONE: (808) 523-4319 • FAX: (808) 523-4730 • INTERNET: www.honolulu.gov



KAU'I HAKAMAHAU
MAYOR

EDUARDO Y. HIRATA
DIRECTOR

TP1105-92285R

May 5, 2005

Mr. Henry Eng, Director
Department of Planning and Permitting
City and County of Honolulu
650 South King Street, 7th Floor
Honolulu, Hawaii 96813

Dear Mr. Eng:

Subject: Waipio Point Access Road Improvements Draft Environmental Assessment

Thank you for providing your comments in your memorandum dated January 28, 2005 on the Waipio Point Access Road Draft Environmental Assessment. We would like to provide the following responses to your comments, which have been numbered. A copy of your letter with the numbering is provided.

Comment 1: Section 2.5.2.4 will be revised in the Final EA will to state that portions of the project will be within the shoreline setback. Also, Section 2.6 will be revised in the Final EA to disclose that the project will require a Shoreline Setback Variance.

Comment 2: Section 2.5.2.1 will be revised in the Final EA in accordance with this comment.

Comment 3: Section 2.5.2.2 will be revised in the Final EA in accordance with this comment.

Comment 4: Thank you for this information.

Comment 5: Thank you for this information.

Comment 6: We will consider your suggestion regarding a shared-use path along Waipio Point Access Road as well as other options prior to the issuance of the Final EA.

Comment 7: As stated in Section 1.3.3 of the Draft EA, landscaping will be provided as part of other improvements of the project. The details of landscaping, such as specific plantings and irrigation, will be determined during design.



Mr. Henry Eng, Director
Page 2
May 4, 2005

Comment 8: The alternative of providing parking along Waipio Point Access road parked diagonal to the roadway and separated from the road with a landscaping strip was considered but deemed not feasible mainly because it would require a half-acre of "playable" land from the Ted Makalena Golf Course (please see Section 1.4.1 of the Draft EA for additional information). The alternative of providing parking for Navy personnel in the vicinity of the Ted Makalena Golf Course Parking lot was considered unacceptable by the Navy (please see Section 1.4.2 of the Draft EA for additional information). Therefore, a combination of the two alternatives was not considered.

Comment 9: The cross sections will be revised to indicate that the property lines are the Ted Makalena Golf Course fence line and the NAVSEA Inactive Ships On-Site Maintenance Facility fence line.

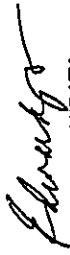
Comment 10: Sections S.3 and 1.3.1 will be revised in the Final EA in accordance with the comment.

Comment 11: The Final EA will be revised to clarify the extent of Waipio Point Access Road.

Comment 12: Section 1.1.1 will be revised in the Final EA in accordance with the comment.

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

Sincerely,


EDWARD Y. HIRATA
Director

DEPARTMENT OF PLANNING AND PERMITTING
CITY AND COUNTY OF HONOLULU
650 SOUTH KING STREET, 11TH FLOOR • HONOLULU, HAWAII 96813
PHONE: (808) 525-4422 FAX: (808) 525-6163
CITY WEB SITE: WWW.HONOLULU.HI.GOV



MAHINA HUNGALU
MAILING

January 28, 2005

TO: EDWARD Y. HIRATA, ACTING DIRECTOR
DEPARTMENT OF TRANSPORTATION SERVICES

FROM: HENRY ENG, FAICP, ACTING DIRECTOR
DEPARTMENT OF PLANNING AND PERMITTING

SUBJECT: WAIPIO POINT ACCESS ROAD IMPROVEMENTS - WAIPAHI
DRAFT ENVIRONMENTAL ASSESSMENT REVIEW

Thank you for the opportunity to review the Draft Environmental Assessment for the proposed improvements to Waipio Point Access Road. We offer the following comments:

- 1 Special Management Area
The DEA correctly states that the project is within the Special Management Area. Please provide information as to whether the project is also located within the Shoreline Setback area.
- 2 General Plan
Page 2-25: The second bullet statement should indicate that the project is consistent with the priority the General Plan places on improving existing roads and utilities, especially correcting substandard or unsafe conditions. (Transportation and Utilities policies D-1 and A-5)
- 3 Central Oahu Sustainable Communities Plan
Page 2-25: The DEA adequately covers the Sustainable Communities Plan's (SCP) planning principle of community integration for parks (section 3-2.2, 3rd bullet). The EA should mention that the project is already shown on the Central Oahu Public Infrastructure Map, as an element of the Waipio Peninsula Recreation Complex PIM project.
- 4 Waipahu Livable Communities Initiative (May 1998)
The Initiative recommends the provision of pedestrian/bikeway access and facilities along Waipio Point Access Road, linking the community to the recreational activities of Waipio Peninsula.

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DEPARTMENT OF TRANSPORTATION SERVICES
2004/EL051-2874(BA)32
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P12

Edward Y. Hirata, Acting Director
Department of Transportation Services
Page 3

We have enclosed a copy of the PHHT Master Plan (compact disc) for your reference. Should you have any questions, please do not hesitate to contact Bonnie Arakawa of our staff at 527-5837.

HE:lb
Doc: 3/7/18
Enclosure

Edward Y. Hirata, Acting Director
Department of Transportation Services
Page 2

5 Pearl Harbor Historic Trail Master Plan (May 2001)
The project provides an opportunity to enhance the Trail's goal of becoming a world-class heritage and recreational facility. The Pearl Harbor Historic Trail (PHHT) Master Plan identifies various trail connections as being priority projects, including this connection from the Waipio Point Access Road to the soccer park.

6 SHARED-USE PATH. The project should be designed to enhance the accessibility of the PHHT, Makalena Golf Course, and the soccer park. The PHHT Master Plan recommends an off-road shared-use path that is acceptable for walking, jogging and bicycling (10-12 feet in paved width). The proposal should consolidate the bike lanes and sidewalk widths to create a single shared-use path along one side of the road way for the length of the project (providing a consistent cross-section). Redirecting bike travel to a shared path (away from the travel lane and maneuvering area of the parking lot) would provide a safer route for cyclists of all ages and increase usage traveling to and from the soccer park. The shared-use path concept would substantially increase the existing narrow 30-inch pathway on the Loch-side of Cross Section C.

7 LANDSCAPE AND IRRIGATION. The EA should discuss general landscape material, including the provision of street trees, and the irrigation system.

OTHER

8 PARKING. The Department of Transportation Services (DTS) should consider a combination of the two alternatives. A portion of the parking could be provided along Waipio Point Access Road, which would include a landscaped separation between the parking area and travel way with diagonal parking, and the balance of the parking provided in the area adjacent to the existing Ted Makalena Golf Course parking lot. Due to increase in activities associated with the Waipio Point Soccer Complex and the increase in traffic along the Access Road, we recommend not permitting or discouraging motorists from reversing onto the vehicular travel way.

9 The Cross Section diagrams should include location of property lines.

10 The project descriptions on pages S-3 and 1-8 should include the planned increase in parking spaces from 75 to about 90.

11 The DTS may want to clarify the extent of Waipio Point Access Road. Sections of the DEA indicate the road dead-ends just past the entrance to the soccer park (bottom of page 1-1, page 3 of Appendix B), however pages 36-39 of Appendix B indicate that the road extends further.

12 Two other land uses located along this stretch of road (the FAA site and Hokuahi Landscaping) should be disclosed on the bottom of page 1-1.

FIRE DEPARTMENT
CITY AND COUNTY OF HONOLULU
 3075 Koaeha Street, Suite 1142S
 Honolulu, Hawaii 96819
 Phone: (808) 831-7778 Fax: (808) 831-7750 Internet: www.honolulu.gov



January 12, 2005

ATILIOK LEONARDI
 FIRE CHIEF
 JOHN CLARK
 DEPUTY FIRE CHIEF

RECEIVED
 05 JAN 14 12:45
 DIRECTOR'S OFFICE
 DEPARTMENT OF TRANSPORTATION SERVICES

TO: EDWARD Y. HIRATA, ACTING DIRECTOR
 DEPARTMENT OF TRANSPORTATION SERVICES

FROM: JOHN CLARK, ACTING FIRE CHIEF

SUBJECT: DRAFT ENVIRONMENTAL ASSESSMENT (DEA)
 WAIPIO POINT ACCESS ROAD IMPROVEMENTS
 WAIPIHU, OAHU, HAWAII

We received a memorandum dated December 21, 2004, from George "Keoki" Miyamoto, former Director of the Department of Transportation Services, requesting our review and comments on the above-mentioned DEA.

The Honolulu Fire Department requires that the following be complied with for the duration of the project:

1. Maintain fire apparatus access throughout the construction site.
2. Notify the Fire Communication Center at 523-4111 regarding any interruption of the existing fire hydrant system.

Should you have any questions, please call Battalion Chief Lloyd Rogers of our Fire Prevention Bureau at 831-7778.

John Clark
 JOHN CLARK
 Acting Fire Chief

JC/SK:hh

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU
 630 SOUTH KING STREET, 3RD FLOOR • HONOLULU, HAWAII 96813
 TELEPHONE: (808) 523-4319 • FAX: (808) 531-4750 • INTERNET: www.ctd.honolulu.gov



May 5, 2005

TP105-90506R

EDWARD Y. HIRATA
 DIRECTOR

MEMORANDUM

TO: ATTILIO LEONARDI, FIRE CHIEF
 FIRE DEPARTMENT

FROM: EDWARD Y. HIRATA, DIRECTOR

SUBJECT: WAIPIO POINT ACCESS ROAD IMPROVEMENTS
DRAFT ENVIRONMENTAL ASSESSMENT

Thank you for your agency's comments in the memorandum dated January 12, 2005 on the Waipio Point Access Road Draft Environmental Assessment (EA).

Your first comment asked that the access along Waipio Point Access Road be maintained throughout construction for emergency services. As stated in Section 2.4.1 of the Draft EA, at least one lane will be open at all times during construction.

Your second comment asked that the Fire Department be informed about any interruption to the fire hydrant system. We will comply with this request.

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

Edward Y. Hirata
 EDWARD Y. HIRATA

POLICE DEPARTMENT
CITY AND COUNTY OF HONOLULU
801 SOUTH BERETANIA STREET
HONOLULU, HAWAII 96813 - AREA CODE (808) 528-3111
<http://www.honolulu.gov>



BOISSE P. CORREA
CHIEF
GLEN S. BAUMANN
POLICE
DEPUTY CHIEF

RECEIVED
JAN 12 9:07
DIRECTOR'S OFFICE
DEPT. OF
TRANSPORTATION SERVICES

January 10, 2005

DUFI WARRMANN
MAYOR

OUR REFERENCE CS-KP

DEPARTMENT OF TRANSPORTATION SERVICES
CITY AND COUNTY OF HONOLULU
640 SOUTH KING STREET, 3RD FLOOR • HONOLULU, HAWAII 96813
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BOISSE P. CORREA
CHIEF

May 5, 2005

TP1105-90136R

EDWARD Y. HIRATA
DIRECTOR

MEMORANDUM

TO: BOISSE P. CORREA, CHIEF
POLICE DEPARTMENT

FROM: EDWARD Y. HIRATA, DIRECTOR

SUBJECT: WAIPIO POINT ACCESS ROAD IMPROVEMENTS
DRAFT ENVIRONMENTAL ASSESSMENT

Thank you for reviewing our Waipio Point Access Road Draft Environmental Assessment (EA).

We will be issuing a Finding of No Significant Impact for the subject project and are currently in the process of completing the Final EA.

Edward Y. Hirata
EDWARD Y. HIRATA

TO: EDWARD Y. HIRATA, ACTING DIRECTOR
DEPARTMENT OF TRANSPORTATION SERVICES

FROM: BOISSE P. CORREA, CHIEF OF POLICE
HONOLULU POLICE DEPARTMENT

SUBJECT: DRAFT ENVIRONMENTAL ASSESSMENT, WAIPIO POINT ACCESS ROAD IMPROVEMENTS

Thank you for the opportunity to review and comment on the subject project.

This project should have minimal impact on the services and facilities of the Honolulu Police Department. However, there may be inevitable construction-related dust, noise, and traffic complaints. As related in section 2.4.1, the use of flagmen and/or police officers to direct traffic safely around lane closures (including during major tournaments at the soccer park) and other potential safety hazards will help in minimizing construction impacts.

If there are any questions, please call Acting Major Randal Macadangdang of District 3 at 455-9055 or Ms. Carol Sodeatani of the Support Services Bureau at 529-3658.

BOISSE P. CORREA
Chief of Police

BY *Karl Godsey*
KARL GODSEY
Assistant Chief of Police
Support Services Bureau

Serving and Protecting with Aloha

APPENDIX F ■

WAPIO POINT ACCESS ROAD TRAFFIC EVALUATION

WAPIO POINT ACCESS ROAD Improvements

FINAL ENVIRONMENTAL ASSESSMENT

TRAFFIC EVALUATION

WAIPIO POINT ACCESS ROAD

WAIPAHU, OAHU, HAWAII

June 2005

Traffic Evaluation

WAIPIO POINT ACCESS ROAD

Waipahu, Oahu, Hawaii

June 2005

Prepared For:
City and County of Honolulu
Department of Transportation Services
650 South King Street 3rd Floor
Honolulu Hawaii 96813



Over a Century of Engineering Excellence

Prepared By:
Parsons Brinckerhoff Quade & Douglas, Inc.
Pacific Tower - Suite 3000
1001 Bishop Street
Honolulu, HI 96813
(808) 531-7094

PBOD Reference Number:
16391A

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I. INTRODUCTION

The City and County of Honolulu has completed phase 1 of the Waipio Peninsula Soccer Park (WPSP) and is planning to complete the entire project. The soccer park currently consists of 19 soccer fields, the first phase of the soccer stadium able to accommodate approximately 4,000 spectators, and locker rooms for participants. The WPSP is planned to be expanded over the coming years to include an additional 13 soccer fields, expansion of the soccer stadium to seat approximately 10,000 spectators, free standing concession stands and restrooms, media and medical facilities and administrative offices. Parking will be increased to accommodate up to 5,000 vehicles.

The only access to the WPSP is currently Waipio Point Access Road (WPAR). One of the key issues associated with the development of the WPSP is the adequacy of WPAR to handle existing and projected traffic generated by soccer-related activities. Currently, WPAR has two roadway lanes (one in each direction). There are limited pedestrian facilities and no bicycle facilities along WPAR. Additionally, there are issues associated with an existing bicycle path crossing of WPAR and parking issues at the Waiani Stream Bridge and at the NAVSEA Inactive Ships On-Site Maintenance Office (NISMO).

Figure 1 illustrates the general location of WPAR and the limits of the study area. As shown, the study area is defined by the OR&L bicycle/pedestrian walkway located mauka of Waiani Stream Bridge to the north, Waipio Peninsula Soccer Park entrance to the south, Pearl Harbor Middle Loch to the east and Ted Makalena Golf Course to the west.

The major purposes of this report are twofold. First, the ability of the WPAR to accommodate the traffic associated with the buildout of the WPSP is addressed. Second, locations along WPAR that have current traffic issues are identified and potential traffic-related improvements are recommended. Key locations along WPAR that are examined in this report are the existing OR&L bicycle/pedestrian walkway crossing of WPAR, the fishing area at the bridge over Waiani Stream, parking for the NAVSEA Inactive Ships On-Site Maintenance Office (NISMO), and the access to the Ted Makalena Municipal Golf Course.

All these analyses are based on traffic conditions expected during a major soccer tournament.

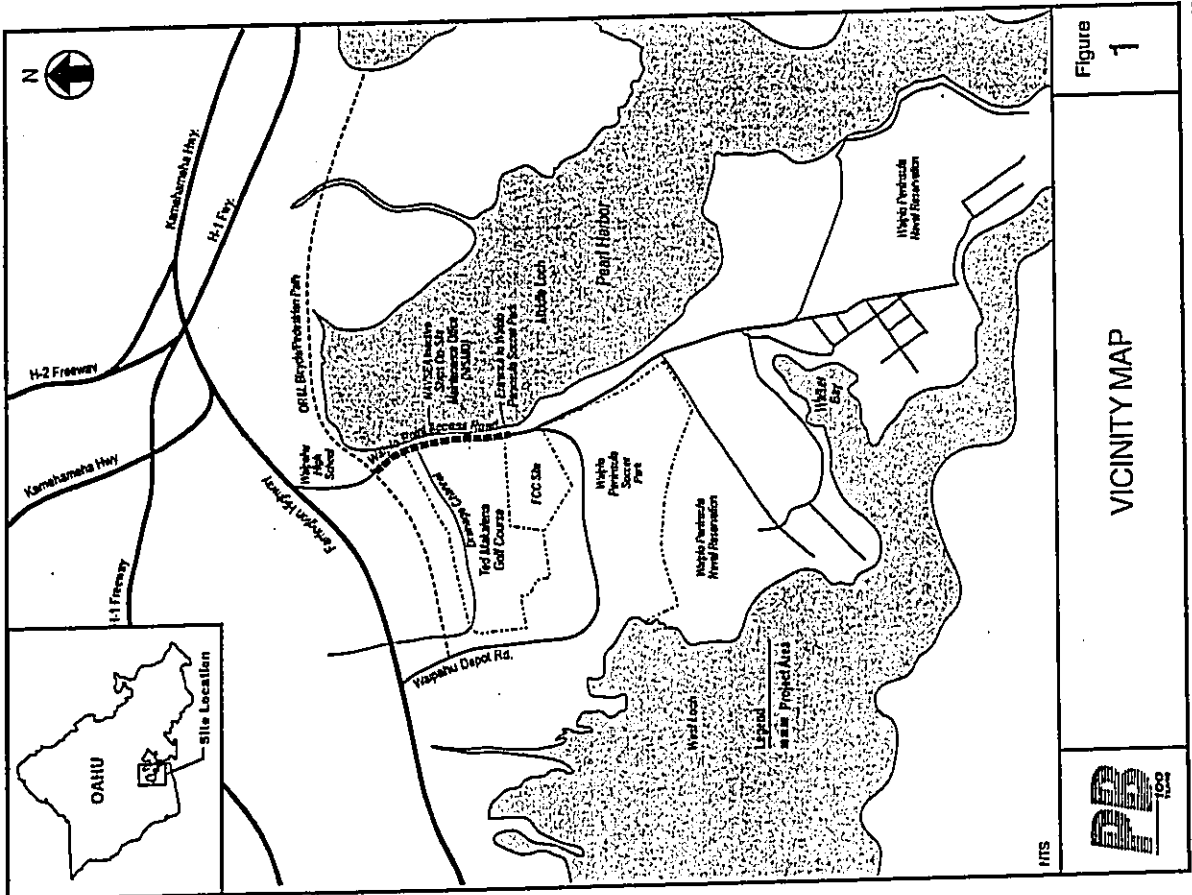


Figure 1
VICINITY MAP
 Waipio Point Access Road
 Traffic Evaluation
 June 2005

II. EXISTING CONDITIONS

A. Existing Roadway System

Farrington Highway provides primary sub-regional access to Waipio Peninsula Soccer Park (WPSP) via Waipio Point Access Road (WPAR). Just makal of Farrington Highway, WPAR provides access for Waipahu High School on its Koko Head-side and two residential neighborhoods on its Ewa-side. Further makal, WPAR crosses the bicycle/pedestrian walkway that travels along the historic Oahu Railway and Land Company (OR&L) right-of-way. The OR&L right-of-way defines the mauka boundary of the study area. Within the study area, WPAR provides access and mauka-makal circulation for the NAVSEA Inactive Ships On-Site Maintenance Office (NISMO), Ted Makalena Golf Course, and the WPSP. Figure 2 illustrates the existing roadways within the study area.

1. Farrington Highway

Farrington Highway is a major arterial roadway providing Ewa-Koko Head mobility in Leeward area of Oahu. The eastern terminus is just east of the Waiawa interchange where it merges with Kamehameha Highway, which continues past Pearl Harbor to Kaili. To the west, Farrington Highway provides regional mobility through Ewa, Kapolei and the Waianae Coast. In the Waipahu area, Farrington Highway is a 4-lane divided roadway from Fort Weaver Road to Kamehameha Highway with curb, gutter and sidewalks. The posted speed limit is 35 miles per hour in this area.

2. Waipio Point Access Road

Waipio Point Access Road (WPAR) is a two-lane collector road that intersects Farrington Highway across from Kahualii Street at a signalized intersection. It runs makal along the Koko Head shore of the Waipio Peninsula. Its makal terminus is located at the entrance to WPSP. WPAR provides access to Waipahu High School, two residential streets serving residential subdivisions Ewa of WPAR, NISMO, and the Ted Makalena Golf Course. The speed limit between the Waiwai Stream Bridge and the WPSP entrance is posted at 20 mph within the study area.

Table 1 Existing Waipio Peninsula Soccer Park Peak Hour Volumes*

Direction of Peak	Mauka Bound Traffic	Makal Bound Traffic	Time
Makal-bound Peak	165	483	8:30 AM - 10:30 AM
Mauka-bound Peak	639	23	6:30 PM - 7:30 PM
Two-way Total Peak	338	387	3:45 PM - 4:45 PM

*during major soccer tournament, volumes expressed as vehicles per hour

Table 2 Peak Hour Traffic Not Associated with Waipio Peninsula Soccer Park

Direction of Peak	Mauka Bound Traffic	Makal Bound Traffic	Time
Makal-bound Peak	23	141	6:00 AM - 7:00AM
Mauka-bound Peak	107	26	3:15 PM - 4:15 PM
Two-way Total Peak	23	141	6:00 AM - 7:00AM

volumes expressed as vehicles per hour

As shown in Tables 1 and 2, the mauka-bound or makal-bound peak traffic volumes associated with the WPSP do not coincide with the peak traffic volumes generated by other uses such as the NAVSEA Inactive Ships On-Site Maintenance Office (NISMO) or Ted Makalena Golf Course. The two-way total peak hour traffic for WPSP does overlap with the mauka-bound peak hour traffic associated with the other uses, but this two-way total peak is less intense than the mauka or makal-bound peaks associated with the WPSP.

Data from a comparable facility was unavailable and would likely have been useless. Insufficient data from similar facilities such as city/state parks were available to make any meaningful comparisons.

Table 3 summarizes the combined WPSP and other use peak hour traffic volumes on WPAR. These combined volumes were used to evaluate roadway capacity and to determine the lane requirements for WPAR.

Table 3 Existing Combined WPSP and Other Use Peak Hour Traffic

Volumes

Direction of Peak	Mauka Bound Traffic	Makal Bound Traffic	Time
Makal-bound	170	517	9:30 AM - 10:30 AM
Mauka-bound	670	28	6:30PM - 7:30 PM
Two-way Total Peak	394	420	3:30 PM - 4:30 PM

volumes expressed as vehicles per hour

Based on the combined WPSP and other use traffic volumes collected, the maximum makal-bound, mauka-bound, and two-way total peaks occur between 9:30AM to 10:30AM, 6:30PM to 7:30PM, and 3:30PM to 4:30PM, respectively. The existing traffic volumes are shown in Figure 3, and traffic data is included in appendix A.

C. Existing Roadway Capacity

The existing laneage requirement for Waipio Point Access Road was evaluated using existing roadway conditions and traffic volumes as input into the 2000 Highway Capacity Manual methodology for two-way, two-lane highways. This methodology characterizes the ability for a two-lane, undivided roadway segment to carry traffic by calculating operating parameters that are related to an index called Level-of-Service (LOS). LOS is represented by a letter designation ranging from A to F. LOS A represents free-flow operating conditions, while LOS F represents congested conditions. More detailed LOS definitions are included in Appendix B

Peak hour traffic volumes collected makal of Wailani Stream Bridge and documented in Table 3 were used to conduct the two-way, two-lane roadway analysis. These volumes include traffic generated by WPSP during a major soccer tournament and the other uses along WPAR. For the purposes of the two-way, two-lane roadway analysis, Waipio Point Access Road was categorized as a Class II type highway based on its collector function and the relatively short roadway segment. The Peak Hour Factor (PHF) was calculated to be 0.783, 0.826, and 0.817 for mauka, makal and two-way peak directions respectively based on existing traffic. Table 4 summarizes the existing roadway LOS.

III. FUTURE EXPANSION OF WAIPIO PENINSULA SOCCER PARK

Year 2025 was used as the analysis year for the ultimate build out of Waipio Peninsula Soccer Park (WPSP). The expansion of WPSP facilities are projected to be completed by this time period.

The ultimate configuration of WPSP includes construction of 13 additional soccer fields, expansion of the soccer stadium to accommodate an additional 6,000 spectators, training facilities, medical facilities and administration offices.

A. Projected Year 2025 Peak Hour Traffic Volumes

It is assumed that the existing conditions (excluding WPSP) served by WPAR will remain unchanged. Therefore, traffic from the NAVSEA Inactive Ships On-Site Maintenance Office (NISMO) and Ted Makalena Golf Course were maintained as summarized in the existing conditions section of this report.

The traffic associated with WPSP is expected to increase with the completion of the additional soccer fields and facilities. To estimate this increase, a linear relationship was formulated relating the existing WPSP traffic volume to the existing number of fields at the park. This relationship was used to project the 2025 buildout traffic volumes on WPAR. The future ancillary facilities proposed for WPSP (the training facility et al) were assumed to have a minimal effect on future traffic generated by WPSP.

Summing the additional WPSP traffic volumes and the existing traffic volumes gives the projected future traffic volumes summarized in Table 5.

Table 4 Waipio Point Access Road Existing LOS

Peak Direction	LOS	PTSF
Makal-Bound Peak	D	71.7%
Mauka-Bound Peak	D	78.2%
Two Way Peak	D	73.5%

PTSF = Percent Time Spent Following, a measure of operational mobility. Higher is more constrained.

The results of the two-way, two-lane analyses indicate that the two-lane configuration of WPAR is adequate to serve the existing traffic demand occurring during a major soccer tournament.

Appendix C contains worksheets of the two-way, two-lane highway analyses.

Table 5 Projected Buildout Traffic Volumes on WPAR*

Direction of Peak	Mauka Bound Traffic	Makai Bound Traffic
Makai-bound	280	855
Mauka-bound	1105	45
Two-way Total Peak	650	695

*During a major soccer tournament, includes other uses along WPAR
 Volumes are expressed as vehicles per hour.

B. Projected Buildout Roadway Operations

The future traffic volumes summarized in Table 5 were used to evaluate traffic operations for WPAR using the same two-way, two-lane roadway capacity methodology used in the existing conditions analysis. Table 6 summarizes the level of service (LOS) projected for WPAR at the buildout of WPSP. LOS is still maintained at a LOS D, the same LOS as existing conditions.

Table 6 Projected Buildout LOS on WPAR

Peak Direction	LOS	PTSF
Makai-Bound	D	77.6%
Mauka-Bound	D	78.9%
Two Way Total Peak	D	80.5%

PTSF = Percent Time Spent Following, a measure of operational mobility.
 Higher is more constrained.

Although traffic volumes increase in the mauka-bound and makai-bound direction by 435 and 338 vehicles per hour, respectively, the percent time spent following increased only slightly because vehicles were already constrained under existing conditions by the no passing zone extending the entire length of the study segment. The two-way directional peak experienced the highest increase of percent time spent following due to the highest cumulative volume of the peak hour traffic volumes. Appendix C contains worksheets of the projected future two-way, two-lane highway analyses.

Based on the results of the analyses, it is determined that the existing two lanes should be sufficient to handle the future traffic demand generated by the expansion of the WPSP.

IV. SUMMARY AND RECOMMENDATIONS FOR REQUIRED LANEAGE ON WPAR

Based on the forecast of buildout traffic volumes and future operational analyses, it is concluded that the existing two-lane cross-section of Waipio Point Access Road (WPAR) between Waipio Peninsula Soccer Park (WPSP) entrance and the OR&L bicycle/pedestrian walkway crossing mauka of Wailani Stream Bridge is capable of accommodating the additional future traffic volumes generated by the expansion of the WPSP.

Although the study segment of WPAR is projected to have the capacity to accommodate current and future traffic demands generated from WPSP, the segment of WPAR mauka of the study segment currently experiences operational difficulties at the Farrington Highway/WPAR intersection during major soccer tournaments. These operational difficulties result in traffic queues that occasionally extend makai into the study segment of WPAR.

The State of Hawaii is currently studying the Farrington/WPAR intersection along with the segment of WPAR between Farrington Highway and the OR&L right-of-way as part of its study to locate alternative access to Leeward Community College (LCC). Part of the scope of the LCC Alternative Access Study is to identify improvements to WPAR in this segment. Although the LCC Alternative Access Study has not proceeded far enough to provide preliminary conclusions to this study of WPAR, it is assumed that improvements necessary to use a segment of WPAR as alternative access to LCC would be sufficient to address operational issues associated with peak soccer tournaments at WPSP.

Another roadway network element that would benefit WPAR operations at Farrington Highway is the potential improvement to Waipahu Depot Road, enabling it to function as alternative access into the WPSP. The existence of another access into WPSP would reduce traffic at the Farrington/WPAR intersection, improving operations there. The City & County of Honolulu Department of Design and Construction (DDC) is currently conducting a study to evaluate the feasibility of doing this. Unfortunately, this study is also in its initial phase and could not offer input into this WPAR study.

Both the State of Hawaii and the DDC efforts indicate improvements will be considered at the Farrington Highway/WPAR intersection and Waipahu Depot Road. Therefore, the

operational issues at the Farrington Highway/WPAR intersection during major soccer tournaments have a high probability of being addressed in some form.

In summary, it is concluded that WPAR could remain a two-lane roadway makai of the OR&L right-of-way. Two-lane highway capacity analysis has indicated that a two-lane WPAR could accommodate the additional traffic generated by the expansion of the WPSP. Increasing the laneage on WPAR in the study segment would not address the issues at the Farrington Highway WPAR intersection.

Waipahu Depot Road is a desirable alternative access from a system redundancy perspective. If feasible and if implemented, it would improve operations on WPAR. This is a desirable but not a necessary improvement within the study segment. If the segment of WPAR between Farrington Highway and the OR&L right-of-way is improved as part of the LCC Alternative Access Study, conditions for users of the WPAR would be enhanced.

In addition to a capacity analysis of the WPAR corridor, localized improvements to WPAR were identified, and these specific improvements are discussed in the following chapter of this report.

V. OTHER WPAR IMPROVEMENTS

Although the roadway laneage evaluation for WPAR found that a two-lane cross-section would be adequate to handle existing and future traffic demand, other roadway improvements would be desirable along WPAR and at specific locations on WPAR.

A. Bicycle Lanes, Pedestrian Walkway, and Landscaping on WPAR

The most comprehensive recommended improvement along WPAR would be a new shared-use path on the Ewa side of WPAR extending from the OR&L bicycle/pedestrian walkway crossing to the WPSP entrance makai of the Hokuahi Lawns, Inc. driveway. The shared-use path will be 10-foot wide and be separated from WPAR by a 4-foot landscape buffer. The shared-use path would be at the same grade as the WPAR pavement and could be asphalt or compacted gravel. The 4-foot wide landscaped area would also be at the same grade as the WPAR pavement.

The proposed shared-use path will have implications at the other locations along WPAR where improvements are recommended, including the OR&L bicycle/pedestrian walkway crossing, the Wailani Stream Bridge, the NAVSEA Inactive Ships On-Site Maintenance Office (NISMO) parking, and the driveway to the Ted Makalena Golf Course. To better understand these implications, illustrations of existing and proposed typical roadway cross-sections and plan view illustrations are used. The locations of these are shown in Figure 3.

B. OR&L Bicycle/Pedestrian Crossing

The OR&L bicycle/pedestrian walkway crosses WPAR approximately 237 feet mauka of Wailani Stream Bridge. It extends just past McGrew Point Naval Housing in the Koko Head direction and to Waipahu Depot Road in the Ewa direction. Within the study area, the bicycle path parallels the OR&L right-of-way, deviating slightly to cross WPAR at grade, while the OR&L right-of-way crosses under WPAR mauka of the bicycle/pedestrian walkway. It also appears that part of the existing bicycle/pedestrian walkway serves as a secondary access road to the Waipahu High School sports fields.

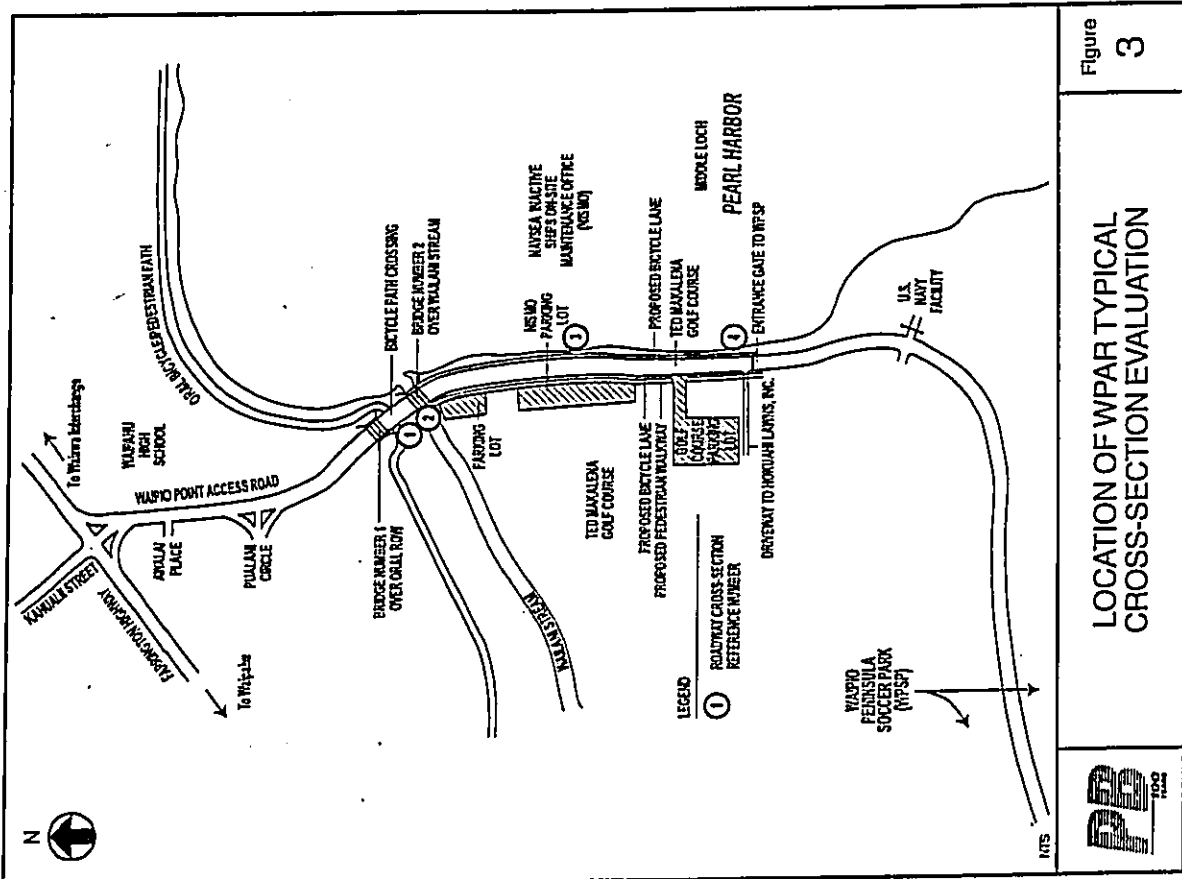


Figure 3
LOCATION OF WPAR TYPICAL CROSS-SECTION EVALUATION

During major soccer tournaments, traffic on WPAR at the at-grade bicycle/pedestrian walkway crossing was found to reach a total of 814 vehicles per hour (vph). Traffic is projected to reach 1,345 vph at buildout of the WPSF. These magnitudes of traffic volumes would make it difficult for bicycle riders and pedestrians to cross WPAR. Especially for buildout conditions, it may be desirable to utilize the existing grade-separation of the OR&L right-of-way to direct bicycle riders and pedestrians under WPAR. This would realign the bicycle/pedestrian walkway and eliminate crossing issues for bicyclists that continue through the WPAR area. The current bicycle crossing would be left in place to allow bicycle riders and pedestrians access to WPAR. The State of Hawaii Department of Transportation is expected to begin a design effort within the next year to improve the bicycle/pedestrian walkway in this area, and it is recommended to coordinate the proposed improvement with them.

Data was collected on July 5, 2003 to count the amount of bicycles utilizing the bicycle path. As shown in Table 7, roughly the same volume of bicycles enters from, exit to, and cross Waijio Point Access Road. Corresponding pedestrian data is shown in Table 8. Appendix A contains a summary of both counts. As the OR&L bicycle/pedestrian walkway is improved to provide more regional continuity and a better path surface, this activity level is expected to increase.

Table 7 Peak Hour Bicycle Path Activity

Direction	Bicycle Peak Hour Volume	Time of Peak	Day
Crossing WPAR	13	9:00 AM - 10:00 AM	Saturday
Entering from WPAR	14	9:00 AM - 10:00 AM	Saturday
Exiting to WPAR	17	9:00 AM - 10:00 AM	Saturday

Volume expressed as bicycles per hour

Table 8 Peak Hour Pedestrian Walkway Activity

Direction	Pedestrian Peak Hour Volume	Time of Peak	Day
Crossing WPAR	1	8:30 AM - 9:30 AM	Saturday
Entering from WPAR	3	8:30 AM - 9:30 AM	Saturday
Exiting to WPAR	6	8:30 AM - 9:30 AM	Saturday

Volume expressed as pedestrians per hour

There are existing bicycle crossing warning signs (W11-1) placed both mauka and makai of the bicycle crossing on WPAR. However, discussions with users of the bicycle path and people who frequent the area near the bicycle crossing indicate that visibility of the bicycle crossing is inadequate.



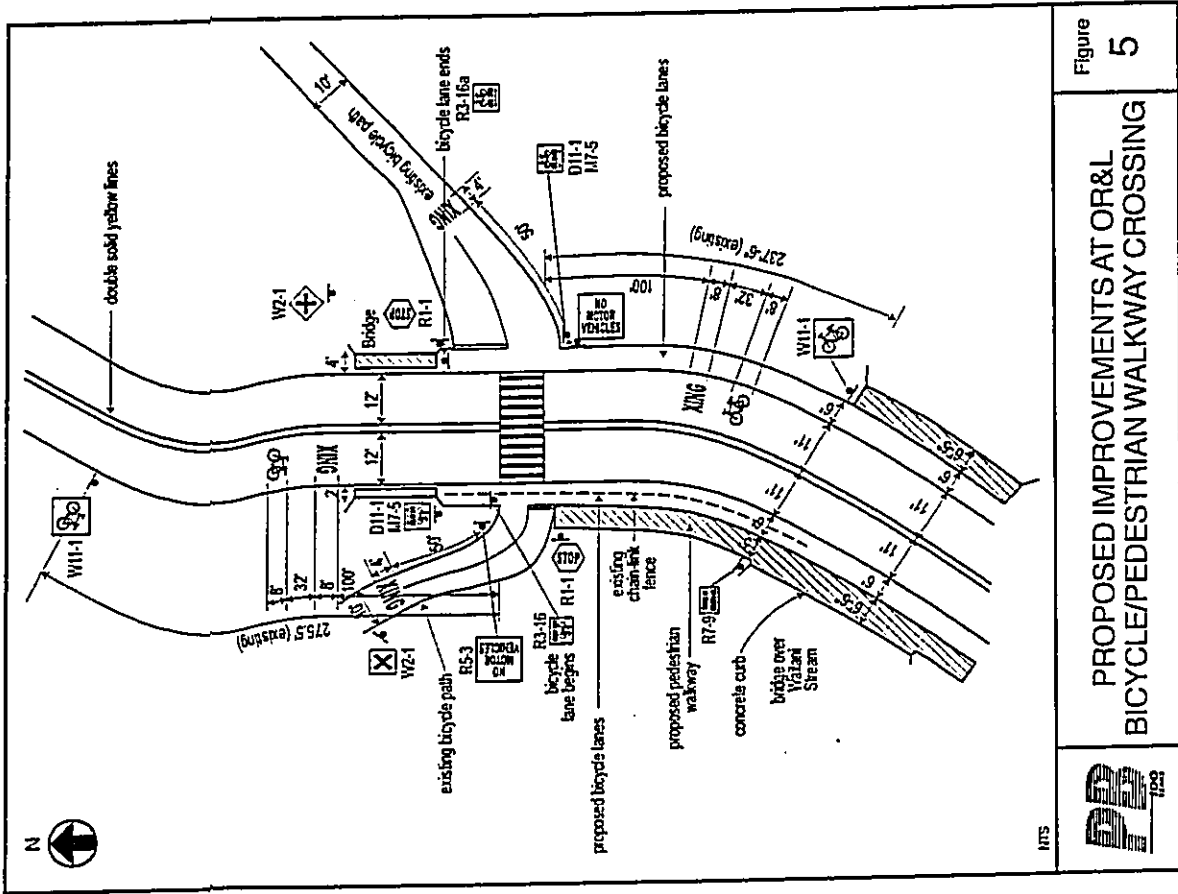
Figure 4 WPAR looking mauka at OR&L Bicycle/Pedestrian Walkway Crossing

Figure 4 is a photograph showing the view of the crossing. To improve visibility of the bicycle crossing, it is recommended to implement a painted crosswalk on WPAR. According to discussions with City and County of Honolulu staff, a standard zebra-stripe crosswalk is typically used for a marked bicycle crossing. In addition to the painted crosswalk, it is also recommended to include special "Bike Xing" pavement markings. There are two W11-1 signs, one in each direction, on WPAR. They are both approximately 235-250 feet from the bicycle crossing. It was decided that this distance would provide sufficient warning because the conditions only involve changing speed and not changing lanes. Since, according to the Manual on Uniform Traffic Control Devices (MUTCD), sign legibility distance is 175 feet, the existing setup in conjunction with the painted bicycle crossing would provide adequate warning to motorists.

Modifications to signs along the bicycle/pedestrian walkway are also recommended. The path on Koko Head side of the bicycle crossing does not have any warning of the upcoming crossing with WPAR. To remedy this, an R1-1 stop sign is recommended for Ewa-bound bicyclists at the crossing (a corresponding R1-1 sign is already in place on the opposite side of the crossing). Additionally, signs and pavement markings (a W2-1 intersection sign and XING pavement marking, respectively) indicating the presence of an intersection ahead would be placed 50 feet down the OR&L bicycle/pedestrian walkway on both sides of the crossing with WPAR. At the entrance of each side of the crossing and visible to the vehicular traffic, R5-3 No Motor Vehicles signs are recommended. It is recommended to investigate providing an alternative access to the Waipahu High School athletic fields in place of the current access that uses the same entry point as the bicycles and pedestrians. Figure 5 illustrates the improvements proposed at the OR&L Bicycle/Pedestrian Walkway Crossing of WPAR.

C. Bicycle and Pedestrian Facilities Along WPAR

Currently, there are no bicycle facilities and only limited pedestrian facilities along WPAR between the OR&L bicycle/pedestrian walkway and the WPSF. As part of linking recreational facilities, it is recommended to improve WPAR by providing a shared use path between the bicycle/pedestrian walkway and WPSF.



PROPOSED IMPROVEMENTS AT OR&L BICYCLE/PEDESTRIAN WALKWAY CROSSING

Figure 5



Figure 6 is a photograph of the segment of WPAR between the OR&L bicycle/pedestrian walkway crossing and the Waianai Stream Bridge, looking mauka toward the OR&L bicycle/pedestrian walkway crossing.



Figure 6 WPAR Between OR&L Bicycle/Pedestrian Walkway Crossing and Waianai Stream Bridge (Photo taken looking mauka)

Figure 7 contains cross-sections that illustrate the existing condition and the proposed improvements to provide a 10-foot shared-use path and a 4-foot landscaped buffer on the Ewa side of WPAR. This improvement will require relocation of the existing Ewa chain link fence bordering WPAR.

D. Wailani Stream Bridge

The existing Wailani Stream Bridge is located toward the mauka end of the study area and conducts WPAR over Wailani Stream, as shown in figure 6.

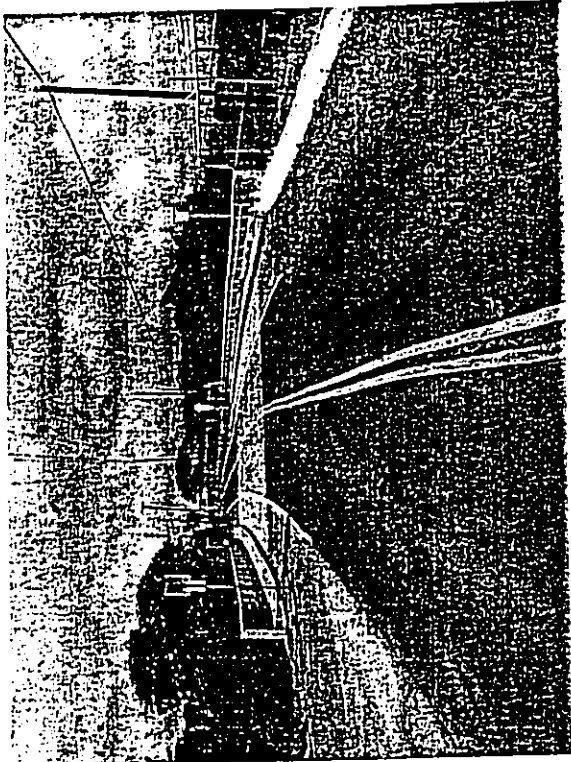
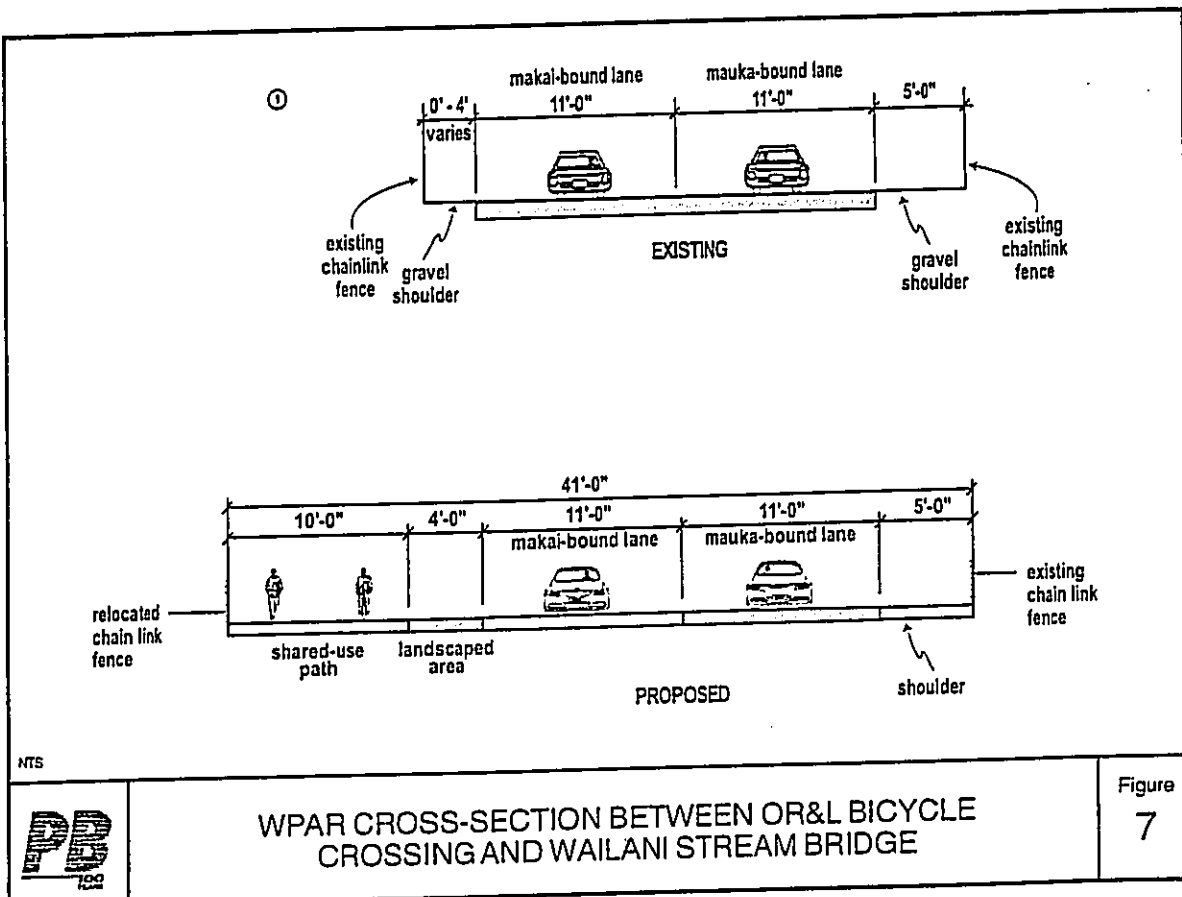


Figure 8 WPAR at Wailani Stream Bridge
(Photo taken looking mauka)

The cross-section of WPAR as it crosses the bridge is 40 feet, consisting of two 12-foot through lanes and two 8-foot delineated shoulders. Figure 9 contains cross-sections illustrating the existing and proposed configuration for WPAR on the Wailani Stream Bridge. The proposed 10-foot shared-use path and 4-foot landscaped buffer would widen the existing sidewalk into the existing roadway area. The Wailani Stream Bridge would not be widened. The curbed area width would be increased and the roadway width would be



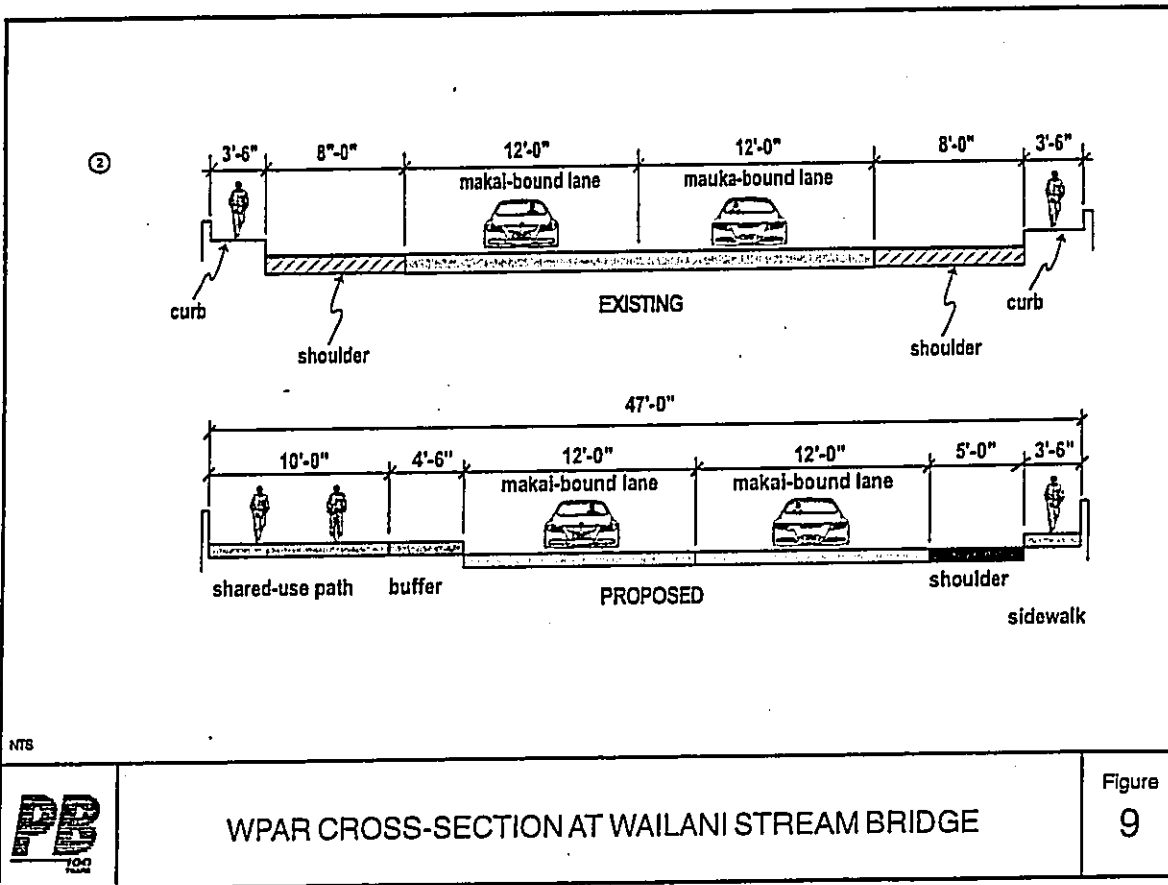
decreased. The Koko Head sidewalk on the bridge is unchanged. The proposed configuration requires modification of the bridge deck.

The bridge is a popular fishing location. Common practice by fishermen is to park their vehicles on the bridge while they fish off the bridge abutment. This occurs in spite of "No Parking on Bridge" signs posted on both sides of the bridge. The vehicles park within the delineated shoulders on both sides of WPAR. In the proposed plan, the shoulder areas will be decreased to accommodate the proposed shared-use path and landscaped buffer, requiring stricter enforcement of the parking prohibition.

E. NAVSEA Inactive Ships On-Site Maintenance Office (NISMO)

The NAVSEA Inactive Ships On-Site Maintenance Office (NISMO) is located on the Koko Head side of WPAR, mauka of the Ted Makalena Golf Course Driveaway. Insufficient space exists on-site to accommodate most of the employee parking, so additional parking (approximately 76 spots were counted) is provided on an unimproved gravel parking area on the Ewa side of WPAR. This existing parking area is configured for 90-degree parking with parking stalls accessing WPAR directly. Most vehicles parked in this area back into the parking stalls. This configuration requires that vehicle parking maneuvers take place within WPAR. This is usually not a problem due to low traffic volume. During major soccer tournaments, however, this maneuver can be difficult. Fortunately, the peak demands for the parking during the morning and afternoon do not coincide with the peak demand for the WPSP. However, a sufficient increase arises during major soccer tournaments such that even off-peak soccer-related vehicular activity affects the ease of parking maneuvers.

Figure 10 illustrates WPAR in the vicinity of the NISMO. In the photograph, employee parking is shown on the left of WPAR, while the NISMO facility is on the right of WPAR. The white curb fronting the NISMO facility will be maintained. Any proposed modifications to WPAR will occur in the Ewa (west) direction. As shown in Figure 11, the area behind the white curb consists of a 2-foot, 6-inch concrete area and a concrete lined slope into the chain link fence bordering the NISMO facility.



WPAR CROSS-SECTION AT WAILANI STREAM BRIDGE

Figure
9

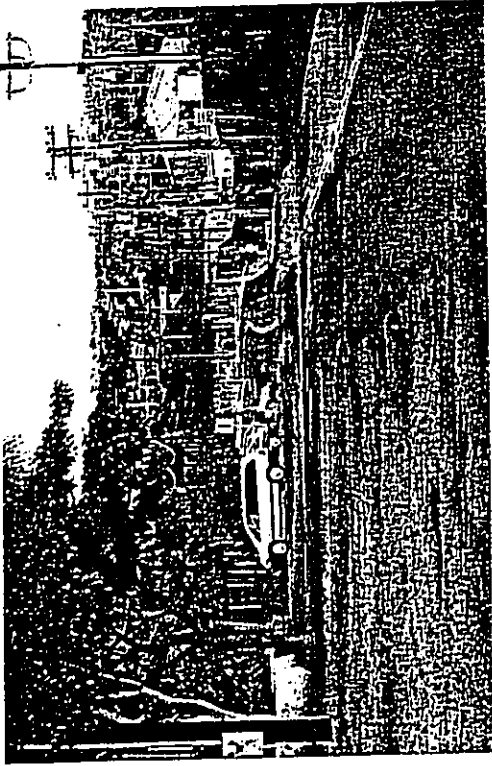


Figure 10 WPAR Looking Mauka at NISMO Parking Area
(Parking area Eva of WPAR, NISMO KKHD of WPAR, Makalena G.C. Driveaway shown)

Because improving parking for the NISMO facility is a condition of the easement negotiated between the City and the Navy regarding the use of WPAR, three alternative parking schemes were investigated as part of this study. Alternative A is a scheme that maintains the existing 90-degree parking, but improves drainage, pedestrian and bicycle access to WPSP. Alternative B provides a parking scheme that provides parking near the NISMO facility and reduces parking maneuver conflicts on WPAR. However, it requires encroachment into the Ted Makalena Golf Course (TMGC). Alternative C relocates the NISMO parking area to an existing grassy area located adjacent to the Ted Makalena Golf Course parking lot while allowing for the same roadway improvements. It also reduces parking maneuver conflicts on WPAR, but is not adjacent to NISMO. Approximately 100 parking stalls will be provided, of which 4 will be reserved for handicapped stalls and 4 will be reserved for service/delivery vehicles. A crosswalk will be painted which will connect the pedestrian walkway to the NISMO entrance.

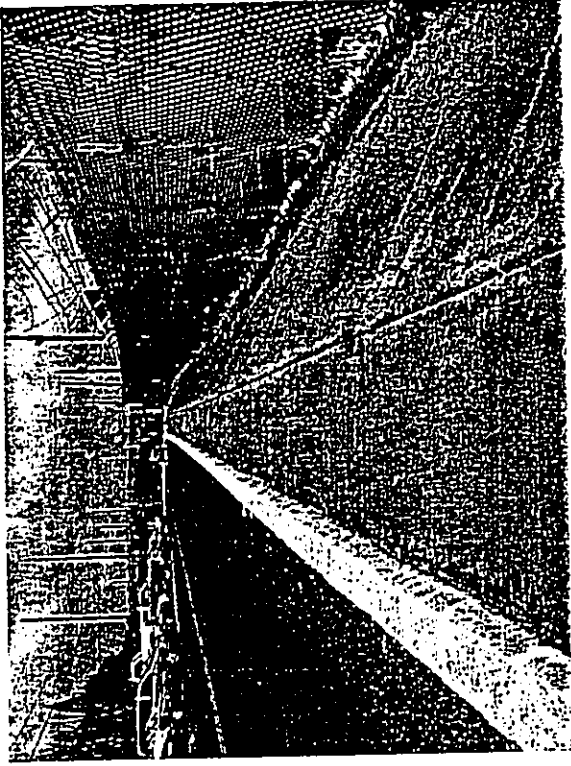


Figure 11 Existing concrete area on KKHD side of WPAR Adjacent to NISMO (looking mauka)

Figure 12 illustrates the typical cross sections of WPAR in the vicinity of NISMO for the existing condition, Alternative A, Alternative B, and Alternative C.

1. Alternative A - 90-degree Parking

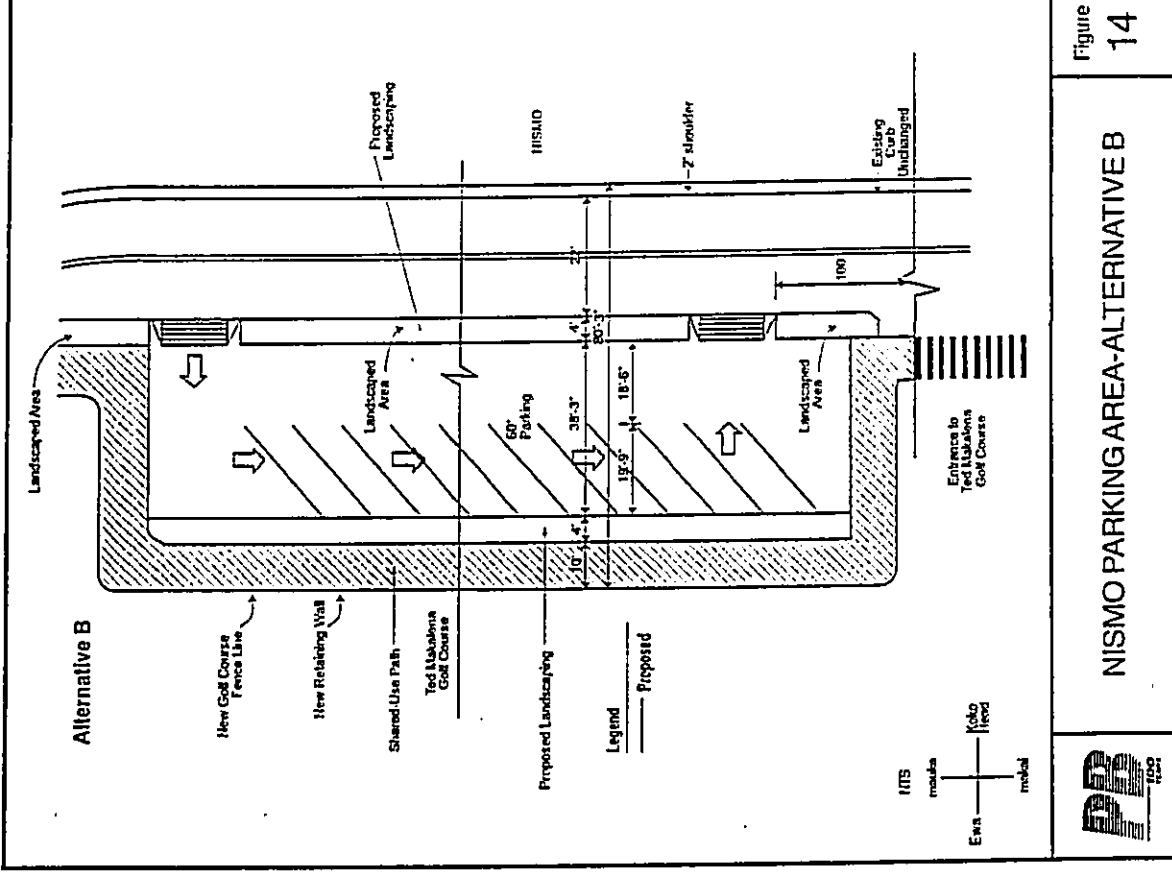
Alternative A maintains the existing parking configuration but accommodates the proposed bicycle lanes, pedestrian walkway, and landscaped area on WPAR by moving the parking lot in the Eva direction.

Alternative A would require the fewest modifications to the existing NISMO parking area. Figure 13 is a plan view illustration of the proposed parking design. This alternative would maintain the existing 90-degree parking configuration but would add a 5-foot shoulder area between the parking stalls and WPAR. The 10-foot shared-use path and 4-foot landscape

buffer are continued on the golf course side of the parking area to minimize conflicts with parking maneuvers. Lane widths for WPAR will be maintained at 11 feet in this area. The shared-use path would encroach upon an existing drainage ditch, necessitating the construction of a box culvert upon which the shared-use path could be constructed. The parking lot would be kept approximately 200 feet mauka of the TMGC driveway to prevent any line-of-sight obstruction and would be able to accommodate approximately 100 parked vehicles. Handicapped parking stalls would be placed opposite the NISMO entrance and would be flanked by delivery/service vehicle parking stalls. Utility poles will be surrounded by landscaping.

2. Alternative B - Diagonal Parking Lot

Alternative B reflects an attempt to keep parking close to the NISMO facility while reducing the interaction between the NISMO parking and WPAR. Figure 14 is a plan view illustration of the proposed parking modifications. Here, a more formal parking lot would be constructed with an entrance and exit located at its mauka and makai extremes, respectively, with a parking aisle oriented in the makai-bound direction. 60-degree parking would be provided. As with Alternative A, the 11-foot lane width would be preserved for WPAR. In addition, the 10-foot shared-use path and 4-foot landscape buffer would also be accommodated on the golf course side of the parking lot. Again, a box culvert would be required to accommodate the shared-use path, landscape buffer, and parking lot. Additionally, approximately 20 feet of golf course property would have to be acquired and a retaining wall would have to be constructed to address the grade difference between the golf course and the proposed parking area. The affected golf course property is designated "out of bounds," meaning it is not a playable area. However, its close proximity to an active fairway might make a higher fence necessary. Figure 15 shows the affected area. This alternative would minimize the NISMO parking lot's interaction with vehicles traveling on WPAR but would require cooperation from the golf course and would likely cost the most. The parking lot would be kept 100 feet mauka of the TMGC driveway to provide sight distance for the TMGC driveway and would be able to accommodate approximately 100 parked vehicles. As with Alternative A, handicapped stalls would be located directly across the street from the NISMO entrance, flanking an island which would provide a path to the crosswalk. Utility poles will be surrounded by landscaping.



NISMO PARKING AREA-ALTERNATIVE B

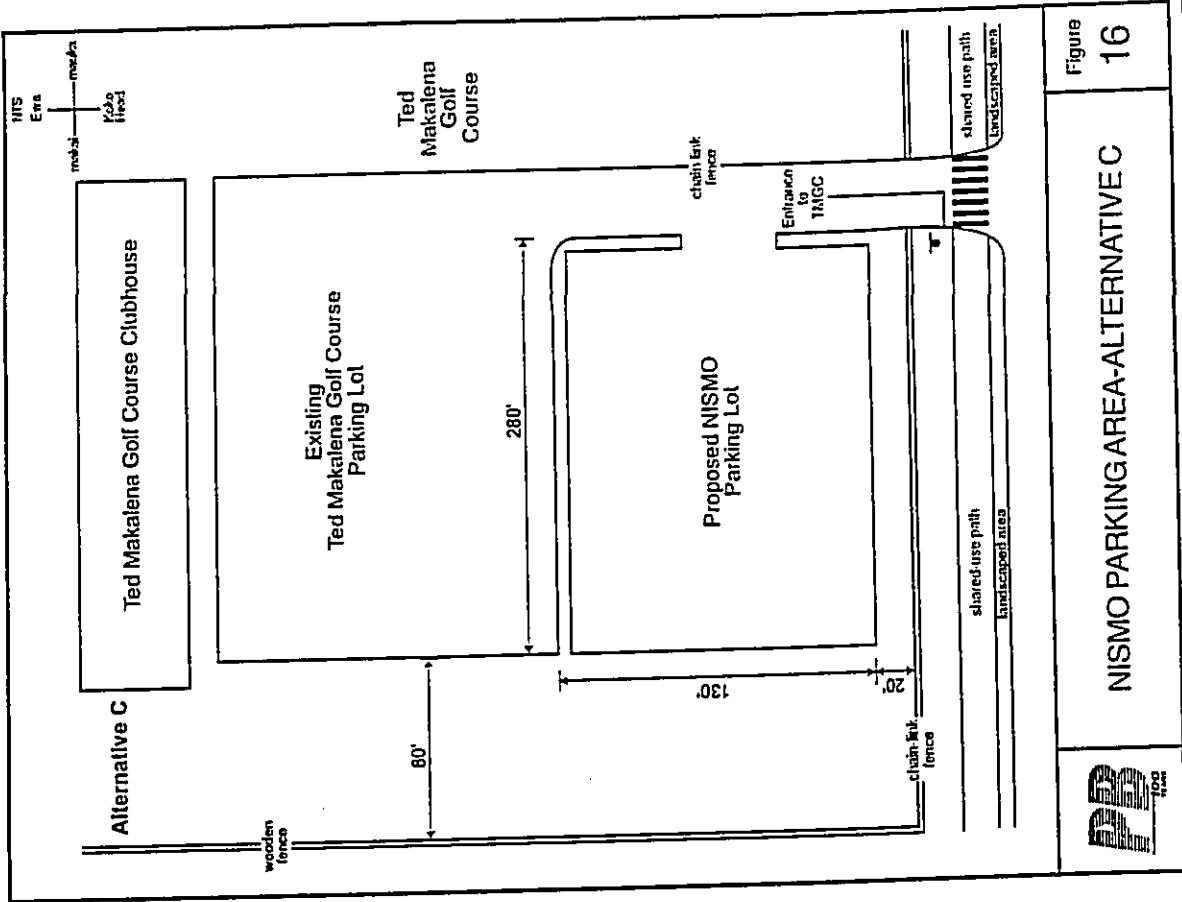
Figure
14



Figure 15 Ted Makalena Golf Course Ewa of Chain Link Fence

3. **Alternative C - Parking Lot Adjacent to TMGC Parking Lot**

Alternative C would relocate the NISMO parking area to a currently vacant area adjacent to the Ted Makalena Golf Course parking lot. The Alternative C concept is shown in Figure 16. Keeping the two parking areas separate is recommended to prevent confused golfers from using NISMO parking spaces, though both would likely access the TMGC driveway. Finally, as with the other two alternatives, the 11-foot lane width would be preserved for WPAR in this area. The 10-foot shared-use path and 4-foot landscape buffer would still be located on the Ewa-side of WPAR. Finally, improvements would be made to the existing drainage ditch. Alternative C would be able to accommodate at least 100 parked vehicles. Again, handicapped stalls would likely be placed in the mauka-KKHD corner of the parking



NISMO PARKING AREA-ALTERNATIVE C

Figure 16

lot, which would be nearest to the NISMO entrance, and an ADA compliant path would be constructed to provide access to the NISMO entrance.

4. Pedestrian Crossing

In all 3 alternatives, a crosswalk would be installed across WPAR, connecting the shared-use path and parking areas to the NISMO entrance. W11-2 (pedestrian crossing) signs in conjunction with W16-7p directional signs would be placed at the crosswalk to indicate its presence. This segment of WPAR will be designated with a 15 mph speed limit. Advance crosswalk warning signs were not considered necessary due to the relative straightness (and therefore unimpeded visibility) of WPAR. In addition, it was determined that since the distance needed to identify the crosswalk and decelerate from 15 mph to 0 mph is less than the sign legibility distance, placing crosswalk signs at the crosswalk itself would be sufficient. This traffic signing program is illustrated in Figure 17 of this report. To maintain ADA accessibility, a part of the sloped paved area located Koko Head of WPAR and adjacent to NISMO would need to be reconstructed to provide a path outside the roadway for pedestrian and wheelchair access to NISMO.

5. Parking Alternative Selected

In order to determine the preferred parking alternative, several factors had to be considered. Reducing the parking maneuver conflicts while being located in close proximity to NISMO are two major criteria. Therefore, Alternative B-Diagonal Parking Lot would be the first choice as preferred parking alternative.

The one drawback of Alternative B is that approximately 0.5 acres of new right-of-way would be acquired from the Ted Makalena Golf Course (TMGC), which would require moving the existing golf course boundary and fence up to approximately 23 feet in the Ewa direction. Since Waipio Point Access Road is owned by the U.S. Department of Navy and the GOE requires that all roadway improvements meet the requirements of the National Environmental Protection Act (NEPA) and all applicable environmental laws and regulations. The Section 4(f) regulations regarding the use of parklands must be met. Section 4(f) stipulates that the Federal Highway Administration (FHWA) will not approve any program or project which requires the use of any publicly owned public park,

recreation area, or wildlife or waterfront refuge, or any land from an historic site of national, state, or local significance unless:

1. there is no feasible and prudent alternative to the use, and
2. all possible planning to minimize harm resulting from such use is included.

The use of lands protected under Section 4(f) must be evaluated and approved by FHWA through a Section 4(f) Evaluation.

Alternative B was deemed not feasible since it would not pass the 4(f) process. It requires 0.5 acres of Ted Makalena Golf Course property from the 18th hole which is considered a major use of 4(f) lands. In addition, there are feasible and prudent alternatives to Alternative B.

Given that 4(f) issues eliminate Alternative B, evaluation of the criteria contained in the GOE suggested Alternative C, the Parking Lot Adjacent to the existing TMGC parking lot. However, when this alternative was reviewed with the U.S. Navy, this alternative was deemed not feasible because, according to the Navy, it would not fulfill the Grant of Easement requirement of maintaining parking in the immediate proximity to the NISMO facility and would impede the daily operations of their facility.

This left Alternative A as the remaining parking alternative. It is adjacent to the NISMO facility, fulfilling one of the criteria. However, it does not appreciably reduce the parking maneuver conflicts with traffic on WPAR. Given the conditions imposed on the project, it is the only feasible alternative remaining of the three identified. To address the concerns of the parking maneuver conflicts, the following are recommended:

- lower the speed limit on WPAR adjacent to the 90-degree parking area. Discussions with the City and County Department of Transportation Services (DTS) indicated that 15 mph would be the speed limit within this segment. The existing speed limit in this area is 20 mph. Advance W3-5 warning signs located 225 feet in advance of the parking area in both directions of travel would warn of a reduction in speed to 15 mph. At the start of parking in both directions of travel, an R2-1 "Speed Limit 15" sign is recommended;

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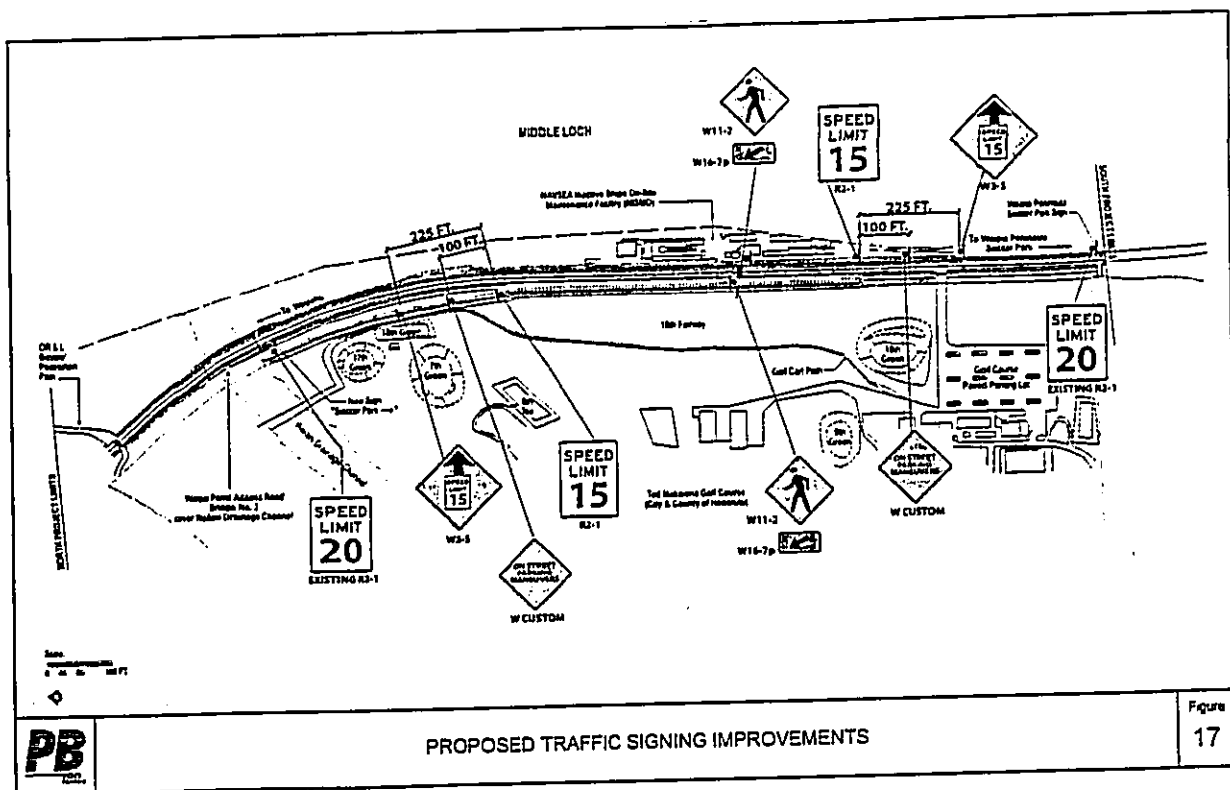
- install warning signs to alert drivers on WPAR to on-street parking maneuvers. It is recommended that a custom word message sign similar to a W8-6 sign that substitutes the words, "On-Street Parking Maneuvers." Custom word message warning signs are allowed in Section 2C.02 of MUTCD. It is recommended to install these signs 100 feet in advance of the NISMO 90-degree parking area in both directions of WPAR.

The purpose of these traffic signing improvements is to alert drivers on WPAR to the on-street parking maneuvers caused by the NISMO 90-degree parking and represent an incremental improvement over the existing condition in which no warning is provided for the parking maneuvers. Figure 17 summarizes these improvements.

F. Ted Makalena Golf Course Driveway

Figure 18 illustrates the driveway entrance to the Ted Makalena Golf Course. The shared-use path could be terminated at this driveway or continued makai into the WPSP. As shown previously in Figure 10, the area makai of the Ted Makalena Golf Course Driveway contains relatively few constraints on the Ewa side and could accommodate the shared-use path if so desired. Therefore, the decision on whether to extend the shared-use path is an economic one. If it is decided to terminate the shared-use path, the Ted Makalena Golf Course Driveway is a convenient terminus. If the shared-use path is continued makai, a crosswalk should be provided across the TMGC driveway.

Another issue at this driveway is the justification for installing a median turn lane within WPAR for traffic turning left from mauka-bound WPAR into the Ted Makalena Golf Course Driveway. Based on traffic observations, the mauka-bound left-turn from WPAR into Ted Makalena Golf Course is a very infrequent movement. While it is acknowledged that during major tournaments, a vehicle desiring to execute this movement could delay mauka-bound traffic while waiting for a gap in makai-bound traffic, this left-turn movement is so rare that this median turn lane is not recommended.



PROPOSED TRAFFIC SIGNING IMPROVEMENTS

Figure 17

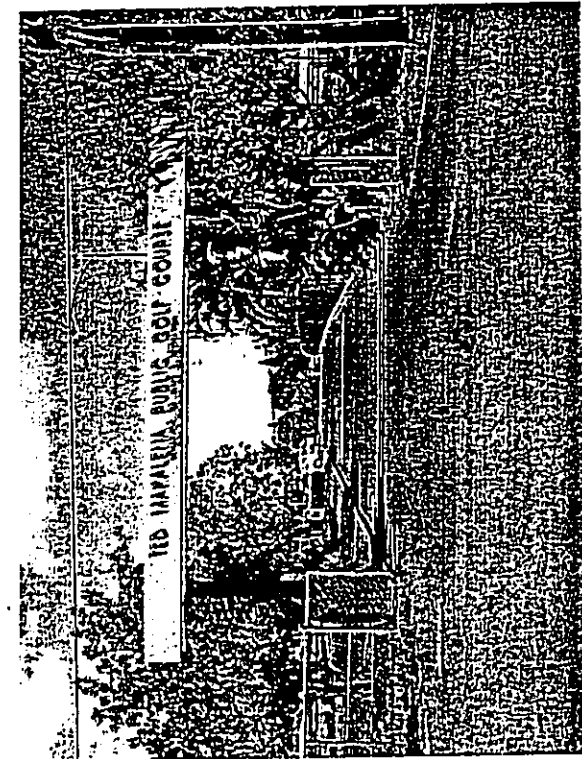


Figure 18 Ted Makalena Golf Course Driveway

G. WSPSP Entrance (near Hokuahi Lawns, Inc. Driveway)

Figure 19 is a photograph at the WSPSP Entrance just makai of the Hokuahi Lawns, Inc. Driveway.

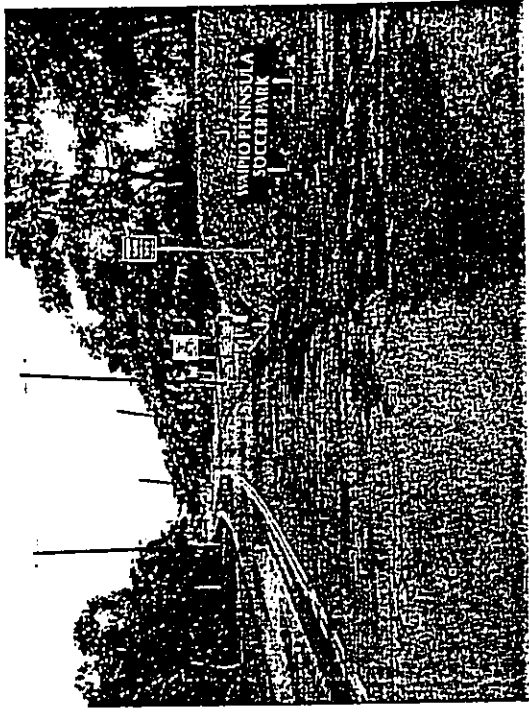


Figure 19 WSPSP Entrance Near Hokuahi Lawns, Inc. Driveway
(Picture Taken Looking Makai)

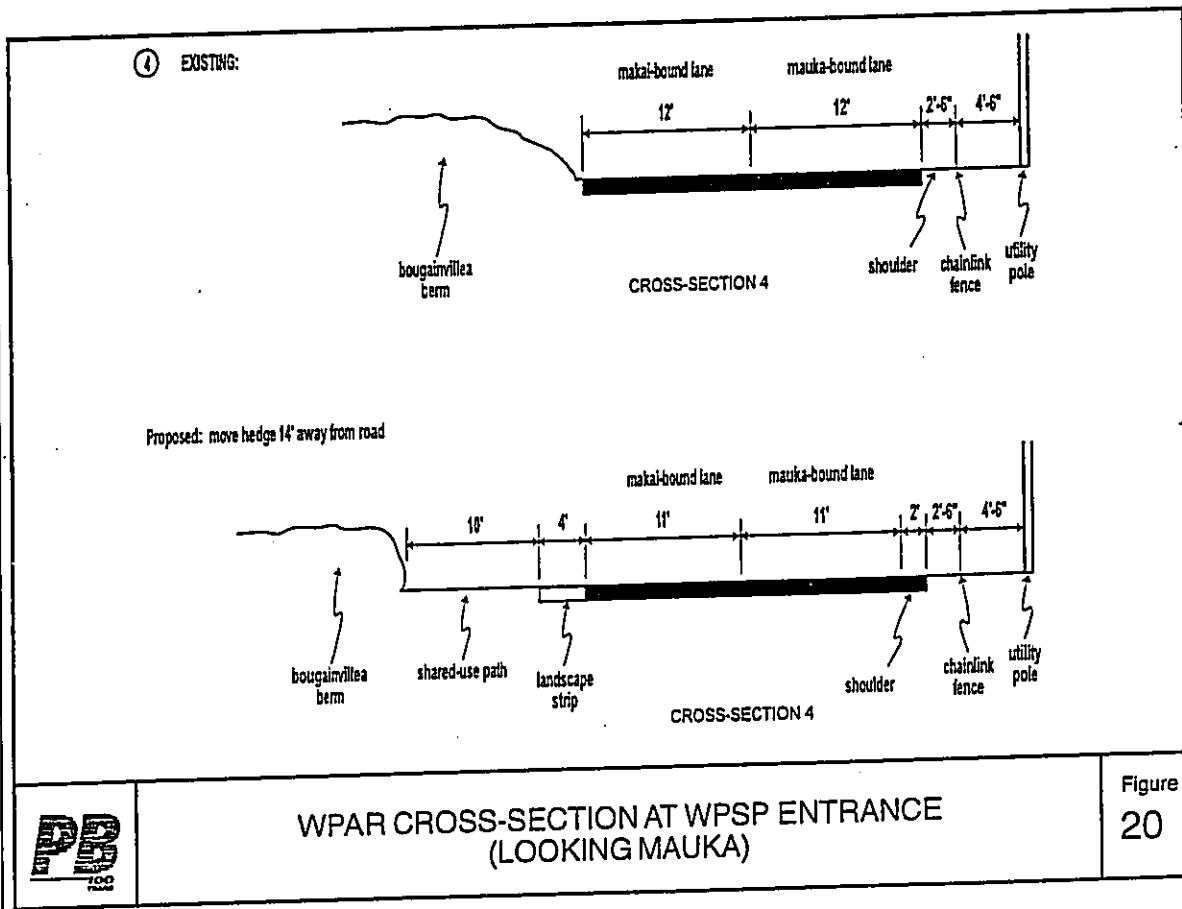
The entrance to the Waipio Peninsula Soccer Park lies just makai of the driveway to Hokuahi Lawns, Inc. This point would provide another logical terminus for the shared-use path. Alternately, the shared-use path could be continued past the park entrance to the WSPSP parking areas. In this case, the soccer park entrance would have to be widened by an additional 10 feet to 14 feet into the existing Bougainvillea berm. The path could then follow a meandering alignment through the utility poles and the trees. Figure 20 contains cross sections of the existing and proposed conditions at this location.

H. WPAR Within WPSP

Within the WPSP, WPAR maintains a two-lane, undivided roadway configuration. Immediately mauka of the WPSP entrance, WPAR is bordered by trees and utility poles as previously shown in Figure 18. As WPAR curves to the west within WPSP, the character of the roadway changes. Concrete and timber pole barriers border this segment of WPAR as shown in Figure 21.

In this part of WPSP, there are concrete sidewalks located behind these barriers. The shared-use path proposed along the Ewa side of WPAR could be extended and connected to this existing sidewalk.

This part of WPAR has existing speed bumps to maintain slow vehicular speeds. It is recommended that the proposed shared-use path transition into a bicycle route environment as WPAR enters the WPSP where bicycles share the road with vehicles. This transition could occur at the intersection of WPAR and the Navy Facility Road shown in Figure 3. Alternatively, the shared-use path could be terminated at the WPSP entrance with the bicycle route designation beginning concurrently. In that case, it is recommended that speed bumps be installed all the way to the WPSP entrance located makai of the Hokuahi Lawns, Inc. driveway.



**WPAR CROSS-SECTION AT WPSP ENTRANCE
(LOOKING MAUKA)**

**Figure
20**



Figure 21 Barriers Along WPAR Within WPSP

VI. RECOMMENDED WPAR IMPROVEMENTS

After studying the WPAR area extensively, the following recommendations were made:

- Improvements should be made at the OR&L bicycle/pedestrian crossing to improve the overall safety of the crossing. This includes a painted crosswalk and updated signage.
- A pedestrian and bicycle facility should be added between the OR&L bicycle/pedestrian crossing and the WPSP entrance with the option of extending the facility into the park.
- Improvements should be made to the NISMO parking lot to allow the bicycle lanes and pedestrian walkway to continue past the parking area without having to interact with parking lot traffic. NISMO Parking Alternative A was selected because Alternatives B and C were deemed not feasible. Traffic signing improvements are recommended to make drivers on WPAR aware of on-street parking maneuvers in the NISMO area and a crosswalk should be installed across WPAR, connecting the NISMO entrance to the shared-use path via an island in the parking area. Handicapped and delivery/service vehicle parking stalls will be located adjacent to this island area.

WPSF mauka bound.txt

0	0	0	0	0	0	2	7	15	25	41	74	70	137	99	49	32	6	5	3	0	0	0	0
0	3	0	0	0	1	0	12	16	27	54	53	63	102	72	33	14	4	3	1	0	0	0	2
0	0	0	0	1	5	1	18	17	36	62	80	112	77	102	36	4	3	1	1	0	1	1	0

AM Peak Hour 11:00 to 12:00 (273 vehicles)
AM Peak Hour Factor 85.3%
PM Peak Hour 12:45 to 13:45 (464 vehicles)
PM Peak Hour Factor 84.7%

07/05/03 Channel: 1 Direction: N

0100	0200	0300	0400	0500	0600	0700	0800	0900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000	2100	2200	2300	2400	Totals
1	1	0	0	1	0	2	13	57	67	233	247	295	412	270	148	45	31	19	6	0	0	0	0	1848
0	0	0	0	0	0	0	1	3	9	59	52	80	84	53	53	8	6	6	5	0	0	0	0	0
1	0	0	0	0	0	1	4	17	14	62	50	88	137	58	34	19	8	7	1	0	0	0	0	0
0	0	0	0	0	0	1	1	19	19	60	38	63	116	61	42	16	9	2	0	0	0	0	0	0
0	1	0	0	1	0	0	7	18	25	52	107	64	75	98	19	2	8	4	0	0	0	0	0	0

AM Peak Hour 11:00 to 12:00 (247 vehicles)
AM Peak Hour Factor 57.7%
PM Peak Hour 13:00 to 14:00 (412 vehicles)
PM Peak Hour Factor 75.2%

Sunday 07/06/03 Channel: 1 Direction: N

0100	0200	0300	0400	0500	0600	0700	0800	0900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000	2100	2200	2300	2400	Totals
0	0	0	0	1	0	5	12	25	72	129	125	165	185	179	100	60	26	12	2	3	1	0	0	1102
0	0	0	0	0	0	0	1	11	14	30	17	38	50	67	18	15	9	4	1	3	0	0	0	0
0	0	0	0	1	0	2	2	4	18	44	38	34	54	32	39	9	6	1	1	0	0	0	0	0
0	0	0	0	0	0	1	3	5	13	38	38	52	32	43	30	24	7	5	0	0	1	0	0	0
0	0	0	0	0	0	2	6	5	27	17	32	41	49	37	13	12	4	2	0	0	0	0	0	0

AM Peak Hour 09:45 to 10:45 (139 vehicles)
AM Peak Hour Factor 79.0%
PM Peak Hour 13:15 to 14:15 (202 vehicles)
PM Peak Hour Factor 75.4%

Monday 07/07/03 Channel: 1 Direction: N

0100	0200	0300	0400	0500	0600	0700	0800	0900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000	2100	2200	2300	2400	Totals
0	0	1	0	0	0	1	8	9	14	14	4													51
0	0	0	0	0	0	0	3	2	5	9	4													4
0	0	0	0	0	0	1	2	3	2	1														1
0	0	1	0	0	0	0	0	3	4	1														1
0	0	0	0	0	0	0	3	1	3	3														3

AM Peak Hour 09:15 to 10:15 (18 vehicles)
AM Peak Hour Factor 50.0%
PM Peak Hour Unavailable
PM Peak Hour Factor Unavailable

WPSF mauka bound.txt

Volume Count Report
Generated by MSC3000 Version 2.01 Copyright 1990-1992 Mitron Systems Corporation

Location Waipio Point Access Road
Location Code 35
County
Recorder Set 07/02/03 11:44
Recording Start 07/02/03 12:00
Recording End 07/07/03 11:15
Sample Time 15 Minutes
Operator Number 22
Machine Number 1
Channel 2
Divide By 2
Summation No
Two-way Yes

z-#e 07/02/03 Channel: 2 Direction: S

0100	0200	0300	0400	0500	0600	0700	0800	0900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000	2100	2200	2300	2400	Totals
							77	336	411	350	383	199	49	13	7	1	0	0	0	0	0	0	0	1826
							23	43	95	100	103	65	15	8	1	0	0	0	0	0	0	0	0	0
							11	74	96	88	118	76	21	2	5	1	0	0	0	0	0	0	0	0
							13	109	104	82	89	26	9	1	1	0	0	0	0	0	0	0	0	0
							30	110	116	80	73	32	4	2	0	0	0	0	0	0	0	0	0	0

AM Peak Hour Unavailable
AM Peak Hour Factor Unavailable
PM Peak Hour 14:15 to 15:15 (416 vehicles)
PM Peak Hour Factor 89.7%

07/03/03 Channel: 2 Direction: S

0100	0200	0300	0400	0500	0600	0700	0800	0900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000	2100	2200	2300	2400	Totals
0	0	0	0	0	0	8	18	19	28	29	33	50	336	428	412	381	142	52	15	1	0	0	0	1952
0	0	0	0	0	0	2	3	2	8	6	9	14	25	92	132	98	50	18	6	1	0	0	0	0
0	0	0	0	0	0	2	4	8	4	6	4	9	85	109	91	09	38	12	3	0	0	0	0	0
0	0	0	0	0	0	3	4	3	7	10	5	12	119	103	86	97	32	9	3	0	0	0	0	0
0	0	0	0	0	0	1	7	6	9	7	15	15	107	124	103	97	22	13	3	0	0	0	0	0

AM Peak Hour 11:00 to 12:00 (33 vehicles)
AM Peak Hour Factor 55.0%
PM Peak Hour 14:15 to 15:15 (468 vehicles)
PM Peak Hour Factor 88.6%

n#ev#e# 07/04/03 Channel: 2 Direction: S

0100	0200	0300	0400	0500	0600	0700	0800	0900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000	2100	2200	2300	2400	Totals
1	1	0	0	0	3	41	348	471	403	353	254	141	78	28	21	19	12	5	9	0	1	3	3	2195
1	0	0	0	0	0	6	45	124	100	87	69	47	20	10	8	4	4	2	4	0	0	3	0	0

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WSPR mauka bound.txt
 0 0 0 0 0 0 9 96 106 86 102 71 40 27 9 5 7 0 1 2 0 0 0 0
 0 1 0 0 0 0 11 85 112 102 84 59 35 14 4 5 2 3 1 2 0 0 0 0
 0 0 0 0 0 0 3 15 122 129 115 80 55 19 17 5 3 6 5 1 1 0 1 0 0

AM Peak Hour 08:00 to 09:00 (471 vehicles)
 AM Peak Hour Factor 91.3%
 PM Peak Hour 12:00 to 13:00 (141 vehicles)
 PM Peak Hour Factor 75.0%

07/05/03 Channel: 2 Direction: S
 0100 0200 0300 0400 0500 0600 0700 0800 0900 1000 1100 1200 1300 1400 1500 1600 1700 1800 1900 2000 2100 2200 2300 2400 Totals
 2 2 0 0 0 1 31 89 376 409 423 274 149 67 35 20 21 5 2 0 3 0 1 0 1910
 0 0 0 0 0 0 4 12 98 73 92 87 36 23 12 5 5 2 1 0 3 0 0 0
 1 0 0 0 0 0 5 14 113 91 146 70 44 18 7 4 9 2 1 0 0 0 0 0
 1 0 0 0 0 0 3 18 81 119 88 71 34 14 9 5 3 1 0 0 0 0 0 0
 0 2 0 0 0 1 19 45 84 126 97 46 35 12 7 6 4 0 0 0 0 0 0 0

AM Peak Hour 09:30 to 10:30 (483 vehicles)
 AM Peak Hour Factor 82.7%
 PM Peak Hour 12:00 to 13:00 (149 vehicles)
 PM Peak Hour Factor 84.7%

Sunday 07/06/03 Channel: 2 Direction: S
 0100 0200 0300 0400 0500 0600 0700 0800 0900 1000 1100 1200 1300 1400 1500 1600 1700 1800 1900 2000 2100 2200 2300 2400 Totals
 4 0 0 0 5 2 36 141 227 218 162 120 66 55 25 25 22 12 10 4 1 1 0 0 1136
 0 0 0 0 3 1 5 10 68 51 58 30 20 12 7 5 12 3 3 1 1 0 0 0
 1 0 0 0 2 0 6 17 58 70 32 22 13 16 8 7 4 5 1 3 0 0 0 0
 0 0 0 0 0 0 12 51 54 47 31 44 19 15 6 5 5 3 5 0 0 1 0 0
 3 0 0 0 0 1 13 63 47 50 41 24 14 12 4 8 1 1 1 0 0 0 0 0

AM Peak Hour 07:45 to 08:45 (243 vehicles)
 AM Peak Hour Factor 89.3%
 PM Peak Hour 12:00 to 13:00 (66 vehicles)
 PM Peak Hour Factor 82.5%

Monday 07/07/03 Channel: 2 Direction: S
 0100 0200 0300 0400 0500 0600 0700 0800 0900 1000 1100 1200 1300 1400 1500 1600 1700 1800 1900 2000 2100 2200 2300 2400 Totals
 0 0 2 0 0 7 14 15 19 18 15 4
 0 0 0 0 0 2 2 5 5 2 1 4
 0 0 1 0 0 0 5 3 4 6 2 7
 0 0 1 0 0 1 0 4 5 7 7
 0 0 0 0 0 4 7 3 5 3 5

AM Peak Hour 08:45 to 09:45 (20 vehicles)
 AM Peak Hour Factor 71.4%
 PM Peak Hour Unavailable
 PM Peak Hour Factor Unavailable

Bridge over OR&L ROW mauka bound.txt
 Volume Count Report
 Generated by MSC3000 Version 2.01 Copyright 1990-1992 Mitron Systems Corporation

Location Waipio Point Access Road 2
 Location Code 53
 County
 Recorder Set 07/02/03 12:27
 Recording Start 07/02/03 13:00
 Recording End 07/07/03 11:30
 Sample Time 15 Minutes
 Operator Number 22
 Machine Number 7
 Channel 1
 Divide By 2
 Summation No
 Two-way Yes

07/02/03 Channel: 1 Direction: N
 0100 0200 0300 0400 0500 0600 0700 0800 0900 1000 1100 1200 1300 1400 1500 1600 1700 1800 1900 2000 2100 2200 2300 2400 Totals
 357 439 382 408 220 61 18 6 3 6 6 1906
 48 113 110 107 75 22 10 3 1 1 0 0
 75 91 98 125 76 26 5 0 1 1 1 0 0
 116 115 90 92 37 9 1 1 1 2 0 0
 118 120 84 84 32 4 2 0 1 1 2 0 0

AM Peak Hour Unavailable
 AM Peak Hour Factor Unavailable
 PM Peak Hour 14:30 to 15:30 (443 vehicles)
 PM Peak Hour Factor 92.3%

07/03/03 Channel: 1 Direction: N
 0100 0200 0300 0400 0500 0600 0700 0800 0900 1000 1100 1200 1300 1400 1500 1600 1700 1800 1900 2000 2100 2200 2300 2400 Totals
 9 11 2 3 18 62 149 89 57 60 57 82 71 369 472 459 415 168 71 25 8 4 5 7 2673
 3 0 0 1 2 8 32 16 17 18 10 18 17 36 93 137 108 61 24 9 5 1 0 0
 3 2 1 0 4 12 33 33 15 13 16 17 86 124 108 98 37 18 5 2 2 1 0 0
 2 3 0 1 6 16 42 18 14 11 15 20 14 136 120 102 108 39 13 6 0 1 0 0
 1 6 1 1 6 26 42 22 10 18 19 28 23 111 135 112 101 31 16 5 1 0 0 0

AM Peak Hour 06:00 to 07:00 (149 vehicles)
 AM Peak Hour Factor 88.7%
 PM Peak Hour 14:15 to 15:15 (516 vehicles)
 PM Peak Hour Factor 94.2%

07/04/03 Channel: 1 Direction: N
 0100 0200 0300 0400 0500 0600 0700 0800 0900 1000 1100 1200 1300 1400 1500 1600 1700 1800 1900 2000 2100 2200 2300 2400 Totals
 7 3 3 1 8 45 101 397 486 434 374 275 176 92 44 32 30 21 8 10 20 6 8 3 2584
 5 0 1 1 1 11 26 55 128 110 98 67 56 28 11 10 8 7 2 7 6 2 3 0

bridge over OR&L ROW makai bound.txt

3	3	0	0	1	2	6	8	19	32	41	76	86	142	110	53	36	16	16	7	1	3	1	2
0	4	0	0	2	2	2	14	19	32	61	56	74	102	83	44	24	14	12	14	5	2	3	2
0	0	3	0	5	2	3	22	20	42	63	87	116	90	112	44	15	12	12	11	4	3	1	3

AM Peak Hour 11:00 to 12:00 (289 vehicles)
AM Peak Hour Factor 83.0%
PM Peak Hour 12:45 to 13:45 (480 vehicles)
PM Peak Hour Factor 84.5%

07/05/03 Channel: 2 Direction: S

0100	0200	0300	0400	0500	0600	0700	0800	0900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000	2100	2200	2300	2400	Totals
5	4	2	5	3	5	4	27	72	85	243	254	321	452	299	192	85	57	32	55	5	15	6	9	2237
1	0	0	0	0	1	1	2	5	14	59	52	94	87	63	60	22	12	8	14	2	4	1	4	
3	1	1	4	1	2	1	7	20	25	65	57	97	149	64	45	28	13	14	10	0	0	0	2	
0	1	0	0	2	1	2	8	27	22	68	45	67	131	64	60	24	19	5	22	2	0	3	2	
1	2	1	1	0	1	0	10	20	24	51	100	63	85	100	27	11	13	5	9	1	11	2	1	

AM Peak Hour 11:00 to 12:00 (254 vehicles)
AM Peak Hour Factor 63.5%
PM Peak Hour 13:00 to 14:00 (452 vehicles)
PM Peak Hour Factor 75.8%

Sunday 07/06/03 Channel: 2 Direction: S

0100	0200	0300	0400	0500	0600	0700	0800	0900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000	2100	2200	2300	2400	Totals
7	3	6	5	8	5	9	20	44	81	151	156	181	211	222	119	95	52	39	41	8	11	4	11	1489
1	1	2	1	0	1	0	2	13	17	32	23	45	60	75	24	29	13	15	5	2	3	1	3	
1	1	3	2	1	2	3	7	16	49	43	34	59	59	56	45	13	14	6	6	2	3	0	4	
3	1	1	2	4	1	3	4	11	19	46	50	58	42	49	32	33	12	13	24	2	2	1	2	
2	0	0	0	2	2	4	11	13	29	24	40	44	50	42	18	20	13	5	6	2	2	1	2	

AM Peak Hour 09:45 to 10:45 (156 vehicles)
AM Peak Hour Factor 79.6%
PM Peak Hour 13:45 to 14:45 (230 vehicles)
PM Peak Hour Factor 76.7%

Monday 07/07/03 Channel: 2 Direction: S

0100	0200	0300	0400	0500	0600	0700	0800	0900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000	2100	2200	2300	2400	Totals
3	3	2	1	3	10	23	42	41	59	51	36													274
0	0	0	1	0	2	3	11	7	20	19	10													
2	1	0	0	1	4	9	14	8	15	8	26													
1	0	1	0	1	1	5	6	21	11	9														
0	2	1	0	1	3	6	11	5	13	15														

AM Peak Hour 08:30 to 09:30 (61 vehicles)
AM Peak Hour Factor 72.6%
PM Peak Hour Unavailable
PM Peak Hour Factor Unavailable

Makalena mauka bound.txt

Volume Count Report
Generated by MSC3000 Version 2.01 Copyright 1990-1992 Mitron Systems Corporation

Location Waipio Point Access Road
County 35
Recorder Set 07/02/03 12:01
Recording Start 07/03/03 12:00
Recording End 07/07/03 11:15
Sample Time 15 Minutes
Operator Number 22
Machine Number 7
Channel 1
Divide by 2
Summation No
Two-way Yes

07/03/03 Channel: 1 Direction: N

0100	0200	0300	0400	0500	0600	0700	0800	0900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000	2100	2200	2300	2400	Totals
							74	108	106	174	357	471	395	453	29	12	12	8	2159					
							32	24	25	34	141	72	94	166	20	2	5	2						
							16	35	29	25	97	113	75	152	5	1	1	1						
							10	22	18	45	65	168	116	84	3	7	1	1						
							16	27	34	70	54	118	110	51	1	2	5	3						

AM Peak Hour Unavailable
AM Peak Hour Factor Unavailable
PM Peak Hour 18:30 to 19:30 (544 vehicles)
PM Peak Hour Factor 61.9%

07/04/03 Channel: 1 Direction: N

0100	0200	0300	0400	0500	0600	0700	0800	0900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000	2100	2200	2300	2400	Totals
5	7	1	1	3	2	13	54	72	135	257	294	383	454	393	207	106	46	46	38	20	7	1	0	2545
3	2	1	1	0	0	3	8	16	30	88	74	101	117	93	67	34	6	10	6	8	5	0	0	
2	1	0	0	0	1	5	9	15	30	42	77	87	142	109	56	37	16	12	11	1	1	1	0	
0	4	0	0	1	2	15	19	34	64	59	73	107	80	42	22	12	12	12	12	9	1	0	0	
0	0	0	0	2	0	3	22	22	41	63	84	122	88	111	42	13	12	12	9	2	0	0	0	

AM Peak Hour 11:00 to 12:00 (294 vehicles)
AM Peak Hour Factor 87.5%
PM Peak Hour 12:45 to 13:45 (488 vehicles)
PM Peak Hour Factor 85.9%

07/05/03 Channel: 1 Direction: N

0100	0200	0300	0400	0500	0600	0700	0800	0900	1000	1100	1200	1300	1400	1500	1600	1700	1800	1900	2000	2100	2200	2300	2400	Totals
2	3	1	2	1	2	6	26	71	90	253	270	313	452	300	186	87	51	26	54	3	7	2	3	2211
0	0	0	0	0	0	3	2	4	17	65	53	87	92	62	60	21	10	7	14	1	1	0	2	

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APPENDIX B LEVEL OF SERVICE DEFINITIONS

The *Highway Capacity Manual* defines six Levels of Service (LOS), labeled A through F, from best to worst conditions. Levels of Service for signalized and unsignalized intersections are defined in terms of average user delays. Delay is a measure of driver discomfort, frustration, fuel consumption, and lost travel time.

For unsignalized intersections, the *Highway Capacity Manual* evaluates gaps in the major street traffic flow and calculates available gaps for left-turns across oncoming traffic and for the left and right-turns onto the major roadway from the minor street.

LEVEL-OF-SERVICE A: Little or no delay.

LEVEL-OF-SERVICE B: Short traffic delays.

LEVEL-OF-SERVICE C: Average traffic delays.

LEVEL-OF-SERVICE D: Long traffic delays.

LEVEL-OF-SERVICE E: Very long traffic delays.

LEVEL-OF-SERVICE F: Demand volume exceeds capacity, resulting in extreme delays with queuing that may cause severe congestion and affect other movements at the intersection.

APPENDIX C CAPACITY ANALYSIS WORKSHEETS

